



May 19, 2025

Village of Libertyville
200 E. Cook Ave
Libertyville, IL 60048

Attention: David Smith, AICP

Subject: **Memorandum to Resubmittal of Planning Commission Documents
– Greenway Chase Development
610 Peterson Road, Libertyville, IL**

To Whom It May Concern,

It is the intent of this memorandum to address the concerns heard at the Libertyville Planning Commission meeting on April 28th, 2025. Chief among those voiced were 1) Density of the proposed Greenway Chase community, 2) Traffic impacts to the Rt. 137 corridor, and 3) Buffering with respect to our eastern and western neighbors. This memorandum shall serve as primer to our next PC meeting, scheduled for June 9th, 2025. At that time, we will supplement our resubmittal documents with additional exhibits and have consultants available to provide expert testimony and address further questions.

Before exploring the changes submitted, it is important to address the purpose and intent of the proposed Greenway Chase development. We arrived at the site plan and product blend we're presenting through a collaborative process with Village Staff, a thorough review of the Libertyville Comprehensive Plan, and a detailed analysis of market trends and housing needs of the community. Based upon our research and collaboration with Village Staff, Pulte is proposing a blend of our single-family Springs and 55+ duplex Landings, which Pulte believes will best serve the Village of Libertyville. The Comprehensive Plan underscores the changing demographics of Libertyville, including emphasizing a need for a "greater diversity of housing at various price points to accommodate aging seniors, young professionals/families and working households." Consistent with this initiative, we were intentional in developing a plan that would enrich Libertyville's housing stock and provide opportunities for underserved segments of the market.

1). Density

Density was a primary topic at the April 28th meeting, and it underpins the purpose and intent of the development. Density is what allows Greenway Chase to be an attainable, viable project. The natural features and wetlands on the property drive the site layout, which has been designed to emphasize community open space versus individual ownership of larger lots. The existing natural features offer little in the way of flexibility for layout design. As a result, we have created a more clustered development where you have pockets of residential homes with paired clusters of open space, which is consistent with modern land planning practices and smart growth principles. Despite limitations of the site, approximately 42% of the site has been dedicated to open space. The result is a community configuration with abundant open space, a variety of home offerings, and a relatively low gross density of 3.18 du/ac. For context, it is important to emphasize that our



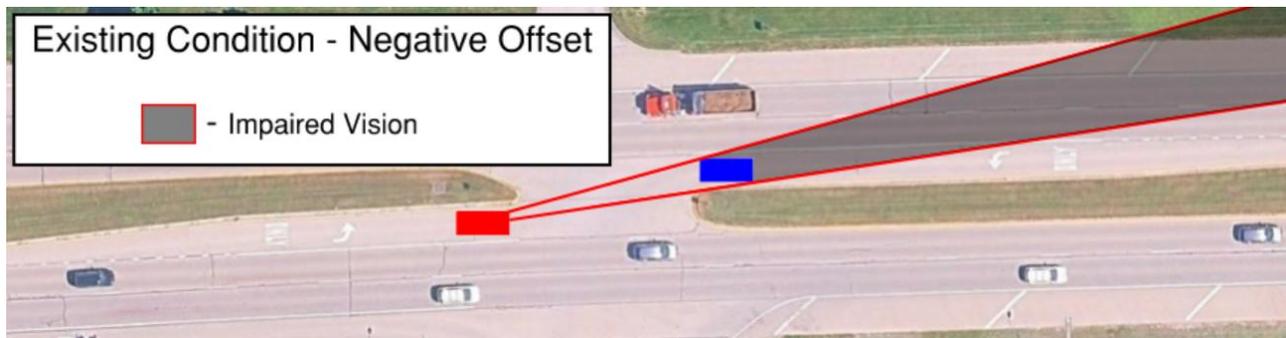
density is aligned with our eastern (3.05 du/ac) and western (5.33 du/ac) neighbors. In fact, we are considerably less dense than Victoria Park to the west.

Finally, Greenway Chase was planned with an insular layout, meaning that the community has little frontage toward non-resident passersby. We highlight this to assuage concerns around departure from the character of Libertyville. The density of Greenway Chase is such because we are catering to rising key segments of the market to achieve goals outlined by the Comprehensive Plan. Our development serves these goals.

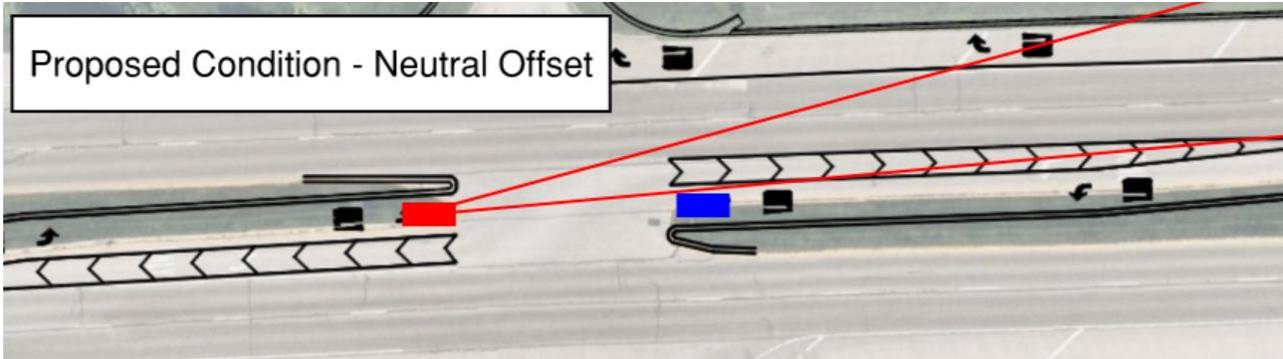
2). Traffic

Regarding impact, we wish to better emphasize the research Pulte has conducted and the improvements we will provide. KLOA conducted a thorough review of Greenway Chase's impact on traffic patterns. That study concluded that the area roadway system can accommodate the traffic generated by Greenway Chase. That same study was also reviewed by the Village of Libertyville and IDOT. IDOT was amenable to allowing Greenway Chase to preserve the two existing access points provided we modernize both intersections. This is a critical example of how the development will be able to improve Libertyville and the current conditions on Rt 137.

These intersections were built to their current state in the early 1990's and have remained largely unchanged as Libertyville developed around them. Pulte proposes that substantially extending storage, introducing dedicated deceleration lanes, and converting the offset of turning lanes from negative to neutral will significantly improve the safety and usability of both intersections. Further explanation, expert testimony, and additional exhibits will be provided at our hearing for clarification as to site traffic conditions, but the exhibit below shows a comparative improvement.



Above: Existing, negative offset conditions create a critical blindspot from oncoming traffic, greatly impeding left turns and u-turns. Blindspot represented by black shading.



Above: Proposed, neutral offset conditions shift oncoming turning traffic out of sightlines, providing longer field of vision and safer left turns and U-turns.

3). Buffers

The final major concern from Plan Commission's review of Greenway Chase was buffering to the eastern and western neighbors. Addressing this constitutes the most extensive change to our submitted documents as Pulte enhanced our already substantial buffers. Please consider our Landscaping Plan and note that we have adjusted our NE detention basin based on received feedback. This provided space to preserve 15 more trees between us and the residents in Forest Creek. We will be presenting renderings to this effect at Planning Commission. This is in addition to the tree preservation zone we're outlining in the site plan and setting out in our HOA documents.

To the west, we reviewed the existing mature evergreen screen on Victoria Park's perimeter and identified gaps which could be mitigated with additional plantings. We are providing illustrative exhibits to show how those will fill in. Further, we utilized our buffer outlot to add additional trees alongside the entire existing screen of Victoria Park. We intend to plant 37 additional trees along the border with Victoria Park to supplement the approximately 40 mature trees there presently. Since these are being planted in our outlot, they will be maintained by the HOA, thereby guaranteeing the growth and health of this buffer.

In summary, the western perimeter will have homes 40' from the property line, with 18.6' of that being HOA-controlled and maintained public space. We will preserve and enhance the existing evergreen screen while adding our own additional tree screen. To the east, our homes are 85' from the property line with 55' of that being maintained tree preservation. Both neighbors on the east and the west are approximately 25' from their property lines, so we are providing sufficient buffer and acting in fair accordance with our neighbors.

This memorandum underscores that we are listening to concerns that have been expressed by both the Plan Commission and public regarding density, traffic, and buffering. Other questions, including concept examples of our proposed park sites, supporting evidence around drainage, considerations around lot coverage, etc. will be discussed on June 9th in detail.



Pulte requests that Plan Commission consider again the purpose and intent of Greenway Chase. We are ameliorating issues identified in the Comprehensive Plan with more diverse housing stock and, subsequently, diversity in opportunity.

Thank you,

Nick Andriano
Entitlements Manager
Pulte Homes



May 19th, 2025

Village of Libertyville
200 East Cook Avenue
Libertyville, IL 60048

**Re: Greenway Chase Residential Subdivision
610 Peterson Road, Libertyville, Illinois
RE: PC 25-05, PC25-06, PC25-07, PC25-08, PC25-09, PC25-10, PC25-11**

This letter is prepared in response to the Development Committee Report prepared by Village staff dated April 23rd, 2025. The documents included in this resubmittal are provided for review pursuant to a June 9th, 2025 Plan Commission. See below responses to comments provided:

DEVELOPMENT REVIEW COMMITTEE REVIEW AND REPORT FORMAT:

1. A separate Supplemental Review Memorandum from the Development Review Committee lists review comments that Staff recommends the petitioner address satisfactorily at a later stage, prior to application for either final development considerations or any site development permits or building permits for construction (memo denotes stage required) and are not deemed to be required as conditions for the ordinances for the Preliminary Plat of Subdivision. See attached memorandum.
Response: Comment noted – these items will be addressed at a later date as indicated.
2. The remaining Development Review Committee review comments that are provided in this Development Review Committee staff report shall be addressed prior to resubmittal and prior to the Plan Commission making a recommendation to the Village Board of Trustees.
Response: Comment noted – refer to responses herein.

ECONOMIC DEVELOPMENT DIVISION COMMENTS:

1. The proposed development in Libertyville directly addresses housing shortages identified in the Lake County Housing Analysis (2023) conducted by Kretchmer Associates in partnership with Lake County Partners, a regional economic development organization—particularly the need for more senior-friendly housing in the South-Central region of Lake County, which includes Libertyville. By adding new age-restricted and single-family housing units, the project enables seniors to remain in the community while introducing a diverse mix of residents.
Response: Correct and noted. The purpose and the intent of the development was to fill an underserved niche in the Libertyville submarket. By providing greater diversity in housing stock (namely, more attainable single family and age-restricted ranch duplexes), we are providing opportunities that otherwise do not exist. This is outlined as a critical priority in the 2030 Comprehensive Plan.
2. With over (300) new residents anticipated, the development is expected to generate positive economic synergy, given its proximity to local retailers, restaurants, and service providers. Future homeowners in Greenway Chase are likely to shop and dine nearby, contributing to increased sales tax revenue and overall economic activity along the Peterson Road commercial corridor and downtown Libertyville. As

evidenced by the fiscal analysis provided, the sales tax generation is expected to be over \$91,000 annually. (Gruen Gruen and Associates, p. 8).

Response: Correct and noted.

3. Additionally, the removal of a functionally obsolete asset not only enhances the immediate neighborhood, but also stimulates broader economic growth by demonstrating the potential for future reinvestment opportunities throughout Libertyville. Considering the recent industrial and manufacturing growth in Libertyville, providing housing options supports attracting and retaining employers which depend on the skilled, stable, and diverse labor pool offered in Lake County.

Response: Comment noted.

ENGINEERING DIVISION COMMENTS:

1. **Site Plan Coordination** – The applicant has submitted a revised Preliminary Plat and Civil Engineering Plans with some subtle changes to lot layout and site plan. These changes include shifting lots to accommodate drainage out-lots and keep driveways away from intersections, along with the elimination of some crosswalks. Other site plan exhibits have not yet been updated with these modifications. All other site plans and exhibits included in the applicant’s submittal package should be coordinated with the civil plans to show the same lot layout and site geometry.

Response: Comment noted. A review of all submittal items has been performed and updated for consistency within the full submittal package provided.

2. **Storm Sewer** – The proposed layout of storm sewers should be revised to address the following comments for further review:

- a. Public Storm Sewer shall be located within the public right of way whenever possible. Where necessary, public storm sewer should connect as directly as possible to a detention basin via an appropriately sized side yard easement (15’ minimum easement width over Village-owned utilities).

Response: Storm sewer has been located in conjunction with the grading and site design. Direct storm sewer connections to the detention basins are proposed where feasible. Effort will be made to provide minimum 15’ side yard easements. As shown, narrowest side yard storm sewer easement is 12’. We understand additional revisions to the utility schematic may be necessary based on future coordination and the applicant is willing to coordinate with the Village on best placement of proposed utilities.

- b. Roadway drainage and sump pump discharges should be connected to public storm sewer, typically within the right of way.

Response: Comment noted – sump connections to be reviewed with final engineering.

- c. It’s anticipated that any storm sewer proposed for rear yard drainage would be privately owned and maintained by the association of property owners.

Response: Comment noted.

- d. The designer has indicated a preference to keep storm sewers in the proposed (primarily) rear yard locations; however, this would not be acceptable to the Village as proposed. Further comments should be anticipated upon review of future submittals.

Response: Comment noted – final placement of proposed storm sewer will be reviewed in more detail with the Engineering Department.

3. Traffic & Roadway Geometry

- a. A Traffic Impact Study (TIS) was submitted by the applicant and reviewed by the Village’s traffic consultant. Please be sure to address the comments within the Technical Memorandum dated April 17, 2025, prepared by Civiltech Engineering, Inc.

Response: Comment noted.

- b. A “Traffic Signal Warrant Analysis” was submitted and reviewed by the Village’s traffic consultant. Please be sure to address the comments within the Technical Memorandum dated March 19, 2025, prepared by Civiltech Engineering, Inc.

Response: Comment noted.

- c. IDOT approval would be required for the final plat of subdivision, and an IDOT permit would be needed based on work required in the IL 137 (Peterson Rd) right- of-way. The designer has indicated that coordination with IDOT is ongoing. Please CC the Village Engineering Division on all correspondence with IDOT.

Response: Comment noted. IDOT correspondence is ongoing. Acknowledged that IDOT signature would be required for the final Plat of Subdivision. Access Control Notes and IDOT feedback on the Plat will be incorporated prior to signature.

- d. Please note that any significant change to the proposed access points (including location and/or movement restrictions) after Village Board Approval (including preliminary site plan or PUD approval) would likely be considered a ‘major change’ to the original site plan approval, thus requiring the applicants to go back through the Village approval process for said change.

Response: Comment noted. It is not the petitioner’s intent to modify and or move the access points following Village Board.

4. Sanitary Sewer Main – Staff notes the following regarding proposed public sanitary sewer, and additional comments should be anticipated upon review of future plan submittals:

- a. Based on review comments from the Village’s sanitary sewer consultant and subsequent discussion between staff and the design engineer, it’s understood that a new sanitary lift station will need to be constructed within the subdivision. Flows from upstream areas and a portion of the development would be directed to a sanitary sewer that crosses Peterson Road and connects to the existing public sewer within Elderberry Drive.

The proposed location and route of the sanitary sewer appear to be generally acceptable. However, per previous conversations with the design engineer, the provision of a gravity pipe underneath Peterson Road is preferred to a force main. It is anticipated that a gravity pipe will be proposed for the roadway crossing unless analysis is provided for our review and approval, demonstrating that a gravity pipe is not feasible. The details of the lift station and force main will be reviewed during final engineering.

Response: Comment noted. Confirmation that the capacity of the downstream sanitary sewer is understood based on the latest sanitary sewer layout and inclusion of a lift station/forcemain. We understand that additional coordination with Village during final engineering will be required to include a finalized capacity memorandum and details on the lift station/forcemain design.

- b. Typically, sanitary mains should be located underneath the roadway pavement to allow for separation from other utilities (gas, electric, etc.) that need to be located within the parkway. Also, extra sanitary manholes (and shorter lengths of sewer between them) should be avoided wherever possible. Staff is open to discussions about the ideal location(s) for Sanitary main.

The designer has indicated that dry utilities will be located in the rear and that there would be no conflict with the proposed sanitary sewer alignment. However, it's anticipated that gas mains will need to be located in public parkways, and that locating sewer mains in the roadway would help reduce excess bends and manholes. Further comments should be anticipated upon review of future submittals.

Response: Agreed that gas mains will be located within the parkways or within the front yard easements. Final layout of the sanitary sewer to create the most efficient system necessary will be performed with final engineering. We welcome further coordination with the Village on preferred route.

5. **Driveway Locations** – To confirm the layout of proposed lots works with the proposed roadway configuration, the size and location of all driveways should be shown. It is acknowledged that lot layouts were modified since the previous submittal, and certain driveway locations were shown with the intent to address conflicts at street intersections. However, additional locations, e.g. Lot 75, may also need to be addressed. Regarding Lot 80, it is anticipated that the eastern leg of the intersection will be stop-controlled for westbound traffic. It is unclear if a required stop bar/stop sign would be in conflict with the driveway location.

Response: Final driveway/layouts will be provided. Lot 75 will provide a right-hand garage/driveway that will not conflict with any pedestrian crossings. Rotating the building footprint on Lot 80 to avoid conflicts with the proposed stop bar/stop sign is anticipated with final design.

6. **Existing Utilities and Easements** – The preliminary engineering plans indicate that various existing utilities and easements within the property would be abandoned. Copies of existing easement documents have been provided, which are still under review by staff for verification of the rights granted. Any approval of the Preliminary Plat would be conditional on proper written authorization for the abandonment(s), from each utility owner or beneficiary of an easement. Should any existing easements or utilities need to remain within the property, the plans will need to demonstrate how these are accommodated by the proposed subdivision layout.

Response: Comment noted. Coordination with existing and proposed utility companies regarding easement rights is anticipated and will be ongoing through final engineering. Pulte's dry utility coordination has been in contact with Northshore Gas, ComEd, and AT&T regarding vacation of said easements following building demolition and there are no foreseen issues with this being finalized ahead of closing on the property.

7. **Easement for potential future Watermain Extension** – The subdivision plat appears to identify a Watermain Easement which could accommodate a potential future extension of the Village water system to the north property line of the development. The minimum width of the Village watermain easement shall be 15 feet. This easement shall be dimensioned and defined on the plat of subdivision.

Response: The 15' watermain easement is provided as noted. Provisions and additional details will be coordinated with the Village at Final Engineering/Platting.

8. **Sewer & Water Recapture Fees** – The proposed development limits include "Parcel 25" as identified within Village Ordinance 91-O-56 and therefore appears to be subject to water and sewer recapture

fees. A preliminary estimate of the fee due per the ordinance is \$810,675.81 (which had been calculated based on a payment date of January 1, 2025). This amount was calculated for only the property within Parcel 25, totaling approximately 25.48 acres.

Response: Comment noted.

9. **Preliminary Plat / Plat of Annexation** – The preliminary plat should be revised as needed to address all comments, for further review. It should be noted that staff has not reviewed provisions or certificates on the Preliminary Plat or Plat of Annexation. In general, these items would not be considered finalized on the subdivision plat until they are reviewed and approved in conjunction with the Final Engineering Plans. Any comments on the Plat of Annexation would be provided prior to its execution. Further comments on these items should be anticipated during review of subsequent submittals.

Response: Comment noted.

10. **Development Agreement** –The applicant will enter into a development agreement with the Village, which development agreement must be finalized prior to approval of the Final Plat of Subdivision.

Response: Comment noted. Pulte has met with Village Staff and had preliminary discussions regarding formatting of the Development Agreement. This conversation is underway and ongoing.

SUPPLEMENTAL REVIEW MEMORANDUM – FOR REFERENCE ONLY (ADDITIONAL REVIEW/COORDINATION NECESSARY)

ENGINEERING DIVISION

1. **Stormwater Management** – In addition to the specific items noted within the DRC report referenced above, engineering review of the plans has been conducted by the Village’s consultant consultant and Lake County Stormwater Management Commission. Please be sure to address all comments including the following:
 - a. Comment within the attached review letter dated April 14, 2025, prepared by Kimley- Horn and Associates, Inc.
Response: Comment noted
 - b. Comments within the attached review letter dated April 23, 2025, issued by Lake County Stormwater Management Commission (SMC). Please Note: Response to these comments should be submitted by the Applicant to SMC directly, in addition to providing a copy of all responses and subsequent correspondence to the Village.
Response: Comment noted
2. **Curb and Gutter Type / Curb Cuts** – The Village standards are B6.12 Curb and Gutter, which should be depressed fully for each driveway location. Please revise the plans to show B6.12 C&G on the typical roadway section. Depressed curb locations (and driveway apron slopes) should be shown on final engineering plan submittals for coordination with locations of sidewalk curb ramps, parkway trees, light poles, and any other fixtures within the ROW.
Response: Comment noted. B6.12 to be utilized in lieu of M3.12.
3. **Grading** – Proposed grading (1-foot contours and spot grades at each key location) will be required for all outlots, ROWs, and public spaces.
Response: Comment noted. To be provided in final engineering.

4. **Overland Flow Routes** – It is noted that the plans have been revised to accommodate overland flow routes between homes within out-lots. Staff agrees with this approach, but notes that during final engineering, calculations will be required to confirm that the width of the out-lots is appropriate to convey overland flows.
Response: Comment noted. Overland flood route calculations will be provided using FlowMaster version 10.03. Cross sections of the major overland flood routes will be detailed and provided on the detailed grading and drainage plan sheets.
5. **Public Watermain** – The plan shows that a portion of the public watermain would be located within front yard easements. Village design standards state that public watermain within easements shall be Class 55 DIP.
Response: Comment noted. Watermain will be provided within the public right-of-way to the extent possible. Revisions forthcoming on final utility layout/schematic.
6. **Sanitary Sewer Flows** – While it is generally acceptable for a portion of the proposed development to be tributary to the existing Cass Avenue lift station, sufficient exhibits and flow calculations still need to be provided, demonstrating that the total flow tributary to Cass Avenue lift station will decrease compared to the existing condition.
Response: Comment noted.
7. **Utility Services** – Please show all utility services (water, sanitary, and storm) on the plans, stubbed to each lot. B-boxes should be located in the grass parkway, at least 4 feet behind the curb.
Response: Sanitary and Water services will be shown in final engineering plans. Please refer to utility Service blowups for details. Watermain service previously provided per Village of Libertyville standard detail WM-06. The B-box is currently shown 16.5' behind back of curb. B-Box to be relocated 4' behind back of curb. Please note relocating b-box within 4' back of curb will place b-box near proposed sanitary sewer alignment.
8. **Easements** – As stated within the DRC report referenced above, easement boundaries and provisions on the subdivision plat would not be considered finalized until they are reviewed and approved in conjunction with the Final Engineering Plans. Further comments should be anticipated during future submittals. At this time, please note the following items:
 - a. All stormwater management system elements (e.g. ponds, overland flow paths, storm sewers, etc.) should be located within an easement or stormwater restriction on the subdivision plat.
Response: Comment noted.
 - b. Utility easements should have a minimum width of 15 feet. Where easements are located along rear lot lines, the minimum width is typically 20 feet. Based on the proposed lot setbacks, it is understood that side yard easements would likely not be able to meet these standards. Side yard utility easements should still be provided where appropriate, and further comments may be issued upon review of future plan submittals.
Response: Comment noted. Side yard easements to be provided where applicable and feasible. There may be opportunity in final engineering to provide minimum 15' side yard easements. Otherwise, side yards will be designed to adequately handle overland flood routes and installation of storm sewer where necessary for design.
 - c. It is understood that dry utility locations may not be finalized prior to approval of the subdivision plat. Public utility easements should be located and sized as generously as possible to provide for new and future installations within the subdivision. (Per Village standards, wires, cables, pipes, conduit and similar improvements shall be shown as installed underground. Service equipment and

improvements shall be shown as installed along rear lot lines to the fullest extent practicable.)

Response: Comment noted – a 10' wide easement is provided within the rear yards but can be further review with staff prior to final design of dry utilities.

- d. Document submittals should show all proposed easements, including those needed in accordance with the provisions of annexation and development agreements, DRC Report comments, and / or Supplemental Review comments.

Response: Comment noted.

- e. Village Engineering staff can provide preferred easement provision language upon request, please contact the Engineering Division for more information.

Response: Comment noted.

- 9. **Landscaping in the Public Right of Way** – Landscaping installed in the ROW is subject to review and approval of the Public Works Department. Parkway trees shall be planted with at least (1) tree planted per lot or parcel and at least (1) tree every (60) feet. Trees shall be centered in the parkway between the curb and sidewalk. Canopy trees should be spaced so there is a minimum of 30' between trees. To the extent possible, parkway trees should also be located at least 10 feet from the edge of any driveway, 10 feet from manhole structures, and 15 feet from streetlights. Further comments may be issued upon review of future plan submittals.

Response: Comment noted.

- 10. **Construction Phasing** – During Final Engineering Plan submittals (and earlier if available), proposed construction phasing information should be included for review.

Response: Comment noted.

- 11. **Existing Conditions on the Property** – Staff noted the following items during review of the preliminary documents, and additional comments may be issued in response to future submittals:

- a. A clean "Existing Conditions" plan should be included in the Final Engineering Plans, in addition to the Demolition Plan showing all symbols and notes for proposed removals.

Response: Comment noted. Existing Conditions and Demolition Plan to be detailed out during final engineering.

- b. Existing water supply wells or septic systems on the property should be identified on the existing conditions and demolition plans, including notes requiring them to be abandoned in accordance with applicable State and County standards.

Response: Comment noted.

- c. Copies of existing easement documents have been provided, which are still under staff review for verification of the rights granted. Proper written authorization for the abandonment(s) would be needed from each utility owner or beneficiary of an easement.

Response: Comment noted.

- d. The survey and demolition plan show (4) existing connections to the public watermain along Peterson Rd. Based on a recent review of our records, it appears likely that there are only (3) connections to the public main; and that the watermain that loops around the east side of the existing building does not extend south to connect to the public main but westward in the front of the building to connect to the nearest water line inside the property. This information is noted for reference only, as staff is unable to verify the location of water lines or number of public watermain connections.

Response: Existing watermain connections to be coordinated with surveyor and field verified by contractor. Final Engineering Plans to be updated as needed.

- e. The boundary lines of parcels adjacent to the subdivision (particularly to the north) should be verified and coordinated on all drawings where they are shown.

Response: Comment noted.

- f. As noted within the Natural Resources Information Report dated January 29, 2025 prepared by McHenry-Lake County Soil & Water Conservation District, a field tile survey should be completed, and this information should be submitted for review.

Response: A field tile survey has been contracted and will be reviewed and included in a subsequent submittal.

- 12. **Development Agreement** – As noted within the DRC report referenced above, it’s anticipated that a Development Agreement will need to be finalized prior to approval of the Final Plat of Subdivision.

Response: Comment noted.

- 13. **Public Improvements (Resident Engineer)** – This project will include the construction of public improvements. Therefore, the project will be subject to the requirements described in Section 9 of the Village’s “Engineering Design & Construction Standards” (available on the Engineering Division webpage).

Response: Comment noted. Resident Engineer to be contracted at time of construction.

- 14. **Declarations / Association** – It’s anticipated the development agreement will need to require the establishment of declarations (DCCRs) and a homeowner’s association (HOA) to address long-term maintenance of the common elements, e.g. stormwater management, landscaping, etc.

Response: Correct. Draft CCRs will be included with our Final Engineering submittal.

- 15. **Lake County Sanitary Sewer Connection Fee** – The development site is tributary to Lake County’s Interceptor Sewer; therefore, Lake County sanitary connection fees will need to be collected at the time of construction permit issuance, by agreement between the Village and County. The Applicant should contact Lake County Public Works (LCPW) at 847-377-7500 to request a determination of the amount of credit that would be applied based on the existing sanitary sewer connection(s) for the property.

Response: Comment noted.

- 16. Permits will be required from the following outside agencies:

- a. **IDOT Permit**

Permit authorization from IDOT would need to be issued before any work can proceed within the IL 137 (Peterson Rd) right of way.

Response: Comment noted.

- b. **United States Army Corps of Engineers (USACE)**

It’s anticipated the USACE would need to issue wetland permit authorization associated with impacts to Waters of the United States, based on their jurisdictional determination letter dated 8/10/2021.

Response: The applicant will coordinate with the wetland specialist who prepared the wetland delineation and report for any additional coordination/authroziation required by the USACE.

- c. **Lake County Stormwater Management Commission (LCSMC)**

For this development, issuance of a Watershed Development Permit (WDP) has been deferred from the Village to LCSMC, for verification of compliance with the Lake County Watershed Development Ordinance; and the WDP would include any necessary approval for impacts to isolated wetlands. (In conjunction with the WDP, a Village Site Development Permit will also be issued, for verification of conformance with all other municipal regulations and standards.)

Response: Comment noted.

d. Lake County Health Department

For abandonment of existing water supply wells or septic systems on the property, documentation of compliance with Lake County Health Department permitting requirements should be submitted when available.

Response: Comment noted.

e. IEPA Sanitary Construction Permit

The Applicant would need to provide IEPA Sanitary Permit application documents for our review and execution. LCPW would also need to sign applicable areas of the IEPA Sanitary Permit application.

Response: Comment noted.

f. IEPA Water Construction and Operating Permit

The Applicant would need to provide IEPA Water Construction Permit application documents for our review and execution.

Response: Comment noted.

g. IEPA NPDES Permit (for construction disturbance greater than 1 acre)

Response: Comment noted. SWPPP to be provided as well.

17. Please note that these comments are based on the nature of the preliminary submittal documents, and additional comments should be anticipated in response to subsequent submittals.

Response: Comment noted.

PLANNING DIVISION

1. Identify the property lines on Sheet L1.1 and verify that the proposed Residential Development monument signs maintain a minimum setback of 15 feet or more.

Response: Comment noted. Sheet L1.1 has been updated to identify the property lines and more clearly show that the monument signs are setback a minimum of 15' from the property line.

2. All individual lot landscaping including trees, shrubs, ornamental grasses and perennials as depicted on Sheets L1.4 and L1.5 shall be regulated by the HOA Declarations, Covenant, Conditions and Restrictions (DCCR) to maintain a minimum amount of plantings. Any reduction to the minimum landscaping as reflected on the approved landscape plan by an individual homeowner shall be regulated by the HOA. Removal of trees on an individual lot shall be subject to both the HOA's DCCRs and the Village of Libertyville Tree Preservation Ordinance as amended from time to time.

Response: Comment noted

3. It is understood that the applicant has agreed to provide a fee in lieu of providing attainable housing. The amount for which will be incorporated into the Development Agreement.

Response: Correct and comment noted.

4. Sheet L1.0, Landscape Plan of the preliminary landscape plan, dated March 7, 2025, designed by Kimley Horn, shows a landscape buffer between Victoria Park and the subject development. Staff notes that the final quantities of trees are to be field verified to provide additional screening. While final quantities for the proposed landscape buffer have not yet been determined, they should not be less than what is currently shown. Consideration should be given to additional trees where possible and low ground shrubbery to fill any gaps within the landscape buffer.

Additional landscape elements should be concentrated at the edge of the drive aisles to block any headlights from filtering onto the single-family properties. Confirm that headlights will not filter onto the proposed home from the drive aisles of Victoria Park.

Response: Additional landscape has been added to the plan provide additional screening and prevent headlights from impacting the proposed homes.

5. Indicate the location of the seven Silver Maple trees that may be preserved, as referenced in the response letter to the February 17, 2025, ARC Staff Report.

Response: The seven trees saved following the Architectural Review Committee are all noted on the updated Tree Preservation Plan. Additionally, changes have been made to the grading to preserve additional trees in the northeast corner of the site. The plans have been updated to reflect those changes as well as other trees that had been missing on the Tree Preservation Plan. In total, there are 22 additional saved trees compared to the previous submittal. The specific identification numbers are the following: 158, 179, 301, 339, 340, 947, 956, 958, 961, 963, 964, 1112, 1477, 1485, 2016, 2017, 2018, 2019, 2020, 2021, 2022, and 2023.

6. The project shall include a minimum of 70 duplexes maintained as deed-restricted senior residences. Units shall be designed with ground-level primary bedrooms. Applicant shall submit appropriate deed restrictions on the seventy (70) duplex units to restrict sales to persons over 55 years of age prior to occupancy.

Response: Noted. We are happy to coordinate with staff the process by which we age-restrict the duplex lots.

7. Landscaped areas along property lines and tree preservation areas shall be in identified easements. Language regarding easement access and maintenance shall be incorporated in declarations for HOA.

Response: Comment noted.

8. An easement shall be provided with Village approved language designating future bikepath connectivity. HOA documents shall acknowledge and not refute ability to make future connection.

Response: Correct. We look forward to facilitating the connection. The easement will be protected and memorialized in the HOA documents.

9. At time of application for Final Plat and Plan, the applicant shall provide a copy of the draft DCCRs for review. This is to verify that all common areas will be accounted within the responsibilities of the associations or a separate master association. A sample multi-year maintenance budget shall be incorporated into the draft document for the HOA's planning purposes. The DCCRs shall be recorded prior to any occupancy. If the single-family homes and senior duplexes are to have separate associations, clarification should be provided how areas of shared responsibility will be handled (tree preservation zones detention, etc.).

Response: Comment noted and understood. All shall be outlined in the provided documents at time of final application and CCRs shall be recorded prior to any closings. Since the SF and Duplexes will have separate associations, a cost-sharing agreement will be outlined between the two with regards to the common maintenance and preservation zones.

FIRE DEPARTMENT

1. Fire Department previously received acceptable auto-turn diagrams, hydrant layout and acknowledgement of the residential fire sprinkler requirement.
Response: Comment noted.
2. Applicant shall notify the Fire Department if any changes impact these items.
Response: Updated plans to be routed to Fire Department as final engineering progresses.

PRELIMINARY ENGINEERING WITH RESPECT TO LAKE COUNTY WDO

1. If applicable, approval from the following agencies should be provided to the Village:
 - a. US Army Corps of Engineers
Response: Comment noted.
 - b. Illinois Environmental Protection Agency
Response: Comment noted.
 - c. Illinois Department of Transportation
Response: Comment noted.
 - d. Lake County Stormwater Management Commission (if applicable)
Response: Comment noted.
2. Should a portion of the site remain within the limits of Unincorporated Lake County, additional approvals from Lake County Planning, Building and Development will be required. Annexation documents were provided and appear to be in the process of being completed.
Response: The intent is for the site development to be entirely annexed into the Village of Libertyville.
3. Prior to its current condition, it appears that much of the land the development will occupy was used for agricultural purposes. It is recommended that a draitile survey be obtained to verify the location and conditions of any remaining tiles.
Response: A drain tile survey has been contracted and will be reviewed and circulated to the Village once made available.
4. A portion of the site was previously developed prior to 1992. The original development included approximately 5.6 acres of impervious surface associated with a structure and parking areas. Since this impervious area pre-dates, the Lake County Watershed Development Ordinance (WDO), it is considered exempt from stormwater detention.
Response: Comment noted. The stormwater management design has accounted for 5.66 acres of existing impervious area.
5. The proposed development will impact existing depressional storage and possibly riverine floodplain storage associated with Bull Creek. As part of final engineering, compensatory storage calculations will be required to verify the WDO requirements are met with respect to any fill placed within the

floodplain.

Response: As part of the proposed development, the existing depressional storage will be provided within Basin 1. The proposed design does not contemplate fill within the 100-year floodplain. Limits of disturbance are proposed 5' off of the existing 100-year floodplain to provide a buffer for constructability. Additional compensatory storage due to 100-year floodplain fill is not anticipated for this development.

6. During final engineering the following items will be required prior to permit issuance:

- a. Stormwater conveyance calculations. This includes storm sewer sized for the flow rate associated with the 10-year rainfall event and overland flow route calculations adequately sized to pass the peak flow rate associated with the 100-year rainfall event while providing adequate freeboard protection to all proposed structures.

Response: Comment noted. To be provided within final engineering plans and stormwater report.

- b. Description of how Runoff Volume Reduction measures will be incorporated into the overall design of the site.

Response: The detention basin areas will provide RVR benefits by providing some volume below the basin outlet.

- c. A detailed soil erosion and sediment control plan will be required with all applicable details, construction sequencing and standard Lake County notes.

Response: Detailed soil erosion and sediment control plan to be provided in final engineering with SWPPP.

- d. A detailed stormwater management report that includes all applicable calculations.

Response: Comment noted.

- e. Calculations verifying that the water quality treatment requirements of the WDO are provided within the development.

Response: Comment noted.

7. Prior to permit closeout, the following items will be required:

- a. As-Built survey of all stormwater management features including but not limited to:
 - i. Stormwater Management Basins
 - ii. Storm Sewer
 - iii. Critical overland flow path locations

Response: Comment noted.

- b. Placement of all stormwater management features in a deed or plat restriction.

Response: Comment noted.

- c. Maintenance plan for all stormwater features that clearly identifies the maintenance task, the frequency the tasks are performed and funding source.

Response: The Operations and Maintenance (O&M) Plan will be provided with final

engineering. We understand this document will be recorded with the County once prepared and approved.

8. Additional comments may be provided upon receipt and review of the requested information.
Response: Comment noted.

LAKE COUNTY STORMWATER MANAGEMENT COMMISSION

Preliminary Engineering Plans

1. Clarification is needed on whether the proposed grading will allow for the 100-year flow containment and conveyance to the downstream detention basin.
Response: Overland flood routes and detailed calculations will be provided in the final engineering plans to exemplify adequate conveyance of stormwater.
2. Per Section 506.03A of the WDO for Overland Flow Paths, if the upstream drainage area is less than twenty (20) acres, the storm sewer pipe and inlet systems sized for the base flood can be constructed in lieu of providing an overland flow path.
Response: Comment noted. Current intent is to provide overland flood routes where feasible to convey the 100-year event. If overland flood routes cannot feasibly convey the 100-year event, consideration may be taken to oversize storm sewer and inlets to convey the 100-year event. Relief may be requested for Basin 1, as the tributary area is 20.79 acres, just over the 20 acres threshold.
3. Based on the Typical Lot/Unit Grading detail provided, there is a 2% minimum slope proposed for rear yard swales. It appears that this slope may be difficult to achieve for overland flow from Detention Basin 3 to Detention Basin 1, as there is a total drop in elevation of 5 feet over a distance of 1,000 feet (0.5%).
Response: Comment noted. Rear yard drainage at the northwest corner to be evaluated in final engineering.
4. For Wetland C, the buffer will need to be determined, as it does not match Exhibit 8 of Bollinger Environmental Inc.'s Wetland Assessment Report. Please note that an 80/150 will be needed.
Response: Comment noted – Wetland C will be partially impacted but a buffer will be provided for the portion to remain. Additional coordination with the Wetland Specialist is underway and will be incorporated with final design.
5. Clarification is needed on where Detention Basin 3 spillway drains. It appears that it will drain west, north, and then to Detention Basin 1.
Response: Detail will be provided in final engineering. The intent is for Basin 3 to drain west, north, and then to Detention Basin 1.
6. On the east side of the development, will there be adequate space and grading for the overland flow route? The high-water level (HWL) in the overland flow route will need to be above elevation 691 based on the pond HWL.
 - a. Behind Lot 12, can the overland flow route be contained in the low spot without a retaining wall or a storm sewer sized for the 100-year event?
Response: The intent is to provide the overland flood route without a retaining wall and or storm sewer sized for the 100-year event. Details to be provided in final engineering.

7. For Lot 22:
- The rim of the inlet at the northern corner of the lot needs to be above elevation 702. The lowest adjacent grade (LAG) appears to be 693 based on the walkout basement and the drainage divide is between lots 22 and 23.
Response: Comment noted. The inlet at the northern corner will be evaluated considering the drainage divided between lots 22 and 23.
8. For Lots 14 – 15 and 54 – 55:
- Is there an overland flow route proposed between the lots? If so, will there be a drainage easement?
Response: All side yards that include an overland flood route will include be within a drainage easement. There is no overland flood route between lot 54 and lot 55 per the latest plans. There is an overland flood route between lot 14 and lot 15 that will be within a drainage easement.
 - Please provide the LAG for these lots and the HWL for the overland flow route.
Response: Comment noted. Overland flood route calculations and visuals will be provided in final engineering.
9. Please note that proposed LAGs will need to meet the requirements of the Watershed Development Ordinance (WDO – Section 506.03 & 507.01).
- Please confirm that the overland flow routes will have adequate freeboard, per the WDO. Please provide cross-sections for the overland flow routes in the final engineering plans.
Response: Comment noted.
10. Please clarify if there will be any fence or grading restrictions for the lots with drainage easements/overland flow paths.
Response: Fencing is provided as open aluminum per the CCRs, typically. Only single-family lots are allowed fences and considerations for restrictions within final OFR locations will be considered/implemented to not cause drainage issues.

Stormwater Report and Drainage Memorandum

- In Table 1, there appears to be a typo in the existing impervious area for Subcatchment C and should be 1.93ac. (identified in Table 4) instead of 2.93ac.
Response: The existing impervious area for Sub catchment C has been updated to 1.93 acres.
- In Table 3, there appears to be several typos in the impervious area values.
 - SMC believes the Pervious ROW area should be 2.3ac. instead of 230ac., Impervious ROW area should be 3.7ac. instead of 370ac.
Response: The pervious ROW is intended to be 230 SF, and impervious ROW is intended to be 370 SF. These values add up to the 600 SF mentioned in the next comment. The approach intended to calculate the CN value for 10 LF of right-of-way section as an example. The 600 SF is calculated by the 60' wide right-of-way and 10' sample section. Once the CN value of 91 was determined, the CN value of 91 was then applied to the entire roadway right-of-way section.
 - Please clarify if the total area should be 6.00ac. or 6.17ac.
Response: The total area is intended to be 600 SF. Please see explanation in previous comment response.

- c. Please clarify if there truly is zero impervious within the outlots and parks.
Response: The impervious and pervious areas within the outlots and parks have been confirmed. The outlots will have a CN value of 80. The parks will have a composite CN value of 81.68. Majority of the park area will be pervious greenspace, with select portions dedicated to impervious area such as concrete and pavement, gravel, and rubber. The Table 3 in the stormwater report has been updated to reflect this.
3. Please clarify if the rear yards of lots 53 – 55 will drain to Basin 1 or Basin 3 given the overland flow routes and storm sewer configuration?
Response: Lots 53 – 55 are intended to drain to Basin 3.
4. Please clarify if there will be a buffer for Wetland C and if there will be wetland area partially remaining on lots 21 and 22 or wetland buffer.
Response: The wetland will be removed from lots 21 and 22 and an appropriate buffer will be provided.
5. On page 37 of the report, the HydroCAD output table (Proposed Conditions – Theoretical Model, page 7), there appears to be an error in elevations for Pond 6: West Basin and the elevations look to be much higher than what they should be (706.13 instead of 733.13, 707.29 instead of 777.29 etc.)
Response: This portion of the report has been updated to only show the 2-year and 100-year rainfall events. The intent of the HydroCAD multi-event tables is to exhibit the theoretically required volumes. Similarly, for the Proposed Conditions – Actual Model, the intent of the multi-events table is to exhibit both the required volumes and also determine the peak event inflow rates. The peak event inflow rates are then considered for sizing the basin overflow weirs. The invert of the weirs will be set at the high water elevation associated with the 100-year 24-hour event.

CIVILTECH TECHNICAL MEMORANDUM REVIEW – SIGNAL WARRANT ANALYSIS

Signal Warrant Analysis Comments:

1. It should be noted that IDOT does not allow Warrant 2 (Four-Hour Volume) and Warrant 3 (Peak Hour Volume) to be used when warranting a traffic signal on a Strategic Regional Arterial (SRA)
Response: Noted. This was stated on page 6 of the signal warrant analysis but was included for the purpose of the evaluation.
2. For the purposes of this exercise, we concur with estimating the hourly volumes on for the intersection approaches that were not included in the traffic count data using the methodology provided. Normally, physical counts are required for the entire period analyzed in a warrant study.
Response: Noted.
3. Although it does not affect the outcome of this analysis, the number of right turning vehicles should be reduced per IDOT's methodology for warrant analyses.
Response: Noted.
4. We concur with the finding that a traffic signal is not currently warranted and will not be warranted in the

future even with the added Greenway Chase development traffic at the intersection of Peterson Road and Elderberry Drive.

Response: Noted.

CIVILTECH TECHNICAL MEMORANDUM REVIEW – TRAFFIC IMPACT STUDY

Traffic Analysis Comments:

1. IDOT concurrence with the conceptual site access geometrics is noted, although there are still IDOT comments to be addressed. An approved IDOT access permit must be acquired prior to final development approval.

Response: Noted.

2. While we understand there are no existing stop signs at the Libertyville Manor entrances, they should be shown on the TIS figures to reflect the actual type of traffic control on those approaches, especially the total traffic exhibit on Figure 9, which should reflect proposed traffic control.

Response: Noted. Figures 5 through 9 have been revised to show the proposed stop signs. See revised traffic impact study.

3. In Table 4, the northbound left turn delay should be 55.1 instead of 5.1 for the weekday morning peak hour under existing conditions. This was not addressed in the revised study.

Response: Noted. See revised traffic impact study.

4. We concur with the findings of the traffic signal warrant analysis, as reviewed by Civiltech on March 10, 2025 in a separate memo.

Response: Noted.

We trust these responses and the description of changes above adequately address your comments. If you have any questions or require any additional information, please contact me at 331-425-8039.

Sincerely,



Ryan Martin, P.E.
Kimley-Horn and Associates, Inc.
Phone: 331-425-8039
Email: Ryan.Martin@kimley-horn.com

STATE OF ILLINOIS)
)
COUNTY OF LAKE)
)
VILLAGE OF LIBERTYVILLE)

**PETITION TO THE LIBERTYVILLE VILLAGE BOARD, APPEARANCE REVIEW
COMMISSION, AND PLANNING AND ZONING COMMISSION FOR
DEVELOPMENT APPROVAL**

THE UNDERSIGNED Petitioner, Pulte Home Company, LLC, a Michigan limited liability company (“**Petitioner**” or “**Pulte**”), as the contract purchaser of the property legally described on **Exhibit A** (“**Subject Property**”), respectfully petitions the Village of Libertyville (“**Village**”) to:

(i) Annex an unincorporated 13.82-acre portion the Subject Property to the Village pursuant to a separate petition submitted by the owners of record;

(ii) Upon annexation of the Annexation Property, rezone the Subject Property to the Village’s R-7 “Single-Family Attached Residential District”;

(iii) Amend the Comprehensive Plan Future Land Use Map to “Single-Family Attached Residential”;

(iv) Approve a Special Use for a Planned Development for the development of Greenway Chase, a one hundred thirty-four (134) unit mixed-residential development with associated deviations as more specifically detailed on **Exhibit B** (“**Requested Deviations**”) and depicted on the submitted development plans;

(v) Approve a Planned Development Concept Plan;

(vi) Approve a Special Use to permit Senior Citizen Housing in the R-7 zoning district;

(vii) Approve a Preliminary Plat of Subdivision, a copy of which is attached as **Exhibit C** (“**Subdivision Plat**”); and

(viii) Approve such other relief from the Libertyville, Illinois Municipal Code (“**Village**

Code”) as may be deemed necessary and appropriate to develop the Subject Property consistent with the plans submitted herewith.

In support of this Petition, the Petitioner states to the Village of Libertyville as follows:

1. SB Reserve Properties, Inc., and Illinois corporation (owner of Parcels 1, 2, 3, and 6 as legally described on Exhibit A) and SB Holdings, Ltd., an Illinois corporation (owner of Parcels 4 and 5 as legally described on Exhibit A) are the owners of the Subject Property and shall be collectively referred to as “**Owner**”.

2. The Petitioner, Pulte Home Company, LLC, whose address is 1900 E. Golf Road, Suite 300, Schaumburg, IL 60173, is the contract purchaser of the Subject Property.

3. The Subject Property consists of approximately 42.11 acres of real property located north of Peterson Road/IL Route 137 and west of Milwaukee Avenue/ IL Route 21, and has a common address of 610 Peterson Rd, Libertyville, IL 60048.

4. The Subject Property is currently improved with the approximately 174-bed Libertyville Manor Extended Care facility (“**Libertyville Manor**”).

5. Petitioner seeks to acquire the Subject Property to demolish Libertyville Manor and redevelop the Subject Property with sixty-four (64) single-family homes and seventy (70) age-restricted duplex units.

6. That part of the Subject Property being described as Parcels 2 and 3 on Exhibit A (“**Annexation Property**”), which comprises approximately 13.82 acres of the Subject Property, is located in unincorporated Lake County and zoned R-1 “Residential District”.

7. Owner separately filed a petition for annexation of the Annexation Property to the Village.

8. That part of the Subject Property being described as Parcels 1, 4, 5, and 6 on Exhibit

A (“**Annexed Property**”) is located in the Village and zoned IB “Institutional Buildings District”, R-1 “Single-Family Residential District,” and C-3 “General Commercial District” as depicted on the zoning exhibit attached as **Exhibit D (“Zoning Exhibit”)**.

9. Petitioner seeks to rezone the Subject Property to the R-7 “Single-family Attached Residential District” with a special use for a planned development.

10. The existing land uses surrounding the Subject Property are as follows:

- a. North: R-1 “Residential District” in unincorporated Lake County – ComEd Substation.
- b. East: R-1 “Single-Family Residential District” and C-3 “General Commercial District” in the Village of Libertyville – Forest Creek Subdivision (townhomes) and various commercial uses.
- c. South: R-4 “Single-Family Residential District” in the Village of Libertyville – Wineberry Subdivision (single-family detached).
- d. West: IB “Institutional Buildings District” in the Village of Libertyville – Victoria Park Subdivision (townhomes, 55+ age-restricted).

11. The proposed entitlement requests meet all Village and State requirements for the development of property and will facilitate the beneficial use of the Subject Property as stated below.

SUMMARY OF DEVELOPMENT

The Petitioner and builder is Pulte Home Company, LLC. Pulte is a multi-brand homebuilding company capable of serving homeowners during all phases of life. Pulte has been building homes in the Chicago area since the 1960s, bringing decades of construction and development experience to each project. Petitioner has found success building communities that

offer quality housing options designed to serve the diverse needs of the local community. Petitioner looks forward to providing new housing options for Libertyville's existing residents as well as those that would like to live in a well-respected municipality with excellent schools and park facilities.

Petitioner seeks Village approval of a mixed-use residential development consisting of sixty-four (64) single-family homes and seventy (70) age-restricted duplex units to be known as Greenway Chase (“**Greenway Chase**” or “**Development**”). The proposed Development will provide the Village with additional housing options, will help support retail uses along Peterson Road, and will complement the surrounding residential subdivisions, including the Forest Creek townhomes to the east and age-restricted townhomes in the Victoria Park subdivision to the west.

Greenway Chase has been strategically designed to prioritize large contiguous common open spaces. These common open spaces serve multiple purposes. To the north, the common open space minimizes the impact of the Development on natural features such as wetlands and wooded areas. To the east and west, common open spaces will provide a landscape buffer or transitional yard to minimize impact on surrounding development. To the south, the common open space will feature a rolling berm with new manicured landscaping that will both improve the character of the roadway frontage and serve as a buffer yard for the benefit of future residents. Within the development, common open space includes a newly dedicated park, a private amenity lawn for the age-restricted portion of the community, and a tree preservation zone. In total, forty-two percent (42%) of the area of the Subject Property will be preserved as common open space that is either dedicated to preservation, landscape enhancement, or recreational spaces designed to meet the needs of future residents. This represents 140% of the open space required under the Village's PUD standards.

There are five (5) wetland located on the Subject Property. These wetlands are primarily located along the northern perimeter of the Subject Property. The Development entirely avoids impact to jurisdictional U.S. Army Corps of Engineers' wetlands and minimizes impact to lower quality wetlands regulated by the Lake County Stormwater Management Commission (SMC). Two (2) stormwater basins are proposed within the northern section of the Subject Property, proximate to the preserved wetlands. The naturalized detention basins will both serve the stormwater needs of the Development and support the continued function of the wetlands. While the basins will be manmade, they are designed with wetland signatures and will be improved with native plantings, making them complimentary to the existing wetlands and open space to the north. Post development, the combination of the wetlands and associated native detention basins will improve drainage and water quality while creating new habitat for the benefit of the broader community.

The common open space provided by the Development will help to satisfy the recreational needs of future residents of the Development. The preserved wetlands and new native detention basins will provide passive open space for the benefit of residents. Two (2) separate amenity/park spaces are intended to provide more active recreational space within the Development. Within the age-restricted portion of the Development, a 1.41-acre private amenity space is planned. This space will feature a pedestrian pathway, yoga lawn, and outdoor seating/bench area. A separate 1.14-acre public park area has also been planned near the west entrance of the Development. This park space is anticipated to be improved with playground equipment and will be ultimately dedicated to the Village for use by the public. While the park spaces are compact, they are strategically located and appropriately sized to provide recreational opportunities for Greenway Chase residents.

The aforementioned natural features and proposed common open space guides the ultimate layout and design of the proposed Development. Of the 134 homes in the Development, ninety-three percent (93%) back up to open space! The adjacent common open space will make individual lots feel larger than their dimensions would otherwise suggest. As depicted on the Subdivision Plat, the single-family homes have been strategically placed at the west end of the Development adjacent to the Victoria Park Subdivision to the west. The duplexes have been placed in transitional locations between the proposed single-family homes and adjacent retail uses and Forest Creek townhomes. While Greenway Chase is bound by ComEd property to the north, the ComEd property consists primarily of sprawling open space that can be reasonably anticipated to remain in its current condition. Beyond the ComEd property are the Donnelley Prairies and Oaks Preserve, Casey Trail and Greenway, and Timber Creek Park areas, which total over 300 acres. A Village easement has been provided for a potential future pathway connection from Greenway Chase to the existing pathway network in the park and open space areas to the north and northwest.

Landscape treatments will be utilized to enhance the visual appeal of the community, including a mix of parkway trees, shade trees, and shrubs. The landscape design enhancements will include decorative plantings around the perimeter of the Development. Existing landscape buffers will be preserved where feasible and enhanced throughout the perimeter of the community to create the feel of a residential enclave and separate the single-family homes from the age-restricted duplexes. Extra care has been taken to provide additional landscape buffering between the Development and Peterson Road to the south. Within the community, Pulte plans foundation landscape plantings for all of the homes. A typical foundation planting plan is included within submittal materials, but is not intended to be a controlling PUD document. Pulte intends to depict the nature of the foundation or on-lot plantings that are proposed, but wants homeowners to have

ultimate flexibility to modify plantings as may be appropriate.

The Subject Property is located in close proximity to key transportation corridors like US Route 45, Milwaukee Avenue, and I-94, and will be easily accessible via two access points off Peterson Road. Both access points will align with the subdivision entrances to the south of Peterson Road to address and avoid conflicting traffic movements and provide safe and convenient access to and from the Development. The curvilinear roadway internal to the Development will provide a more interesting streetscape and ensure homes are not all identically situated along street frontage. The streets will be dedicated to the Village and have been designed to circulate traffic efficiently and safely.

As part of the Greenway Chase development, sidewalks throughout will provide interconnectivity within the Development. The future residents will benefit from the development's proximity to shopping districts, recreational amenities, and employment centers. Within a 3-mile radius, there are a large variety of retail establishments, groceries, restaurants, entertainment opportunities, and recreational amenities. Nearby employment corridors offer future residents the additional feature of conveniently located employment opportunities.

Pulte proposes two distinct housing lines – the “**Springs Series**” (single-family homes) and the “**Landings Series**” (age-restricted duplexes). Pulte strategically identified these homes as being appropriate for the Subject Property based on both demographic trends in the Community and the direction set forth in the Village's 2030 Comprehensive Plan. According to Chicago Metropolitan Agency for Planning (“**CMAP**”) data, the Village had a net loss of population between 2000 and 2022, with the Village's total population shrinking by about one percent. In that same timeframe, the Village's population of 50+ year olds grew by 10%, and as of 2022, 58.8% of Village households were 1-2 persons. Despite the aging population and small household

size of the Village, only 2.7% of the homes in the Village are duplexes. Further, the percentage of attached single-family homes in the Village's housing stock shrank over the past 15 years. At the same time, houses have been getting larger, with the share of 3-, 4-, and 5-bedroom homes all increasing, with the largest growth percentage being for homes with 5 or more bedrooms. Given the statistical data, there is a need for new housing in the Village, specifically for smaller single-family homes and duplexes as proposed for Greenway Chase.

The Springs Series homes will consist of sixty-four (64) single-family detached homes located on the west half of the Development. The Landings Series will consist of seventy (70) age-restricted duplex units (35 buildings) located along the east half of the Development.

The Springs Series single-family detached homes will consist of 30'-wide homes on a standard 41'-wide and 4,500 square foot lot. The Springs Series will offer five (5) model options ranging in size from 1,678 to 2,567 square feet, 3 to 5 bedrooms, 2.5 to 4 baths, and feature a two-car garage. The smaller footprint of the Springs Series homes will help address a gap in the Village's housing market. The architecture of the Springs Series can be characterized as modern American and will be based on traditional styling that includes farmhouse, low country, heritage, and craftsman designs. While not new to Pulte, the Springs Series was only recently introduced to the Chicago region (being first approved for Naperville's "Polo Club" subdivision) to promote lower cost options targeted at the young professional demographic. The smaller 41' lots of the Springs Series reduce development costs by requiring less road, less watermain, less sewer, and less overall infrastructure per lot. This reduction in infrastructure also translates to reduction in long-term maintenance expenses for public improvements.

As a national leader in new home construction, Pulte has first-hand knowledge of the changing dynamics in the residential real estate market. Trends toward a more "experiential"

lifestyle have affected buying patterns, particularly for younger homebuyers. Younger homebuyers are likely to avoid the maintenance and repair associated with older homes. Younger homebuyers generally prioritize amenitized common open space over large private yards. Young families tend to lead much busier lives and prioritize spending time and money on “experiential” activities rather than possessions – like a large private yard. These trends have been reflected in Pulte’s residential subdivisions, particularly over the last decade, which emphasize common amenities over private yards. This shift from private space to community space lessens residents’ maintenance obligations without sacrificing amenities. With less time and money spent on yard maintenance, Pulte has seen an increase in purchaser demand for housing that features more options associated with the livability of the home, including sunrooms, smart home upgrades, upgraded flooring, gourmet kitchens, and upgraded bathroom options.

In addition to the single-family detached homes in the Springs Series, Pulte also proposes its age-restricted Landings Series duplex homes to satisfy demand in the community for age-restricted housing. In the age-restricted residential market, homebuyers are interested in smaller lots, which in turn reduce homeowners’ association fees for maintenance and upkeep. Age-restricted communities have gained in residential market share as our population ages. Given the trend of demographics in Libertyville, there is a need to add housing product that is specifically targeted to the aging population.

The Landings Series duplexes will vary in size from 1,577 square feet to 2,697 square feet, 2 to 4 bedrooms, and 2 to 4 baths, and will feature a two-car garage. The architecture of the duplex homes will be consistent with market trends for age-restricted communities, with low maintenance exteriors and quality materials that will withstand the test of time. These age-restricted homes will be developed in accordance with the Fair Housing Amendments Act of 1988, as amended

from time-to-time (“**FHAA**”), including, but not limited to, the provisions of the “Housing for Older Persons Act of 1995” (“**HOPA**”).

As previously mentioned, the duplexes will include a separate, private outdoor amenity space consisting of approximately 1.41 acres that will feature a yoga lawn, pedestrian pathway, and seating area with benches; affording an outdoor recreational opportunity for the residents within the age-restricted community. A homeowners’ association with two separate committees and budgets will be established for Greenway Chase, with each committee being responsible for either the single-family or age-restricted pod within the community. The HOA will perform maintenance on the amenity spaces, open areas, and green space areas of the Greenway Chase neighborhood. Snow removal, lawn mowing, landscape maintenance, and exterior maintenance will be provided for the age-restricted duplexes.

The proposed mixed-use residential community will be an appropriate transitional use along a principal arterial roadway corridor like Peterson Road. While the area further east along Peterson Road towards Milwaukee Avenue is generally retail/commercial, the area surrounding Greenway Chase hosts a variety of residential housing developments. Greenway Chase will complement these subdivisions, which include the Forest Creek townhomes to the east and age-restricted townhomes in the Victoria Park subdivision to the west. In addition, the proposed homes will help support the existing retail and commercial uses to the east, which are easily accessible via Peterson Road. Given the proposed Development’s location and accessibility to nearby commercial uses and local parks, the Subject Property presents an excellent housing opportunity that is suitable for a variety of homebuyers, including some of the fastest growing housing segments of our population – young professionals and the 55+ demographic.

**APPROVAL OF A MAP AMENDMENT REZONING THE SUBJECT
PROPERTY TO R-7 AND AN AMENDMENT TO THE COMPREHENSIVE PLAN'S
FUTURE LAND USE PLAN TO SINGLE-FAMILY ATTACHED RESIDENTIAL**

The requested zoning map amendment from R-1 “Single-Family Residential District” and IB “Institutional Buildings District” to the R-7 Single-family Attached Residential District meets the requirements for a rezoning pursuant to Village Code Section 26-16-14.5 and is appropriate based upon the following factors:

a. The consistency of the proposed amendment with the purposes of this chapter.

The proposed map amendment (rezoning) and amendment to the Future Land Use Map (“**Future Land Use Map**”) contained in the Libertyville 2030 Comprehensive Plan adopted March 23, 2021 (“**Comprehensive Plan**”) (collectively, the “**Map Amendment**”) are consistent with the purposes of Village Code. A stated purpose of the map amendment process is to provide a means for making changes the zoning map in light of changing, newly discovered or newly important conditions, situations or knowledge. In this case, the existing Libertyville Manor is obsolete, rendering the land value of the Subject Property greater than the value of the Subject Property as currently improved.

The Annexation Property comprising approximately 13.82 acres of the Subject Property is currently zoned R-1 “Residential District” in unincorporated Lake County. Upon its annexation to the Village, it will be automatically zoned R-1 “Single-Family Residential District.”

The Annexed Property comprising the remainder of the Subject Property is zoned IB “Institutional Buildings District”, R-1 “Single-Family Residential District,” and C-3 “General Commercial District” in the Village of Libertyville as depicted on Exhibit D.

In addition to the existing zoning designations of the Annexation Property and Annexed Property, the Future Land Use Map contained in the Comprehensive Plan slates the future use of

the Subject Property for institutional, destination commercial corridor, and single-family attached residential (“SFAR”). The zoning and future land use designations are inconsistent with the proposed Development, such that upon annexation of the Annexation Property, Petitioner seeks to rezone the entire Subject Property to the Village’s R-7 “Single-Family Attached Residential District” and amend the Future Land Use Map’s designation to SFAR.

While commercial and retail uses exist further east and west along Peterson Road, the immediately adjacent properties have been largely developed with residential subdivisions, including the Forest Creek townhomes to the east, single-family homes in the Wineberry subdivision to the south, and age-restricted townhomes in the Victoria Park subdivision to the west. The requested Map Amendment will facilitate redevelopment of the Subject Property with a mixed-residential development. The proposed development will be consistent with trends in the market, will complement the surrounding residential uses, and will help support the commercial and retail uses in the area, providing for the Subject Property’s highest and best use.

b. The existing uses and zoning classifications of properties in the vicinity of the subject property.

The proposed Map Amendment will facilitate development of the Subject Property consistent with the uses and zoning classifications of properties in the vicinity of the Subject Property. While commercial uses exist to the east and west along Peterson Road, the Subject Property is isolated from those commercial and retail uses. The Subject Property is bound by residential uses on its east, south, and west sides, such that commercial development on the Subject Property would be out of character with the trend in development.

The properties to the east are currently zoned R-1 and C-3 with Future Land Use Map designations of SFAR and destination commercial corridor. This includes the townhome

development in the Forest Creek Subdivision. The duplexes in the proposed Development have been strategically located adjacent to this townhome development to provide a logical transition between Forest Creek and the single-family detached homes in Greenway Chase. Various commercial and retail uses are located east of the Subject Property along Peterson Road, which uses the proposed Development will help support. The properties to the south of the proposed development are zoned R-4 and comprise the single-family detached homes in the Wineberry Subdivision. Finally, while the property to the west is zoned IB with a future land use designation as institutional, its use is residential and comprised of a 55+ age-restricted community commonly known as Victoria Park.

- c. The trend of development in the vicinity of the subject property, including changes, if any, in such trend since the subject property was placed in its present zoning classification.*

The proposed Map Amendment is consistent with the trend of development in the vicinity of the Subject Property since the Subject Property was placed in its present zoning classification. Libertyville Manor on the Subject Property was constructed in the early 1980's. While technically institutional, it is residential in nature. Since Libertyville Manor's construction, the surrounding properties have been developed with residential uses – the Forest Creek Subdivision to the east was developed in the late 1980's, the Wineberry Subdivision to the south was developed in the 1990's, and the Victoria Park Subdivision to the west was developed in the mid 2000's. The proposed Map Amendment will bring the zoning designation and Future Land Use Map designation of the Subject Property up-to-date and consistent with the proposed use and existing residential uses of the adjacent properties.

- d. The extent to which the value of the subject property is diminished by its present zoning classification.*

The value of the Subject Property is diminished by its present zoning classification. As noted, Libertyville Manor was constructed in the early 1980's but is now obsolete, rendering the land value of the Subject Property greater than the value of the Subject Property as currently improved. As such, the Petitioner has been provided with the opportunity to purchase and redevelop the Subject Property with its highest and best use as a mixed residential development. The proposed Map Amendment and Development will improve the Village's housing stock, serve under-represented demographics, and create new revenue streams through property, utility, and sales taxes. Further, the proposed Greenway Chase subdivision will provide a seamless transition between the surrounding residential uses while supporting the existing retail and commercial uses along Peterson Road.

e. The extent to which such diminution in value is offset by an increase in the public health, safety and welfare.

The diminution in value of the Subject Property under its present zoning classification is not offset by an increase in public health, safety and welfare. As noted, the existing Libertyville Manor is obsolete and an underutilized use of the Subject Property. In comparison, Petitioner's proposed redevelopment of the Subject Property with a mixed residential use will benefit the public health, safety, and welfare of the community. The development of Greenway Chase will improve the Village's housing stock, serve under-represented demographics, and create new revenue streams through property, utility, and sales taxes. Further, the proposed Greenway Chase subdivision will provide a seamless transition between the surrounding residential uses while supporting the existing retail and commercial uses along Peterson Road.

f. The extent, if any, to which the use and enjoyment of adjacent properties would be adversely affected by the proposed amendment.

The use and enjoyment of adjacent properties will not be adversely affected by the proposed Map Amendment. As previously noted, the proposed Development is consistent with the uses of the adjacent properties, which are largely residential. The Forest Creek townhome community is situated east of the Subject Property and townhomes in the age-restricted Victoria Park townhome community are located to the west. The proposed Greenway Chase Subdivision will complement these developments and will provide a logical transition between uses. In addition, while Greenway Chase is bound by the ComEd property to the north, the ComEd property consists primarily of sprawling open space that can be reasonably anticipated to remain in its current condition. Beyond the ComEd property are the Donnelley Prairies and Oaks Preserve, Casey Trail and Greenway, and Timber Creek Park areas, which total over 300 acres. A Village easement has been provided for a potential future pathway connection from Greenway Chase to the existing pathway network in the park and open space areas to the north and northwest. Finally, the proposed Development will be an appropriate transitional use along a principal arterial roadway corridor like Peterson Road. The uses along Peterson Road become more retail and commercial in nature the further east you travel, which uses the proposed Development will help support.

g. The extent, if any, to which the value of adjacent properties would be adversely affected by the proposed amendment.

The value of adjacent properties will not be adversely affected by the proposed Map Amendment. To the contrary, the proposed Development and requisite Map Amendment will enhance the value of the adjacent properties. The Libertyville Manor building was constructed in the 1980's, but its use is now obsolete. Petitioner now has the opportunity to purchase and redevelop the Subject Property with its highest and best use as a mixed residential development.

The surrounding area is largely residential in nature, making the proposed R-7 zoning and Map Amendment consistent with the use of the adjacent properties.

The Forest Creek subdivision to the east was developed in the late 1980's, the Wineberry subdivision to the south was developed in the 1990's, and the Victoria Park subdivision to the west was developed in the mid 2000's. Development of the Subject Property with a new, up-to-date mixed residential subdivision that is complementary to the surrounding uses will attract a variety of homebuyers, including some of the fastest growing housing segments of our population – young professionals and the 55+ demographic.

Landscape treatments will be utilized to enhance the visual appeal of the community, including a mix of parkway trees, shade trees, and shrubs. The landscape design enhancements will include decorative plantings around the perimeter of the Development to improve the character of the roadway frontage and serve as a buffer yard for the benefit of future residents. Existing landscape buffers will be preserved where feasible and enhanced throughout the perimeter of the community to create the feel of a residential enclave and separate the single-family homes from the age-restricted duplexes. Extra care has been taken to provide additional landscape buffering between the Development and Peterson Road to the south to provide an attractive roadway façade for residents and passerby's.

h. The extent, if any, to which the future orderly development of adjacent properties would be adversely affected by the proposed amendment.

The future orderly development of adjacent properties will not be adversely affected by the proposed amendment. To the contrary, the proposed zoning designation permitted by the Map Amendment will be more consistent with the trend in development of the area as compared to the existing zoning and Future Land Use Map designations. Libertyville Manor was constructed in

the early 1980's. While technically institutional, its use is residential in nature. While commercial uses exist to the east and west along Peterson Road, the Subject Property is isolated from those commercial and retail uses. The Subject Property is bound by residential uses on its east, south, and west sides, such that commercial development in this location would be out of character with the trend in development of the area. The proposed Map Amendment will provide a zoning and future land use designation consistent with the development and use of the adjacent properties and promote orderly development of the Subject Property and surrounding area.

i. The suitability of the subject property for uses permitted or specially permitted under its zoning classification.

The Subject Property is not suitable for uses permitted or specially permitted under its current zoning classification. The Subject Property is currently zoned R-1 in unincorporated Lake County and IB, R-1, and C-3 in the Village of Libertyville. The Future Land Use Map designations for the Subject Property are institutional, destination commercial corridor, and SFAR. While commercial uses exist to the east and west along Peterson Road, the Subject Property is isolated from those commercial and retail uses. The Subject Property is bound by residential uses on its east, south, and west sides, such that commercial development in this location would be out of character with the trend in development. The proposed Map Amendment will zone and designate the Subject Property in a manner consistent with the adjacent uses to promote redevelopment of the Subject Property with a mixed residential community complementary of the surrounding uses.

j. The availability of adequate ingress to and egress from the subject property and the extent to which traffic conditions in the immediate vicinity of the subject property would be affected by the proposed amendment.

There is adequate ingress to and egress from the Subject Property and the traffic conditions in the immediate vicinity will not be negatively affected by the proposed Map Amendment. The

Subject Property is located in close proximity to key transportation corridors like US Route 45, Milwaukee Avenue, and I-94, and will be easily accessible via two access points off Peterson Road. Both access points will align with the subdivision entrances to the south of Peterson Road to address and avoid conflicting traffic movements and provide safe and convenient access to and from the Development. The curvilinear roadway internal to the Development will be dedicated to the Village and is designed to circulate traffic efficiently and safely.

Further, a Traffic Impact Study dated December 20, 2024 (“**Traffic Study**”) was completed by Lindgren, O’Hara, Aboona, Inc. (“**KLOA**”) and included with the submittal material. The Traffic Study contemplates existing and proposed roadway conditions and concludes the traffic that will be generated by the proposed development will be sufficiently accommodated by the area roadway system.

- k. The availability of adequate utilities and essential public services to the subject property to accommodate the uses permitted or specially permitted under its present and proposed zoning classification.*

Adequate utilities and essential public services to the Subject Property are available to accommodate the uses permitted or specially permitted under its proposed zoning classification. Petitioner has coordinated with the Village’s engineering, public utility, and public works departments to coordinate easements and associated infrastructure to ensure adequate utilities and public services are available to adequately serve Greenway Chase. Utility service necessary for the service of the subdivision will be constructed by Petitioner consistent with the Village’s design standards.

- l. The length of time, if any, that the subject property has been vacant, considered in the context of the pace of development in the vicinity of the subject property.*

Not applicable. The Libertyville Manor has been situated on the Subject Property since

the 1980's.

m. The community need for the proposed amendment and for the uses and development it would allow.

There is a community need for the proposed Map Amendment and the uses and development it would allow. The Development will consist of two distinct housing lines – the Springs Series (single-family homes) and the Landings Series (age-restricted duplexes) to satisfy market demand within the Village. According to CMAP data, the Village had a net loss of population between 2000 and 2022, with the Village's total population shrinking by about one percent. In that same timeframe, the Village's population of 50+ year olds grew by 10%, and as of 2022, 58.8% of Village households were 1-2 persons. Despite the aging population and small household size of the Village, only 2.7% of the homes in the Village are duplexes. Further, the percentage of attached single-family homes in the Village's housing stock shrank over the past 15 years. At the same time, houses have been getting larger, with the share of 3-, 4-, and 5-bedroom homes all increasing, with the largest growth percentage being for homes with 5 or more bedrooms. Given the statistical data, there is a need for new housing in the Village, specifically for smaller single-family homes and duplexes as proposed for Greenway Chase.

The Springs Series homes will consist of sixty-four (64) single-family detached homes located on the west half of the Development. The Landings Series will consist of seventy (70) age-restricted duplex units (35 buildings) located along the east half of the Development.

The Springs Series single-family detached homes will consist of 30'-wide homes on a standard 41'-wide and 4,500 square foot lot. The Springs Series will offer five (5) model options ranging in size from 1,678 to 2,567 square feet, 3 to 5 bedrooms, 2.5 to 4 baths, and feature a two-car garage. The smaller lots, slightly smaller homes, and variety of upgrades and options will meet

the diverse needs of the community. The architecture of the Springs Series can be characterized as American and will be based on traditional styling that includes farmhouse, low country, heritage, and craftsman designs. While not new to Pulte, the Springs Series was only recently introduced to the Chicago region (being first approved for Naperville's "Polo Club" subdivision) to promote lower cost options targeted at the young professional demographic. The smaller 41' lots of the Springs Series reduce development costs by requiring less road, less watermain, less sewer, and less overall infrastructure per lot. This reduction in infrastructure also translates to savings on future maintenance expenses.

As a national leader in new home construction, Pulte has first-hand knowledge of the changing dynamics in the residential real estate market. Trends toward a more "experiential" lifestyle have affected buying patterns, particularly for younger homebuyers. Younger homebuyers are likely to avoid the maintenance and repair associated with older homes. Younger homebuyers generally prioritize amenitized common open space over large private yards. Young families tend to lead much busier lives and prioritize spending time and money on "experiential" activities rather than possessions – like a large private yard. These trends have been reflected in Pulte's residential subdivisions, particularly over the last decade, which emphasize common amenities over private yards. This shift from private space to community space lessens residents' maintenance obligations without sacrificing amenities. With less time and money spent on yard maintenance, Pulte has seen an increase in purchaser demand for housing that features more options associated with the livability of the home, including sunrooms, smart home upgrades, upgraded flooring, gourmet kitchens, and upgraded bathroom options.

In addition to the single-family detached homes in the Springs Series, Pulte also proposes its age-restricted Landings Series duplex homes to satisfy demand in the community for age-

restricted housing. In the age-restricted residential market, homebuyers are interested in smaller lots, which in turn reduce homeowners' association fees for maintenance and upkeep. Age-restricted communities have gained in residential market share as our population ages. Given the trend of demographics in Libertyville, there is a need to add housing product that is specifically targeted to the aging population.

The Landings Series duplexes will vary in size from 1,577 square feet to 2,697 square feet, 2 to 4 bedrooms, and 2 to 4 baths, and will feature a two-car garage. The architecture of the duplex homes will be consistent with market trends for age-restricted communities, with low maintenance exteriors and quality materials that will withstand the test of time. These age-restricted homes will be developed in accordance with the FHAA, including, but not limited to, the provisions of the HOPA.

**APPROVAL OF A SPECIAL USE FOR A PLANNED DEVELOPMENT AND
APPROVAL OF A PLANNED DEVELOPMENT CONCEPT PLAN**

Pursuant to Section 26-16-13.5(a) of Village Code, the approval of a Special Use Permit for a Planned Development is appropriate based on the following factors:

- a. *Code and plan purposes. The proposed use and development will be in harmony with the general and specific purposes for which this chapter was enacted and for which the regulations of the district in question were established and with the general purpose and intent of the official comprehensive plan.*

The proposed use and Development will be in harmony with the general and specific purposes for which the Village Code was enacted and with the general purpose and intent of the Comprehensive Plan. A stated purpose of the Village Code is to perpetuate the existing high quality residential character of the Village by preserving established neighborhoods while

encouraging new residential development. Planned development objectives include the creation of desirable environments, efficient use of land, and promotion of a creative approach to the use of land and related physical facilities resulting in better design and development. Further, a reoccurring theme of the Comprehensive Plan is the need for diversity in housing, particularly housing that targets empty-nesters and young professionals.

The proposed Development satisfies these objectives. Petitioner seeks Village approval of a mixed-use residential development consisting of sixty-four (64) single-family homes and seventy (70) age-restricted duplex units to be known as Greenway Chase. The proposed Development will provide the Village with additional housing options, will help support retail uses along Peterson Road, and will complement the surrounding residential subdivisions.

Greenway Chase has been strategically designed to prioritize large contiguous common open spaces. These common open spaces serve multiple purposes. To the north, the common open space minimizes the impact of the Development on natural features such as wetlands and wooded areas. To the east and west, common open spaces will provide a landscape buffer or transitional yard to minimize impact on surrounding development. To the south, the common open space will feature a rolling berm with new manicured landscaping that will both improve the character of the roadway frontage and serve as a buffer yard for the benefit of future residents. Within the development, common open space includes a newly dedicated park, a private amenity lawn for the age-restricted portion of the community, and a tree preservation zone. In total, forty-two percent (42%) of the area of the Subject Property will be preserved as common open space that is either dedicated to preservation, landscape enhancement, or recreational spaces designed to meet the needs of future residents. This represents 140% of the open space required under the Village's PUD standards.

There are five (5) wetland located on the Subject Property. These wetlands are primarily located along the northern perimeter of the Subject Property. The Development entirely avoids impact to jurisdictional U.S. Army Corps of Engineers' wetlands and minimizes impact to lower quality wetlands regulated by the Lake County Stormwater Management Commission (SMC). Two (2) stormwater basins are proposed within the northern section of the Subject Property, proximate to the preserved wetlands. The naturalized detention basins will both serve the stormwater needs of the Development and support the continued function of the wetlands. While the basins will be manmade, they are designed with wetland signatures and will be improved with native plantings, making them complimentary to the existing wetlands and open space to the north. Post development, the combination of the wetlands and associated native detention basins will improve drainage and water quality while creating new habitat for the benefit of the broader community.

The common open space provided by the Development will help to satisfy the recreational needs of future residents of the Development. The preserved wetlands and new native detention basins will provide passive open space for the benefit of residents. Two (2) separate amenity/park spaces are intended to provide more active recreational space within the Development. Within the age-restricted portion of the Development, a 1.41-acre private amenity space is planned. This space will feature a pedestrian pathway, yoga lawn, and outdoor seating/bench area. A separate 1.14-acre public park area has also been planned near the west entrance of the Development. This park space is anticipated to be improved with playground equipment and will be ultimately dedicated to the Village for use by the public. While the park spaces are compact, they are strategically located and appropriately sized to provide recreational opportunities for Greenway Chase residents.

The aforementioned natural features and proposed common open space guides the ultimate layout and design of the proposed Development. Of the 134 homes in the Development, ninety-three percent (93%) back up to open space! The adjacent common open space will make individual lots feel larger than their dimensions would otherwise suggest. As depicted on the Subdivision Plat, the single-family homes have been strategically placed at the west end of the Development adjacent to the Victoria Park Subdivision to the west. The duplexes have been placed in transitional locations between the proposed single-family homes and adjacent retail uses and Forest Creek townhomes. While Greenway Chase is bound by ComEd property to the north, the ComEd property consists primarily of sprawling open space that can be reasonably anticipated to remain in its current condition. Beyond the ComEd property are the Donnelley Prairies and Oaks Preserve, Casey Trail and Greenway, and Timber Creek Park areas, which total over 300 acres. A Village easement has been provided for a potential future pathway connection from Greenway Chase to the existing pathway network in the park and open space areas to the north and northwest.

Landscape treatments will be utilized to enhance the visual appeal of the community, including a mix of parkway trees, shade trees, and shrubs. The landscape design enhancements will include decorative plantings around the perimeter of the Development. Existing landscape buffers will be preserved where feasible and enhanced throughout the perimeter of the community to create the feel of a residential enclave and separate the single-family homes from the age-restricted duplexes. Extra care has been taken to provide additional landscape buffering between the Development and Peterson Road to the south. Within the community, Pulte plans foundation landscape plantings for all of the homes. A typical foundation planting plan is included within submittal materials, but is not intended to be a controlling PUD document. Pulte intends to depict the nature of the foundation or on-lot plantings that are proposed, but wants homeowners to have

ultimate flexibility to modify plantings as may be appropriate.

The Subject Property is located in close proximity to key transportation corridors like US Route 45, Milwaukee Avenue, and I-94, and will be easily accessible via two access points off Peterson Road. Both access points will align with the subdivision entrances to the south of Peterson Road to address and avoid conflicting traffic movements and provide safe and convenient access to and from the Development. The curvilinear roadway internal to the Development will provide a more interesting streetscape and ensure homes are not all identically situated along street frontage. The streets will be dedicated to the Village and have been designed to circulate traffic efficiently and safely.

As part of the Greenway Chase development, sidewalks throughout will provide interconnectivity within the Development. The future residents will benefit from the development's proximity to shopping districts, recreational amenities, and employment centers. Within a 3-mile radius, there are a large variety of retail establishments, groceries, restaurants, entertainment opportunities, and recreational amenities. Nearby employment corridors offer future residents the additional feature of conveniently located employment opportunities.

The Development will consist of two distinct housing lines – the “Springs Series” (single-family homes) and the “Landings Series” (age-restricted duplexes) to satisfy market demand within the Village. The Springs Series homes will consist of sixty-four (64) single-family detached homes located on the west half of the Development. The Landings Series will consist of seventy (70) age-restricted duplex units (35 buildings) located along the east half of the Development.

The proposed mixed-use residential community will be an appropriate transitional use along a principal arterial roadway corridor like Peterson Road. While the area further east along Peterson Road towards Milwaukee Avenue is generally retail/commercial, the area surrounding

Greenway Chase hosts a variety of residential housing developments. Greenway Chase will complement these subdivisions, which include the Forest Creek townhomes to the east and age-restricted townhomes in the Victoria Park subdivision to the west. In addition, the proposed homes will help support the existing retail and commercial uses to the east, which are easily accessible via Peterson Road. Given the proposed Development's location and accessibility to nearby commercial uses and local parks, the Subject Property presents an excellent housing opportunity that is suitable for a variety of homebuyers, including some of the fastest growing housing segments of our population – young professionals and the 55+ demographic.

b. Adverse impact. The proposed use and development will not have a substantial adverse effect upon adjacent property, the character of the area, or the public health, safety, and general welfare.

The proposed use and Development will not have an adverse effect upon adjacent property, the character of the area, or the public health, safety, and general welfare. The proposed Development will complement the surrounding residential subdivisions, including the Forest Creek townhomes to the east and age-restricted townhomes in the Victoria Park subdivision to the west.

Libertyville Manor was constructed in the 1980's but has since become obsolete. Compared to this underutilized use of the Subject Property, Petitioner's proposed redevelopment of the Subject Property with a mixed residential use will provide an increase in the benefit to the public health, safety, and welfare of the community. The development of Greenway Chase will improve the Village's housing stock, serve under-represented demographics, and create new revenue streams through property, utility, and sales taxes.

c. Interference with surrounding development. *The proposed use and development will be constructed, arranged, and operated so as not to dominate the immediate vicinity or to interfere with the use and development of neighboring property in accordance with the applicable district regulations.*

The proposed use and development will be constructed, arranged, and operated so as not to dominate the immediate vicinity or to interfere with the use and development of neighboring properties. The proposed Development is consistent with the uses of the adjacent properties, which are largely residential. The Forest Creek townhome community is situated east of the subject property, detached single-family homes in the Wineberry subdivision are located to the south, and townhomes in the age-restricted Victoria Park 55+ townhome community are located to the west. The proposed Greenway Chase Subdivision will complement these developments and will provide a logical transition between uses. Extra care has been taken to provide perimeter vegetation and landscaping for separation between Greenway Chase and the adjacent uses, and significant landscape buffer is provided for between the development and Peterson Road.

d. Adequate public facilities. *The proposed use and development will be served adequately by essential public facilities and services such as streets, public utilities, drainage structures, police and fire protection, refuse disposal, parks, libraries, and schools, or the applicant will provide adequately for such services.*

The proposed Development will be served adequately by essential public facilities and services such as streets, public utilities, drainage structures, police and fire protection, refuse disposal, parks, libraries, and schools, or the applicant will provide adequately for such services.

Public utility capacity is available to the Subject Property in sufficient volume and all utility service necessary for the service of the subdivision will be constructed by Petitioner consistent with the Village's design standards. A private trash service will be utilized and tenants will keep their trash bins in their garage and bring them out on garbage day.

- e. *Traffic congestion. The proposed use and development will not cause undue traffic congestion nor draw significant amounts of traffic through local streets and will, when required by the board of trustees, incorporate appropriate vehicular trip reduction features such as, but not limited to, pedestrian/bicycle linkages within and between land uses, the use of traffic mitigation plans, and the incorporation of elements to encourage and facilitate the use of public transportation.*

The proposed use and development will not cause undue traffic congestion nor draw significant amounts of traffic through local streets. The Subject Property is located in close proximity to key transportation corridors like US Route 45, Milwaukee Avenue, and I-94, and will be easily accessible via two access points off Peterson Road. Both access points will align with the subdivision entrances to the south of Peterson Road to address and avoid conflicting traffic movements and provide safe and convenient access to and from the Development. The curvilinear roadway internal to the Development will provide a more interesting streetscape and ensure homes are not all identically situated along street frontage. The streets will be dedicated to the Village and have been designed to circulate traffic efficiently and safely.

Further, a Traffic Study was completed by KLOA and was included with the submittal material. The Traffic Study contemplates existing and proposed roadway conditions and concludes the traffic that will be generated by the proposed development will be sufficiently

accommodated by the area roadway system.

- f. Destruction of significant features. The proposed use and development will not result in the destruction, loss, or damage of any natural, scenic, or historic feature of significant importance.*

The proposed use and development will not result in the destruction, loss, or damage of any natural, scenic, or historic feature of significant importance. The existing Libertyville Manor has been situated on the Subject Property since the 1980's. Petitioner intends to demolish the facility and redevelop the Subject Property with a mixed residential use. No natural, scenic, or historic feature of significant importance are located on the Subject Property. Trees will be preserved where feasible and existing wetlands will be avoided to minimize any detrimental environmental impact. This has been accomplished by emphasizing preservation and creation of open space to be shared by future residents over the size of individual private yards. Approximately 42% of the proposed Development's acreage is dedicated to open space. While five (5) wetland sites are currently located on the Subject Property, the Development entirely avoids jurisdictional U.S. Army Corps of Engineers' wetlands and minimizes impact to those lesser quality wetlands regulated by the Lake County Stormwater Management Commission (SMC).

- g. Compliance with standards. The proposed use and development complies with all additional standards imposed on it by the particular provision of this chapter authorizing such use.*

See below.

**ADDITIONAL STANDARDS FOR PLANNED
DEVELOPMENT APPROVAL**

Pursuant to Section 26-16-13.5(b) of Village Code, the approval of a Special Use Permit for a Planned Development is appropriate based on the following additional standards:

- h. Unified ownership required.* The proposed Development meets the ownership and control requirements. Petitioner is the contract purchaser of the Subject Property.
- i. Minimum area.* The configuration of the PUD provides for a cohesive development and does not lend itself to a separate calculation of lot area.
- j. Covenants and restrictions to be enforceable by Village.* All covenants, deed restrictions, easements, and similar restrictions to be recorded in connection with the planned development shall provide that they may not be modified, removed, or released without the express consent of the board of trustees and that they may be enforced by the village as well as by future landowners within the proposed development.
- k. Public open space and contributions.* Per the Village's request, a 1.14-acre public park area has been planned near the west entrance of the Development. This park space is anticipated to be improved with playground equipment and will be ultimately dedicated to the Village for use by the public.
- l. Common open space.*
 - (i) Amount, location, and use.* Two (2) separate amenity/park spaces are planned for the Development. The first consists of a 1.41-acre private amenity space for the age-restricted portion of the Development. This space will feature a pedestrian pathway, yoga lawn, and outdoor seating/bench area. A separate 1.14-acre public park area has also been planned for near the west entrance of

the Development. This park space is anticipated to be improved with playground equipment and will be ultimately dedicated to the Libertyville Park District for use by the public. While the park spaces are relatively compact, they are strategically located within distinct neighborhoods that comprise the Development to provide open lawn space, creating recreational opportunities for Greenway Chase residents.

(ii) *Preservation.* Adequate safeguards, including recorded covenants or dedication of development rights, shall be provided to prevent the subsequent use of common open space for any use, structure, improvement, or development other than that shown on the approved final plan.

(iii) *Ownership and maintenance.* Common open space shall be owned and maintained by a homeowners' association to be formed for the Development.

(iv) *Property owners' association.* Petitioner agrees the requirements set forth in Section 26-16-13.5(b)(5)d of the Village Code shall be satisfied.

(v) *Landscaping and perimeter treatment.* Landscape treatments will be utilized to enhance the visual appeal of the community, including a mix of parkway trees, shade trees, and shrubs. The landscape design enhancements will include decorative plantings around the perimeter of the Development as buffer between the uses. Existing landscape buffers will be preserved where feasible and enhanced throughout the perimeter of the community to create the feel of a residential enclave separate from the adjacent subdivisions and Peterson Road.

(vi) *Building spacing.* No part of any building shall be closer to any other building by less than ten (10) feet.

(vii) *Private streets.* All streets will be made public and dedicated to the Village.

(viii) *Sidewalks.* With the exception of the deviation request to waive the requirement for a sidewalk along Peterson Road, Village Code compliant sidewalks will be provided throughout the Development to provide interconnectivity within the neighborhood.

(ix) *Utilities.* All utilities will be installed underground.

m. *Vehicular trip reduction features.* Two (2) park/amenity sites will be located within the Development so residents do not have to leave the neighborhood for recreational opportunities. An easement will be provided for the potential future connection of Greenway Chase to the pathway network to the northwest of the Subject Property. For commuting residents, three train stations are located in the Village. The Milwaukee District North line serves both the downtown station on Milwaukee Avenue and the Prairie Crossing station on the west side of the Village, offering service north to Fox Lake and south to Chicago's Union Station. The North Central line serves a second Prairie Crossing station, offering service north to Antioch and south to Union Station, including a stop at O'Hare International Airport.

APPROVAL OF A SPECIAL USE TO PERMIT SENIOR CITIZEN HOUSING IN THE R-7 ZONING DISTRICT

Pursuant to Sections 26-4-8.3 of Village Code, a Special Use is required to permit Senior Citizen Housing in the R-7 zoning district. Pursuant to Section 26-16-9, the approval of a Special Use Permit to permit Senior Citizen Housing in the R-7 zoning district is appropriate based on the following factors:

a. *Code and plan purposes.* The proposed use and development will be in harmony with

the general and specific purposes for which this chapter was enacted and for which the regulations of the district in question were established and with the general purpose and intent of the official comprehensive plan.

The proposed use and Development will be in harmony with the general and specific purposes for which the Village Code was enacted and with the general purpose and intent of the Comprehensive Plan. A stated purpose of the Village Code is to perpetuate the existing high quality residential character of the Village by preserving established neighborhoods while encouraging new residential development. Further, a reoccurring theme of the Comprehensive Plan is the need for diversity in housing, particularly housing that targets empty-nesters and young professionals.

Pulte's age-restricted Landings Series duplex homes have been included as part of Greenway Chase to satisfy demand in the community for age-restricted housing. In the age-restricted residential market, homebuyers are interested in smaller lots, which in turn reduce homeowners' association fees for maintenance and upkeep. These active 55+ homeowners lead active, busy lives, choose to spend time and money on other activities, and have little interest in doing yardwork. Age-restricted communities have gained in residential market share as our population ages, and minimum lot sizes have been steadily decreasing as previously mentioned.

The Landings Series duplexes will vary in size from 1,577 square feet to 2,697 square feet, 2 to 4 bedrooms, and 2 to 4 baths, and will feature a two-car garage. The architecture of the duplex homes will be consistent with market trends for age-restricted communities, with low maintenance exteriors and quality materials that will withstand the test of time. These age-restricted homes will be developed in accordance with the FHAA, including, but not limited to, the provisions of the HOPA.

As mentioned, the duplexes will include a separate, private outdoor amenity space consisting of approximately 1.41 acres that will feature a yoga lawn, pedestrian pathway, and seating area with benches to provide residents an outdoor recreational opportunity within the age-restricted community. A homeowners' association will be established for Greenway Chase with a separate committee for the age-restricted duplex pod. The HOA will perform maintenance on the amenity spaces, open areas, and green space areas of the Greenway Chase neighborhood. Snow removal, lawn mowing, landscape maintenance, and exterior maintenance will be provided for the age-restricted duplexes.

b. Adverse impact. The proposed use and development will not have a substantial adverse effect upon adjacent property, the character of the area, or the public health, safety, and general welfare.

The proposed use and Development will not have an adverse effect upon adjacent property, the character of the area, or the public health, safety, and general welfare. Libertyville Manor was constructed in the early 1980's. Since then, its use has become obsolete, providing Petitioner with the opportunity to purchase and redevelop the Subject Property with its highest and best use as a mixed residential development. The proposed Development and use will complement the surrounding residential subdivisions, including the Forest Creek townhomes to the east and townhomes in the Victoria Park subdivision to the west. Like the proposed age-restricted duplexes, the Victoria Park townhomes are also restricted to the 55+ demographic.

In addition, perimeter vegetation will be preserved and added to provide separation between Greenway Chase and the adjacent uses. Tree preservation zones will also be utilized to provide additional buffer between the single-family and duplex homes internal to the community.

c. Interference with surrounding development. The proposed use and development will be constructed, arranged, and operated so as not to dominate the immediate vicinity or to interfere with the use and development of neighboring property in accordance with the applicable district regulations.

The proposed use and development will be constructed, arranged, and operated so as not to dominate the immediate vicinity or to interfere with the use and development of neighboring properties. The proposed Development is consistent with the uses of the adjacent properties, which are largely residential. The Forest Creek townhome community is situated east of the subject property, detached single-family homes in the Wineberry subdivision are located to the south, and townhomes in the age-restricted Victoria Park 55+ townhome community are located to the west. The proposed Greenway Chase Subdivision will complement these developments and will provide a logical transition between uses. Perimeter vegetation will be preserved and added to provide separation between Greenway Chase and the adjacent uses. Tree preservation zones will also be utilized to provide additional buffer between the single-family and duplex homes internal to the community. In addition, the proposed Development will be an appropriate transitional use along a principal arterial roadway corridor like Peterson Road. The uses along Peterson Road become more retail and commercial in nature the further east you travel, which uses the proposed Development will help support.

d. Adequate public facilities. The proposed use and development will be served adequately by essential public facilities and services such as streets, public utilities, drainage structures, police and fire protection, refuse disposal, parks, libraries, and

schools, or the applicant will provide adequately for such services.

The proposed Development will be served adequately by essential public facilities and services such as streets, public utilities, drainage structures, police and fire protection, refuse disposal, parks, libraries, and schools, or the applicant will provide adequately for such services. Public utility capacity is available to the Subject Property in sufficient volume and all utility service necessary for the service of the subdivision will be constructed by Petitioner consistent with the Village's design standards. A private trash service will be utilized and tenants will keep their trash bins in their garage and bring them out on garbage day.

e. Traffic congestion. The proposed use and development will not cause undue traffic congestion nor draw significant amounts of traffic through local streets and will, when required by the board of trustees, incorporate appropriate vehicular trip reduction features such as, but not limited to, pedestrian/bicycle linkages within and between land uses, the use of traffic mitigation plans, and the incorporation of elements to encourage and facilitate the use of public transportation.

The Subject Property is located in close proximity to key transportation corridors like US Route 45, Milwaukee Avenue, and I-94, and will be easily accessible via two access points off Peterson Road. Both access points will align with the subdivision entrances to the south of Peterson Road to address and avoid conflicting traffic movements and provide safe and convenient access to and from the Development. The curvilinear roadway internal to the Development will provide a more interesting streetscape and ensure homes are not all identically situated along street frontage. The streets will be dedicated to the Village and have been designed to circulate traffic efficiently and safely.

Further, a Traffic Study was completed by KLOA and was included with the submittal material. The Traffic Study contemplates existing and proposed roadway conditions and concludes the traffic that will be generated by the proposed development will be sufficiently accommodated by the area roadway system.

f. Destruction of significant features. The proposed use and development will not result in the destruction, loss, or damage of any natural, scenic, or historic feature of significant importance.

The proposed use and development will not result in the destruction, loss, or damage of any natural, scenic, or historic feature of significant importance. The existing Libertyville Manor has been situated on the Subject Property since the 1980's. Petitioner intends to demolish the facility and redevelop the Subject Property with a mixed residential use. No natural, scenic, or historic feature of significant importance are located on the Subject Property. Trees will be preserved where feasible and existing wetlands will be avoided to minimize any detrimental environmental impact. This has been accomplished by emphasizing preservation and creation of open space to be shared by future residents over the size of individual private yards. Approximately 42% of the proposed Development's acreage is dedicated to open space. While five (5) wetland sites are currently located on the Subject Property, the Development avoids jurisdictional U.S. Army Corps of Engineers' wetlands and minimizes impact to those lesser quality wetlands regulated by the Lake County Stormwater Management Commission (SMC).

g. Compliance with standards. The proposed use and development complies with all additional standards imposed on it by the particular provision of this chapter

authorizing such use.

Not applicable. No additional standards are required for a Special Use Permit for Senior Housing.

APPROVAL OF A PRELIMINARY PLAT OF SUBDIVISION

Petitioner seeks approval of the Preliminary Plat of Subdivision, a copy of which has been submitted herewith, to subdivide the Property to allow the construction of sixty-four (64) single-family homes and seventy (70) age-restricted duplex units. As the proposed Preliminary Plat of Subdivision is compliant with Village Code, Petitioner requests the Preliminary Plat of Subdivision be approved.

APPROVAL OF BULK REGULATION DEVIATIONS

Pursuant to Section 26-16-13.7 of Village Code, the Board of Trustees has the authority, to change, alter, vary, modify or waive any provisions of Village Zoning Code or Village Subdivision Code as they apply to an approved planned development. Approval of the requested deviations is appropriate based on the following factors:

- a. Will achieve the purposes for which planned developments may be approved pursuant to subsection 26-16-13.1;*

The requested deviations will achieve the purposes for which planned developments may be approved. Reoccurring themes highlighted in Village Code Section 26-16-13.1 include creation of a more desirable environment, efficient use of land resulting in smaller networks of utilities and streets to lower development and housing costs, the promotion of a creative approach to the use of land and related physical facilities resulting in better design and development, including aesthetic

amenities, and preservation of conservation areas and open space.

As more specifically set forth on Exhibit B, Petitioner requests deviations to the bulk regulations for the R-7 zoning district and planned developments. These include deviations to lot area, lot width, building setbacks, building coverage, lot coverage, and minimum perimeter landscaping requirements. A deviation has also been requested to waive the requirement for installation of a sidewalk along Peterson Road. The requested deviations will support the development of Greenway Chase in furtherance of the above-listed objectives.

The proposed Development promotes the creative use and development of the Subject Property. Generally speaking, Petitioner's desire to preserve the natural features and provide open space and amenities within the Development guides the layout and design of Greenway Chase. Greenway Chase has been strategically designed to create larger contiguous spaces to be shared by the community that maximize open space and minimize impact to the Subject Property's wetlands, natural features, and conservation areas. This has been accomplished by emphasizing preservation and creation of open space over the size of individual private yards. The result is smaller lots, but smaller lots promote reduced maintenance, preservation and creation of open space, and creation of and park/amenity sites. The smaller lot sizes and associated deviations also reduce development costs by requiring less road, less watermain, less sewer, and less overall infrastructure per lot. This reduction in infrastructure also translates to savings on future maintenance expenses.

While Petitioner also requests a deviation to the perimeter landscaping requirements, there is ample open space and landscaping planned for the Development to provide the necessary buffering within the community and between adjacent uses. The adjacent uses to the east and west are townhomes and compatible with the proposed Development, such that there is no need to

substantially landscape between similar uses. Regardless, perimeter vegetation and landscaping will be preserved where possible and additional landscape treatments will be provided along the perimeter of the community. While Village Code requires 20' of perimeter landscaping, only 19' was possible along portions of the west perimeter due to site constraints. Petitioner has taken extra care to include additional landscaping treatments at this location to ensure separation and privacy are provided between Greenway Chase and the subdivision to the west.

Petitioner has also requested a deviation to the requirement that a sidewalk be installed along Peterson Road. While sidewalks will be installed interior to the Development, there is no need for a sidewalk along Peterson Road. No sidewalk currently exists or is planned for along Peterson Road in the general vicinity of the Subject Property, such that even if Petitioner were to install a sidewalk on the Subject Property, it would have nothing to tie into and would provide no additional benefit to residents of the Development. In lieu of a sidewalk along Peterson Road, Petitioner has provided additional landscaping along Peterson Road to provide an attractive buffer and facade along the roadway. Sidewalks and amenity space have been included within the Development for recreational opportunities. In addition, a Village easement has been provided for a potential future pathway connection from Greenway Chase to the existing pathway network in the park and open space areas to the north for an additional recreational amenity.

b. Will not violate the general purposes, goals, and objectives of this chapter and the official comprehensive plan; and

The proposed Development will not violate the general purposes, goals, and objectives of this chapter and the official comprehensive plan. A stated purpose of the Village Code is to perpetuate the existing high quality residential character of the Village by preserving established neighborhoods while encouraging new residential development. Further, a reoccurring theme of

the Comprehensive Plan is the need for diversity in housing, particularly housing that targets empty-nesters and young professionals. The proposed Development and associated deviations will help satisfy these objectives and generate creation of desirable environments, efficient use of land, and promote a creative approach to the use of land and related physical facilities resulting in better design and development. With both single-family detached and age-restricted offerings, the proposed Development will provide the Village with additional housing options that target multiple demographics, including young professionals and empty-nesters. Greenway Chase will complement the surrounding residential subdivisions, including the Forest Creek townhomes to the east and age-restricted townhomes in the Victoria Park subdivision to the west. Aside from improving the Village's housing stock, Greenway Chase will create new revenue streams through property, utility, and sales taxes.

c. Will result in a development providing compensating amenities to the village.

The proposed Development and deviations will provide compensating amenities to the Village. The proposed Development will provide the Village with additional housing options, will help support retail uses along Peterson Road, and will complement the surrounding residential subdivisions, including the Forest Creek townhomes to the east and age-restricted townhomes in the Victoria Park subdivision to the west.

In total, forty-two percent (42%) of the area of the Subject Property will be preserved as common open space, either dedicated to preservation, landscape enhancement, or recreational spaces designed to meet the needs of future residents. This represents 140% of the open space required under the Village's PUD standards. The result is that of the 134 homes in the Development, ninety-three percent (93%) back up to open space!

In addition, two (2) separate amenity/park spaces are planned for the Development. The

first consists of a 1.41-acre private amenity space for the age-restricted portion of the Development. This space will feature a pedestrian pathway, yoga lawn, and outdoor seating/bench area. A separate 1.14-acre public park area has also been planned for near the west entrance of the Development. This park space is anticipated to be improved with playground equipment and will be ultimately dedicated to the Libertyville Park District for use by the public. While the park spaces are relatively compact, they are strategically located within distinct neighborhoods that comprise the Development to provide open lawn space, creating recreational opportunities for Greenway Chase residents.

APPROVAL OF DESIGN STANDARD DEVIATIONS

Pursuant to Section 26-16-13.7 of Village Code, the Board of Trustees has the authority, to change, alter, vary, modify or waive any provisions of Village Zoning Code or Village Subdivision Code as they apply to an approved planned development. Approval of the requested deviations is appropriate based on the following factors:

- a. Will achieve the purposes for which planned developments may be approved pursuant to subsection 26-16-13.1;*

The requested deviations will achieve the purpose for which planned developments may be approved. A core objective of the Village is the creation of a more desirable environment than would be possible through strict application of other Village land use regulations. To achieve this objective, Petitioner requests deviations to the Village's design standards for the R-7 zoning district. Specifically, Petitioner is requesting deviations to Village Code provisions related to anti-monotony standards, size and location of garage doors, and individual dormer width. The proposed Greenway Chase includes housing designed to meet unique market needs within the community. It has been Petitioner's experience in recent years that in the residential market, homebuyers are

interested in smaller lots, reducing the time and money spent on maintenance and upkeep of larger lots. Families lead much busier lives and choose to spend time and money on other activities and products rather than the upkeep of a large yard. Over the last ten (10) years, Pulte's residential subdivisions have emphasized common amenities over private yards.

The smaller lots provided in Greenway Chase will offer modern lifestyle layouts and amenities in a compact footprint, allowing a broader spectrum of buyers access to quality new construction in Libertyville. To cost-effectively deliver these homes, thereby reducing sale prices, deviations to the architectural design standards are required. The proposed lots will be narrower than typical developments. Given the width of the Springs Series homes, there is limited flexibility with respect to the configuration of the ground floor level. The garage door and front entrance become dominant features by means of necessity. With this, there is little leeway to generate the variety of forms and elements required by Village Code to the extent described.

In lieu of the Village Code requirements, Pulte has provided its own anti-monotony standards for the Village's approval. The proposed anti-monotony standards provide restrictions for both location of different elevations/configurations and colors to provide the community and Village with ample architectural diversity within the Development. A deviation to permit individual dormers with a width exceeding eight (8) feet has also been included to provide additional architectural variety within the community.

b. Will not violate the general purposes, goals, and objectives of this chapter and the official comprehensive plan; and

The requested deviations to the design standards will not violate the general purposes, goals, and objectives of Village Code and the official comprehensive plan. A stated objective of planned developments is the combination and coordination of architectural styles, building forms,

and building relationships. Further, the Comprehensive Plan supports diversity of housing typologies at various price points to accommodate various demographics, specifically empty-nesters and young professionals. The proposed Development and associated deviations support and work to promote these objectives within the Village.

While design standard deviations have been requested, the proposed single-family elevations provide ample diversity and housing options. The Springs Series will offer five (5) model options ranging in size from 1,678 to 2,567 square feet, 3 to 5 bedrooms, 2.5 to 4 baths, and feature a two-car garage to meet the diverse needs of the community. The architecture of the Springs Series can be characterized as modern American and will be based on traditional styling that includes farmhouse, low country, heritage, and craftsman designs to bring a new and refreshing housing option to the Village. As noted, in lieu of the Village Code requirements, Pulte has provided its own anti-monotony standards for the Village's approval. The proposed anti-monotony standards provide restrictions for both location of different elevations/configurations and home colors to provide the community and Village with ample architectural diversity within the community. A deviation to permit individual dormers with a width exceeding eight (8) feet has also been included to provide additional architectural variety within the community.

In addition, a common theme in the Comprehensive Plan is preservation and protection of existing neighborhoods. Notably, Greenway Chase has no connection or real relationship to either the Victoria Park or Forest Creek subdivisions. Given the nature of access and connectivity of Greenway Chase to the adjacent subdivisions, the requested deviations will not impair the adjacent neighborhoods and underlying purpose and intent of the design standards.

c. Will result in a development providing compensating amenities to the village.

The proposed deviations will result in a development providing compensating amenities to

the Village. The proposed architectural and design standard deviations will not undercut the overall benefit of the proposed to the Village. Pulte’s proposed anti-monotony standards have promoted successful developments across the Chicagoland area, providing communities with diversity in housing while appealing to a wide range of prospective home buyers. While Petitioner could build large homes on large lots consistent with the trend of teardown activity in the Village, the 2030 Comprehensive Plan specifically identifies the need for diversity in housing product. The development of Greenway Chase will compensate for the deviations through satisfaction of the Village’s goal toward diversity of housing stock.

WHEREFORE, by reason of the foregoing, the undersigned Petitioner, Pulte Home Company, LLC, respectfully petitions the Village of Libertyville to:

- (i) Annex the Annexation Property to the Village pursuant to a separate petition submitted by the owners of record;
- (ii) Upon annexation of the Annexation Property, rezone the entire Subject Property to the Village’s R-7 “Single-Family Attached Residential District”;
- (iii) Amend the Comprehensive Plan Future Land Use Map to “Single-Family Attached Residential”;
- (iv) Approve a Special Use for a Planned Development for the development of Greenway Chase, a one hundred thirty-four (134) unit mixed-residential development with associated deviations as more specifically detailed on Exhibit B and depicted on the submitted development plans;
- (v) Approve a Planned Development Concept Plan;
- (vi) Approve a Special Use to permit Senior Citizen Housing in the R-7 zoning district;

(vii) Approve a Preliminary Plat of Subdivision; and

(viii) Approve such other relief from the Village Code as may be deemed necessary and appropriate to develop the Subject Property consistent with the plans submitted herewith.

RESPECTFULLY SUBMITTED this 28th day of February, 2025.

PETITIONER:
PULTE HOME COMPANY, LLC,
a Michigan limited liability company



Rosanova & Whitaker, Ltd.
Attorneys for the Petitioner

EXHIBIT A
LEGAL DESCRIPTION – SUBJECT PROPERTY

PARCEL 1:

THAT PART OF THE NORTHEAST QUARTER OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE SOUTH LINE OF AND 6.82 CHAINS WEST FROM THE SOUTHEAST CORNER OF SAID NORTHEAST QUARTER; THENCE WEST ALONG SAID SOUTH LINE, 2229.68 FEET TO THE SOUTHWEST CORNER OF SAID NORTHEAST QUARTER; THENCE NORTH ALONG THE WEST LINE OF SAID NORTHEAST QUARTER, 1410.30 FEET; THENCE EAST 2230.38 FEET TO A POINT WHICH IS 6.82 CHAINS WEST FROM THE EAST LINE OF SAID NORTHEAST QUARTER AND 1405.2 FEET NORTH FROM THE SOUTH LINE OF SAID NORTHEAST QUARTER; THENCE SOUTH TO THE PLACE OF BEGINNING (EXCEPTING THEREFROM THE FOLLOWING: (A) THE EAST 1920.68 FEET THEREOF; (B) THAT PART THEREOF, IF ANY, FALLING IN BROOKHILL PARK, A SUBDIVISION OF PARTS OF SECTIONS 8 AND 9, TOWNSHIP AND RANGE AFORESAID, RECORDED APRIL 24, 1925, AS DOCUMENT 256105, IN BOOK "M" OF PLATS, PAGE 100; AND ALSO (C) EXCEPTING THEREFROM THAT PART CONVEYED BY WARRANTY DEED DATED SEPTEMBER 25, 1967, TO THE STATE OF ILLINOIS FOR THE USE OF DEPARTMENT OF PUBLIC WORKS AND BUILDINGS DESCRIBED AS FOLLOWS: PART OF THE NORTHEAST QUARTER OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, BOUNDED AND FULLY DESCRIBED AS FOLLOWS: BEGINNING AT THE SOUTHWEST CORNER OF SAID NORTHEAST QUARTER; THENCE EASTERLY ALONG THE SOUTH LINE OF SAID NORTHEAST QUARTER, 309.00 FEET; THENCE NORTHERLY PARALLEL TO THE EAST LINE OF SAID NORTHEAST QUARTER, 74.81 FEET; THENCE WESTERLY TOWARD A POINT ON THE WEST LINE OF SAID NORTHEAST QUARTER, SAID POINT BEING 75.00 FEET NORTHERLY FROM THE SOUTHWEST CORNER OF SAID NORTHEAST QUARTER, 217.17 FEET TO A POINT OF CURVE TOWARD THE SOUTH; THENCE ON SAID CURVE TO THE SOUTH, THE RADIUS OF WHICH IS 17,263.74 FEET, 91.83 FEET TO A POINT ON THE WEST LINE OF SAID NORTHEAST QUARTER, THIS POINT BEING 74.76 FEET NORTHERLY FROM THE SOUTHWEST CORNER OF SAID NORTHEAST QUARTER; THENCE SOUTHERLY ON THE WEST LINE OF SAID NORTHEAST QUARTER, 74.76 FEET TO SAID POINT OF BEGINNING), IN LAKE COUNTY, ILLINOIS,

PARCEL 2:

THE EAST 462.0 FEET OF THE SOUTH 1410.30 FEET (AS MEASURED ALONG THE EAST AND SOUTH LINES, RESPECTIVELY) OF THE NORTHWEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, (EXCEPT THE SOUTH 1079.60 FEET THEREOF), ALL IN, LAKE COUNTY, ILLINOIS. AND THE WEST 420.5 FEET OF THE EAST 882.50 FEET OF THE SOUTH 1410.30 FEET (AS MEASURED ALONG THE EAST AND SOUTH LINES, RESPECTIVELY) OF THE

NORTH WEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN (EXCEPT THAT PART DESCRIBED AS FOLLOWS, TO-WIT: BEGINNING AT THE SOUTH EAST CORNER OF THE SAID WEST 420.5 FEET OF THE EAST 882.50 FEET OF THE SOUTH 1410.30 FEET OF THE NORTH WEST 1/4 OF SECTION 8; THENCE NORTHERLY ALONG THE EAST LINE OF THE SAID WEST 420.5 FEET OF THE EAST 882.50 FEET OF THE SOUTH 1410.30 FEET OF THE NORTH WEST 1/4 OF SECTION 8 FOR A DISTANCE OF 898.19 FEET; THENCE WESTERLY ALONG A LINE TO THE WEST LINE OF THE SAID EAST 882.50 FEET OF THE NORTH WEST 1/4 OF SECTION 8 TO A POINT WHICH IS 896.80 FEET NORTHERLY OF THE SOUTH LINE OF THE SAID NORTH WEST 1/4 OF SECTION 8 (AS MEASURED ALONG THE SAID WEST LINE OF THE EAST 882.50 FEET); THENCE SOUTHERLY ALONG THE SAID WEST LINE OF THE EAST 882.50 FEET OF THE SOUTH 1410.30 FEET OF THE NORTH WEST 1/4 OF SECTION 8 FOR A DISTANCE OF 896.80 FEET TO THE SAID SOUTH LINE OF THE NORTH WEST 1/4 OF SECTION 8; THENCE EASTERLY ALONG THE SAID SOUTH LINE OF THE NORTH WEST 1/4 OF SECTION 8 FOR A DISTANCE OF 420.5 FEET TO THE POINT OF BEGINNING OF THIS EXCEPTION), IN LAKE COUNTY, ILLINOIS.

PARCEL 3:

THAT PART OF THE EAST 1/2 OF THE NORTH WEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS BEGINNING AT A POINT ON THE WEST LINE OF THE EAST 1/2 OF SAID NORTH WEST 1/4 WHICH IS 1755 FEET SOUTH OF THE NORTH WEST CORNER THEREOF; THENCE EAST ALONG THE SOUTH LINE OF THE NORTH 1755 FEET OF SAID EAST 1/2 OF THE NORTH WEST 1/4, A DISTANCE OF 453.98 FEET TO THE WEST LINE OF THE EAST 882.50 FEET OF THE NORTH WEST 1/4 OF SAID SECTION 8; THENCE NORTH ALONG SAID WEST LINE OF THE EAST 882.50 FEET, A DISTANCE OF 512.66 FEET TO THE NORTH LINE OF THE SOUTH 1410.30 FEET OF THE NORTH WEST 1/4 OF SAID SECTION 8; THENCE WEST ALONG SAID NORTH LINE OF THE SOUTH 1410.30 FEET OF THE NORTH WEST 1/4, 454.51 FEET TO THE WEST LINE OF THE EAST 1/2 OF THE NORTH WEST 1/4 OF SAID SECTION 8; THENCE SOUTH ALONG SAID WEST LINE OF THE EAST 1/2 OF SAID NORTH WEST 1/4, A DISTANCE OF 513.26 FEET TO THE POINT OF BEGINNING, IN LAKE COUNTY, ILLINOIS.

PARCEL 4:

THE WEST 432 FEET OF THE EAST 462 FEET OF THE SOUTH 701 FEET OF THE NORTHWEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, EXCEPT THEREFROM THAT PART OF THE NORTHWEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, BOUNDED AND FULLY DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT ON THE EAST LINE OF SAID NORTHWEST 1/4 SAID POINT BEING 74.76 FEET NORTH OF THE SOUTHEAST CORNER THEREOF; THENCE SOUTH ON THE EAST LINE OF SAID NORTHWEST 1/4, 74.76 FEET TO THE SOUTHEAST

CORNER THEREOF; THENCE WESTERLY ON THE SOUTH LINE OF SAID NORTHWEST 1/4, 462.00 FEET; THENCE NORTHERLY PARALLEL WITH THE EAST LINE OF SAID NORTHWEST 1/4, 66.61 FEET; THENCE EASTERLY ON A CURVE VARYING TOWARD THE SOUTH. THE RADIUS OF WHICH IS 17,263.74 FEET TO SAID POINT OF BEGINNING), IN LAKE COUNTY, ILLINOIS.

PARCEL 5:

THE WEST 420.5 FEET OF THE EAST 882.5 FEET OF THAT PART OF THE EAST 1/2 OF THE NORTH WEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING SOUTH OF THE NORTH 1755 FEET THEREOF, (EXCEPT THAT PART THEREOF DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE EAST LINE OF SAID PARCEL, 66.61 FEET NORTHERLY FROM THE SOUTH LINE OF SAID NORTH WEST 1/4; THENCE SOUTHERLY ALONG SAID EAST LINE, 66.61 FEET TO THE SOUTH LINE OF SAID NORTH WEST 1/4; THENCE WESTERLY ON THE SOUTH LINE OF SAID NORTH WEST 1/4, 420.50 FEET; THENCE NORTHERLY PARALLEL TO THE EAST LINE OF SAID NORTH WEST 1/4, 47.99 FEET; THENCE EASTERLY ON A CURVE VARYING TOWARD THE SOUTH, THE RADIUS OF WHICH IS 17,263.74 FEET TO THE POINT OF BEGINNING) IN LAKE COUNTY, ILLINOIS.

PARCEL 6:

THE EAST 462.0 FEET OF THE SOUTH 1079.6 FEET OF THE NORTHWEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, (EXCEPT THE WEST 432.0 FEET OF THE SOUTH 701.0 FEET THEREOF AND ALSO EXCEPTING ANY PART THEREOF FALLING WITHIN THE RIGHT OF WAY OF FEDERAL AID ROUTE 22 (ILLINOIS ROUTE 137)), IN LAKE COUNTY, ILLINOIS.

EXHIBIT B
REQUESTED DEVIATIONS

Bulk Regulation Deviation(s):

- Section 26-4-8.5 for a reduction of the minimum lot area requirements, lot width requirements, and front, interior side, corner side, and rear setback requirements of single-family attached and detached dwelling units.
- Section 26-4-8.5 for an increase of the maximum building coverage and lot coverage requirements of single-family attached and detached dwelling units to permit the following:

		Lot Coverage	Building Coverage
Detached Single Family	Interior	55%	
	Corner	45%	
Attached (Duplexes)	Interior	55%	38%
	Corner	50%	

- Section 26-16-13.5 for a reduction in the minimum depth of the perimeter landscaped open space and to waive the requirement that a sidewalk shall be provided along each side of every street in or abutting a planned development.
- Section 26-11-7 to permit two (2) marketing signs along Peterson Road (IL Route 137) as depicted on the Signage Plan included with the submittal material.

Design Standard Deviation(s):

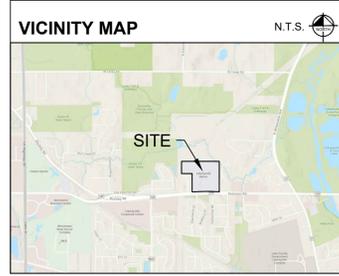
- Section 26-4-8.4(a)(1) exterior design standards to approve Petitioner’s Anti-Monotony Code and Elevation and Color Package Restrictions.
- Section 26-4-8.4(a)(3) to permit individual dormers with a width exceeding eight (8) feet as measured from the exterior framing dimension of the vertical edges of the dormer.
- Section 26-4-8.4(a)(4) to permit garage door(s) on a single-family home to constitute more than 30% of the area of the front face of the dwelling to which such garage is attached.
- Section 26-4-8.4(a)(4) to permit garage door(s) on a single-family home to be oriented on the zoning lot so that the garage door(s) face the front or corner property lines abutting the public right-of-way.

EXHIBIT C
SUBDIVISION PLAT

PRELIMINARY PLAT OF SUBDIVISION GREENWAY CHASE

THAT PART OF THE NORTHEAST QUARTER OF SECTION 8,
TOWNSHIP 44 NORTH, RANGE 11 EAST OF THE
THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS.

Tax PINs:
11-08-100-012
11-08-100-014
11-08-100-035
11-08-100-036
11-08-200-001



Plat Prepared For:
Pulte Group
1900 East Golf Road, Suite 300
Schaumburg, IL 60173

Site Address:
540-610 Peterson Rd.
Libertyville, IL 60048

SURVEY NOTES:

Underground utilities shown hereon are from record drawings obtained from the Village of Libertyville and the engineer of record and the surveyor cannot guarantee the locations of said utilities, except those that are observed on the surface at the time of this survey.

Measurements are made in feet and decimal feet, measurements shown in parentheses ex. (100.00) are record dimensions.

This service meets the Illinois minimum requirements for a boundary and topographic survey.

Field work was completed on October 30, 2024.

Parking Summary: There are 159 parking spaces and 5 handicapped parking spaces.

Iron Pipes set at all corners unless otherwise denoted.

AREA SUMMARY

PARCEL 1: 413,510 SQ.FT.	9.493 AC.
PARCEL 2: 368,588 SQ.FT.	8.461 AC.
PARCEL 3: 233,588 SQ.FT.	5.362 AC.
PARCEL 4: 271,940 SQ.FT.	6.243 AC.
PARCEL 5: 352,772 SQ.FT.	8.099 AC.
PARCEL 6: 193,703 SQ.FT.	4.447 AC.

TOTAL AREA: 1,834,101 SQ.FT. 42.105 AC.

REFERENCE DATUM

North American Vertical Datum 1988

Benchmark #1 Elevation = 755.97'

Chiseled square cut on east side of concrete traffic signal post at the Northwest corner of the intersection of N. Prospect Ave. and the entrance to Baytowne Shopping Square (Southeast corner of site).

Benchmark #2 Elevation = 759.02'

Chiseled square cut on south edge of concrete light pole base at the Northwest corner of site.

FLOOD STATEMENT:

According to Federal Emergency Management Agency's Flood Insurance Rate Map No. 17097C0161K, for Lake County, Illinois and incorporated areas, both dated September 18, 2013, this property is located within

Zone AE defined as "Special Flood Hazard Areas" Areas determined to be within the 1.0% annual chance flood plain with base flood elevations established.

Zone X (unshaded) defined as "Areas determined to be outside the 0.2% annual chance floodplain"

If this site is not within an identified special flood hazard area, this flood statement does not imply that the property and/or the structures thereon will be free from flooding or flood damage. On rare occasions, greater floods can and will occur and flood heights may be increased by man-made or natural causes. This flood statement shall not create liability on the part of the surveyor.

LINE TABLE		
NO.	BEARING	LENGTH
L1	S61°06'44"E	63.97'
L2	N88°50'17"E	48.71'
L3	N89°40'35"E	52.78'
L4	N74°47'13"E	28.14'
L6	S61°06'44"E	36.47'
L7	S89°49'18"E	7.91'
L8	N88°50'17"E	37.24'
L9	N88°50'17"E	11.25'
L10	N89°40'35"E	52.56'
L11	S89°49'18"E	9.67'
L12	N36°19'42"W	13.48'
L13	N80°56'26"W	40.67'
L14	S86°14'27"W	44.97'
L15	S45°50'35"W	44.97'
L16	S06°55'23"W	44.79'
L17	S33°18'16"E	44.79'
L18	S44°58'21"E	33.09'
L19	S73°57'04"E	33.85'
L20	S42°59'38"E	39.49'
L21	S26°04'16"E	46.46'
L22	N50°38'08"W	6.45'
L23	N52°52'49"W	36.59'
L24	N59°05'49"W	33.91'
L25	N64°57'35"W	31.55'
L26	N71°17'08"W	39.07'
L27	N77°52'51"W	34.56'
L28	N84°11'50"W	35.96'

LINE TABLE		
NO.	BEARING	LENGTH
L29	N85°06'47"W	41.09'
L30	N84°35'59"W	41.11'
L31	N84°35'59"W	41.11'
L32	N84°35'59"W	41.10'
L33	S85°03'05"W	43.51'
L34	S71°43'26"W	43.51'
L35	S57°27'00"W	48.36'
L36	S43°05'42"W	44.10'
L37	S29°04'21"W	46.23'
L38	S14°43'03"W	46.23'
L39	S02°03'26"W	36.16'
L40	S09°24'30"E	36.16'
L41	S21°40'27"E	44.07'
L42	N89°54'57"E	3.84'
L43	N89°54'57"E	28.42'
L44	N89°54'57"E	18.84'
L45	S89°47'29"E	18.72'
L46	S89°47'29"E	37.18'
L47	S70°37'01"E	13.14'
L48	N50°38'44"W	14.55'
L49	N14°52'55"W	40.79'
L51	S61°06'44"E	36.51'
L52	N88°50'17"E	44.39'
L53	N88°50'17"E	4.53'
L54	S89°40'35"W	53.00'
L55	N16°55'51"E	32.09'

CURVE TABLE				
NO.	RADIUS	LENGTH	CHORD BEARING	CHORD
C1	100.75'	29.63'	N08°18'22"W	29.52'
C2	100.75'	11.81'	N20°05'14"W	11.80'
C3	250.00'	103.21'	N11°39'41"W	102.48'
C4	199.91'	137.60'	N19°38'04"E	134.90'
C5	180.00'	123.90'	S70°21'54"E	121.47'
C6	150.00'	120.27'	S51°49'05"W	117.07'
C7	150.00'	75.75'	S14°22'57"W	74.94'
C8	175.00'	119.54'	S19°47'07"W	117.23'
C9	100.00'	88.39'	S64°40'38"W	85.54'
C10	270.00'	141.61'	S76°08'14"E	139.99'
C11	270.00'	123.00'	N76°38'24"E	121.94'
C12	270.00'	293.01'	N32°30'01"E	278.84'
C13	284.95'	36.95'	N03°51'43"W	36.93'
C14	303.29'	79.72'	N14°31'33"W	79.49'
C15	434.50'	151.62'	N32°08'08"W	150.85'
C16	100.00'	45.14'	N12°42'58"W	44.76'
C17	135.75'	8.84'	N01°43'55"W	8.84'
C18	180.00'	12.87'	N72°44'18"E	12.87'
C19	180.00'	41.85'	N64°01'46"E	41.75'
C20	180.00'	59.44'	N47°54'33"E	59.17'
C21	300.00'	32.53'	S64°13'06"E	32.51'
C22	300.00'	75.89'	S74°34'16"E	75.69'
C23	300.00'	48.93'	S86°29'24"E	48.87'
C24	299.98'	22.21'	N87°34'07"E	22.21'
C25	299.17'	84.01'	N77°24'30"E	83.73'

CURVE TABLE				
NO.	RADIUS	LENGTH	CHORD BEARING	CHORD
C26	70.00'	31.60'	N12°42'58"W	31.33'
C27	130.00'	16.46'	N03°24'43"W	16.45'
C28	130.00'	42.22'	N16°20'38"W	42.04'
C29	300.00'	85.06'	N49°47'58"E	84.78'
C30	300.00'	79.28'	N34°06'20"E	79.05'
C31	300.00'	78.46'	N19°02'31"E	78.24'
C32	298.89'	53.86'	N06°23'47"E	53.79'
C33	332.78'	49.20'	N04°29'27"W	49.16'
C34	345.88'	79.14'	N15°15'21"W	78.97'
C35	464.50'	80.93'	N27°07'37"W	80.83'
C36	464.50'	80.44'	N37°04'45"W	80.34'
C37	65.00'	65.28'	N71°45'16"W	62.57'
C38	65.00'	61.13'	S52°32'08"W	58.90'
C39	65.00'	13.22'	S19°46'10"W	13.20'
C40	65.00'	60.75'	S12°49'50"E	58.56'
C41	65.00'	75.44'	S72°51'15"E	71.28'
C42	65.00'	35.50'	N58°15'00"E	35.06'
C43	404.50'	12.76'	N41°11'51"W	12.76'
C44	404.50'	128.15'	N31°13'04"W	127.62'
C45	273.29'	37.55'	N18°06'53"W	37.52'
C46	131.29'	52.41'	S78°30'24"W	52.06'
C47	130.00'	62.53'	S53°08'06"W	61.93'
C48	210.00'	39.69'	N56°03'36"W	39.63'
C49	210.00'	38.48'	N66°43'26"W	38.43'
C50	210.00'	39.17'	N81°20'56"W	39.11'

CURVE TABLE				
NO.	RADIUS	LENGTH	CHORD BEARING	CHORD
C51	210.00'	12.43'	N88°23'17"W	12.43'
C52	60.00'	26.23'	S77°23'31"W	26.02'
C53	60.00'	30.07'	S50°30'47"W	29.75'
C54	60.00'	30.07'	S21°48'11"W	29.75'
C55	60.00'	30.07'	S06°54'25"E	29.75'
C56	60.00'	41.98'	S41°18'26"E	41.13'
C57	60.00'	124.34'	N59°16'54"E	103.25'
C58	150.00'	80.27'	N74°45'14"W	79.31'
C59	196.73'	30.14'	N53°59'27"W	30.11'
C60	229.91'	20.20'	S36°50'10"W	20.20'
C61	229.91'	37.70'	S29°37'14"W	37.66'
C62	229.91'	37.70'	S20°13'28"W	37.66'
C63	229.91'	37.70'	S10°49'41"W	37.66'
C64	229.91'	24.86'	S03°01'58"W	24.84'
C65	279.94'	33.42'	S03°15'06"E	33.40'
C66	279.94'	37.74'	S10°31'59"E	37.71'
C67	100.00'	31.30'	N08°47'53"W	31.17'
C68	180.00'	60.69'	N09°36'05"E	60.41'
C69	240.00'	84.06'	S71°08'47"E	83.63'
C70	240.00'	41.81'	S86°10'17"E	41.76'
C71	240.02'	42.18'	N84°39'26"E	42.12'
C72	240.00'	143.47'	N62°29'52"E	141.34'
C73	240.00'	143.45'	N28°14'54"E	141.33'
C74	240.00'	40.39'	N06°18'16"E	40.34'
C75	254.95'	6.83'	N00°56'06"W	6.83'
C76	70.00'	61.88'	S64°40'38"W	59.88'
C77	145.41'	96.75'	S20°14'56"W	94.98'
C78	169.91'	18.82'	S36°10'48"W	18.81'
C79	169.91'	91.76'	S17°32'06"W	90.65'
C80	169.91'	6.45'	S00°58'36"W	6.45'
C81	205.00'	14.20'	S37°22'11"W	14.20'
C82	207.85'	40.30'	S29°45'11"W	40.24'
C83	205.00'	35.48'	S19°09'45"W	35.44'
C84	205.00'	41.48'	S08°24'27"W	41.41'
C85	205.00'	8.57'	S01°24'49"W	8.57'
C86	120.00'	156.88'	N37°20'01"E	145.95'
C87	220.06'	76.81'	S09°50'12"E	76.42'
C88	210.00'	7.00'	S75°03'06"E	6.99'
C89	240.00'	30.36'	S76°00'00"W	30.34'
C90	240.00'	142.38'	N55°22'52"E	140.30'
C91	240.00'	154.58'	S19°56'06"W	151.92'
C92	240.00'	42.18'	N84°39'28"E	42.12'

LOT TABLE		
LOT NO.	ACRES	SQ. FT.
1	0.163	7,087
2	0.104	4,510
3	0.104	4,513
4	0.107	4,656
5	0.289	12,569
6	0.248	10,793
7	0.249	10,866
8	0.304	13,262
9	0.303	13,186
10	0.270	11,773
11	0.274	11,947
12	0.266	11,592
13	0.247	10,765
14	0.247	10,750
15	0.262	11,429
16	0.269	11,716
17	0.263	11,466
18	0.265	11,534
19	0.247	10,750
20	0.247	10,749
21	0.245	10,665
22	0.372	16,211
23	0.350	15,250
24	0.342	14,883
25	0.295	12,849
26	0.385	16,767

LOT TABLE		
LOT NO.	ACRES	SQ. FT.
27	0.255	11,112
28	0.378	16,465
29	0.135	5,895
30	0.104	4,510
31	0.104	4,510
32	0.104	4,510
33	0.104	4,510
34	0.104	4,510
35	0.104	4,510
36	0.104	4,510
37	0.104	4,510
38	0.104	4,510
39	0.104	4,510
40	0.124	5,409
41	0.129	5,600
42	0.136	5,909
43	0.130	5,644
44	0.105	4,571
45	0.108	4,723
46	0.112	4,885
47	0.116	5,044
48	0.167	7,257
49	0.174	7,583
50	0.174	7,583
51	0.146	6,365
52	0.158	6,901

LOT TABLE		
LOT NO.	ACRES	SQ. FT.
53	0.120	5,247
54	0.187	8,130
56	0.104	4,509
57	0.104	4,510
58	0.104	4,510
59	0.104	4,510
60	0.129	5,607
61	0.118	5,156
62	0.124	5,382
63	0.137	5,976
64	0.126	5,500
65	0.114	4,949
66	0.104	4,510
67	0.104	4,510
68	0.104	4,510
69	0.104	4,510
70	0.104	4,510
71	0.104	4,510
72	0.112	4,899
73	0.123	5,371
74	0.112	4,859
75	0.113	4,912
76	0.	

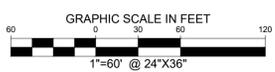
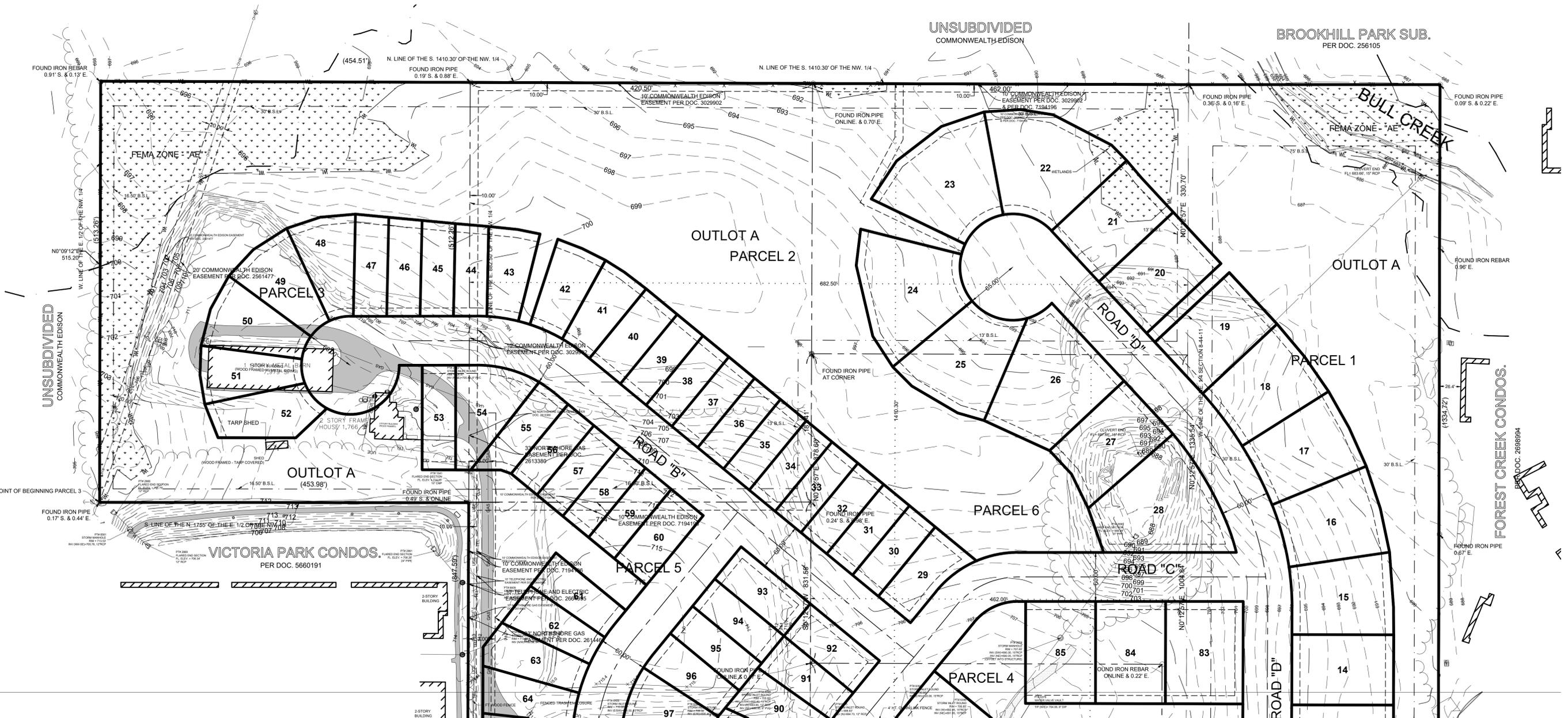
**PRELIMINARY PLAT OF SUBDIVISION
GREENWAY CHASE**

THAT PART OF THE NORTHEAST QUARTER OF SECTION 8,
TOWNSHIP 44 NORTH, RANGE 11 EAST OF THE
THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS.

Tax PINs:
11-08-100-012
11-08-100-014
11-08-100-035
11-08-100-036
11-08-200-001

Plat Prepared For:
Pulte Group
1900 East Golf Road, Suite 300
Schaumburg, IL 60173

Site Address:
540-610 Peterson Rd.
Libertyville, IL 60048



No.	DATE	REVISION DESCRIPTION	Scale	Drawn by	Checked by	Date	Project No.	Sheet No.
1	3/31/25	REVISED OUTLOT GEOMETRY	1"=60'	MGJ	BAS	01/06/25	168247001	2 OF 6

Kimley»Horn

4201 Winfield Road
Warrenville, Illinois 60555
DESIGN FIRM # 184002012-0006
Tel. No. (630) 487-5550
www.kimley-horn.com

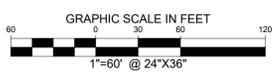
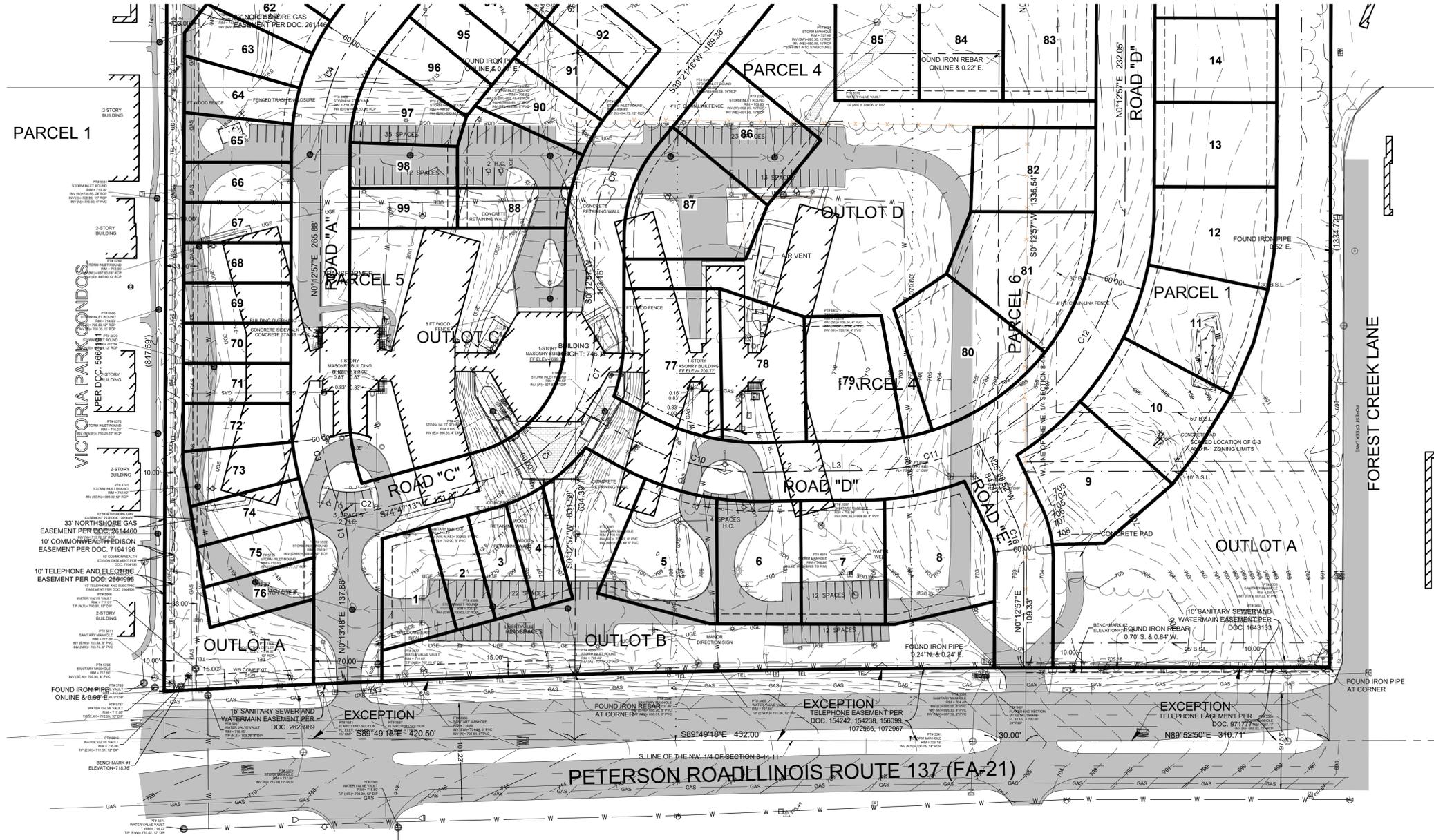
**PRELIMINARY PLAT OF SUBDIVISION
GREENWAY CHASE**

THAT PART OF THE NORTHEAST QUARTER OF SECTION 8,
TOWNSHIP 44 NORTH, RANGE 11 EAST OF THE
THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS.

Tax PINs:
11-08-100-012
11-08-100-014
11-08-100-035
11-08-100-036
11-08-200-001

Plat Prepared For:
Pulte Group
1900 East Golf Road, Suite 300
Schaumburg, IL 60173

Site Address:
540-610 Peterson Rd.
Libertyville, IL 60048



No.	DATE	REVISION DESCRIPTION	Scale	Drawn by	Checked by	Date	Project No.	Sheet No.
1	3/31/25	REVISED OUTLOT GEOMETRY	1"=60'	MGJ	BAS	01/06/25	168247001	3 OF 6

Kimley»Horn
4201 Winfield Road
Warrenville, Illinois 60555
Tel. No. (630) 487-5550
www.kimley-horn.com

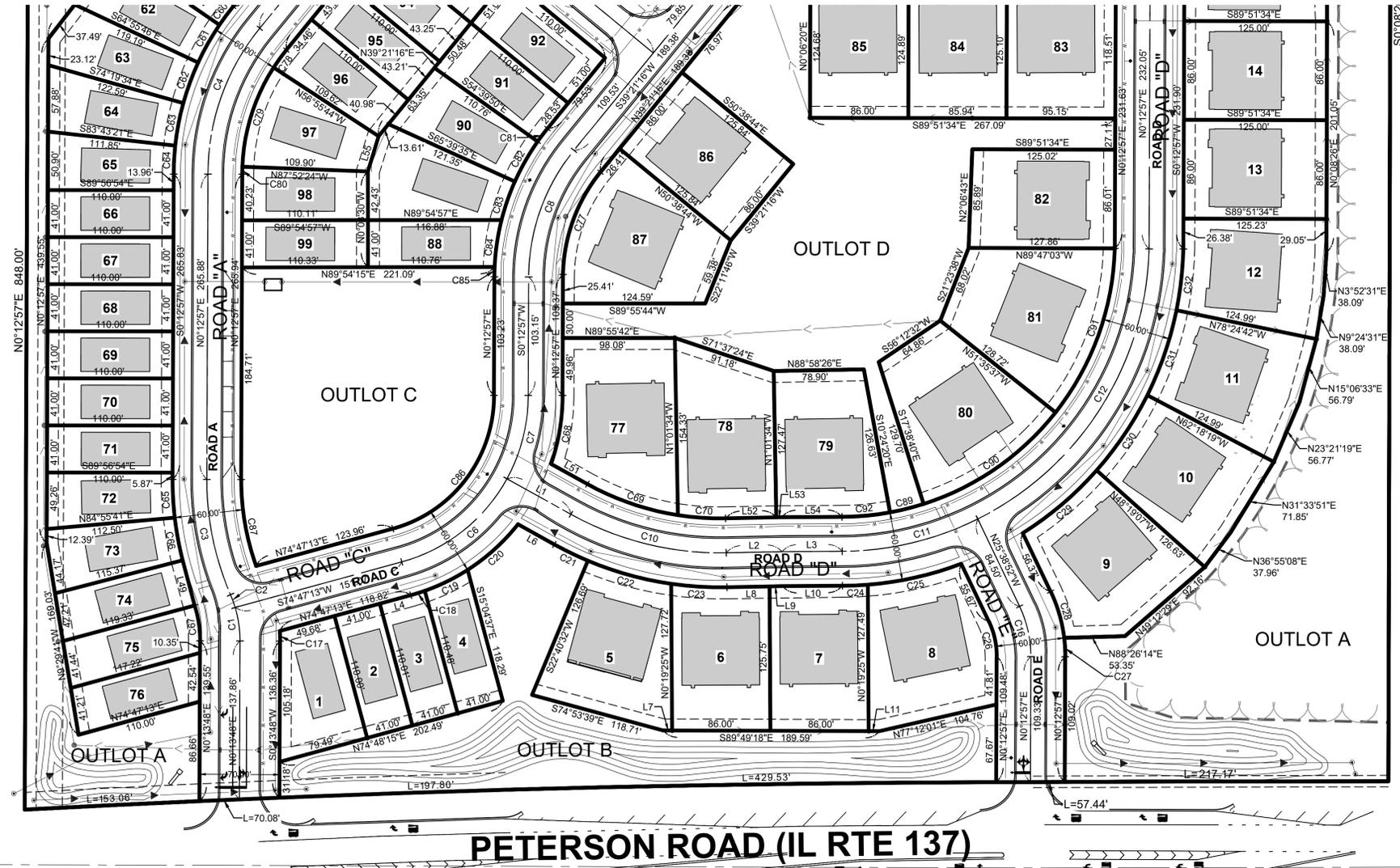
**PRELIMINARY PLAT OF SUBDIVISION
GREENWAY CHASE**

THAT PART OF THE NORTHEAST QUARTER OF SECTION 8,
TOWNSHIP 44 NORTH, RANGE 11 EAST OF THE
THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS.

Tax PINs:
11-08-100-012
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11-08-100-035
11-08-100-036
11-08-200-001

Plat Prepared For:
Pulte Group
1900 East Golf Road, Suite 300
Schaumburg, IL 60173

Site Address:
540-610 Peterson Rd.
Libertyville, IL 60048



PETERSON ROAD (IL RTE 137)



1 3/31/25 REVISED OUTLOT GEOMETRY			4201 Winfield Road Warrenville, Illinois 60555 DESIGN FIRM # 184002012-0006		Tel. No. (630) 487-5550 www.kimley-horn.com	
No.	DATE	REVISION DESCRIPTION	Scale	Drawn by	Checked by	Date
			1"=60'	MGJ	BAS	01/06/25
						Project No. 168247001
						Sheet No. 3 OF 6

PRELIMINARY PLAT OF SUBDIVISION GREENWAY CHASE

THAT PART OF THE NORTHEAST QUARTER OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS.

LAKE COUNTY RECORDER'S CERTIFICATE

STATE OF ILLINOIS)
) S.S.
COUNTY OF LAKE)

THIS INSTRUMENT NUMBER _____ WAS FILED FOR RECORD IN THE RECORDER'S OFFICE OF LAKE COUNTY, ILLINOIS, THIS _____ DAY OF _____, A.D. 20____, AT _____ O'CLOCK ____ M.

RECORDER

COUNTY CLERK CERTIFICATE

STATE OF ILLINOIS)
) S.S.
COUNTY OF LAKE)

I, _____ COUNTY CLERK OF LAKE COUNTY, ILLINOIS, DO HEREBY CERTIFY THAT THERE ARE NO DELINQUENT GENERAL TAXES, NO UNPAID, NO UNPAID FORFEITED TAXES AND NO REDEEMABLE TAX SALES AGAINST ANY OF THE LAND INCLUDED IN THE ATTACHED PLAT. I FURTHER CERTIFY THAT I HAVE RECEIVED ALL STATUTORY FEES IN CONNECTION WITH THE ANNEXED PLAT.

DATED THIS _____ DAY OF _____, A.D. 20____.

BY: _____
COUNTY CLERK

PERMISSION TO RECORD

STATE OF ILLINOIS)
) S.S.
COUNTY OF DUPAGE)

I, BRADLEY A. STROHL, ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3686, HEREBY GRANT PERMISSION TO A REPRESENTATIVE OF THE VILLAGE OF LIBERTYVILLE, ILLINOIS, TO RECORD THIS PLAT ON OR BEFORE DECEMBER 31, 2025. SHALL SHOW PROPER IDENTIFICATION AND PROVIDE THIS SURVEYOR WITH A RECORDED COPY OF SAID PLAT.

DATED THIS _____ DAY OF _____, A.D. 20____.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3686
LICENSE EXPIRES: NOVEMBER 30, 2026

DESIGN FIRM PROFESSIONAL REGISTRATION NO. 184002012-0006
EXPIRES APRIL 30, 2025

SURVEYOR'S CERTIFICATE

STATE OF ILLINOIS)
) S.S.
COUNTY OF DUPAGE)

THIS IS TO DECLARE THAT THE FOLLOWING DESCRIBED PROPERTY WAS SURVEYED AND SUBDIVIDED BY KIMLEY-HORN, INC., UNDER THE SUPERVISION OF AN ILLINOIS PROFESSIONAL LAND SURVEYOR AND THAT THE PLAT HEREON DRAWN IS A CORRECT REPRESENTATION OF SAID SURVEY.

LEGAL DESCRIPTION OF PROPERTY BEING SUBDIVIDED INCLUDED HEREON

SUBDIVIDED PROPERTY CONTAINS 42.105 ACRES, MORE OR LESS AND ALL DISTANCES ARE SHOWN IN FEET AND DECIMAL PARTS THEREOF.

1/2 DIAMETER BY 24" LONG IRON PIPES WILL BE SET AT ALL SUBDIVISION CORNERS, LOT CORNERS, POINTS OF CURVATURE AND POINTS OF TANGENCY IN COMPLIANCE WITH ILLINOIS STATUTES AND APPLICABLE ORDINANCES, UNLESS OTHERWISE NOTED.

THIS IS ALSO TO DECLARE THAT THE PROPERTY AS DESCRIBED ON THE ANNEXED PLAT LIES WITHIN THE CORPORATE LIMITS OF THE VILLAGE OF LIBERTYVILLE, ILLINOIS WHICH HAS ADOPTED A VILLAGE PLAN AND IS EXERCISING THE SPECIAL POWER AUTHORIZED BY DIVISION 12 OF ARTICLE 11 OF THE ILLINOIS MUNICIPAL CODE.

GIVEN UNDER MY HAND AND SEAL THIS _____ DAY OF _____, A.D. 20____.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3686
LICENSE EXPIRES: NOVEMBER 30, 2026

DESIGN FIRM PROFESSIONAL REGISTRATION NO. 184002012-0006
EXPIRES APRIL 30, 2025

Tax PINs:
11-08-100-012
11-08-100-014
11-08-100-035
11-08-100-036
11-08-200-001

Plat Prepared For:
Pulte Group
1900 East Golf Road, Suite 300
Schaumburg, IL 60173

Site Address:
540-610 Peterson Rd.
Libertyville, IL 60048

OWNER'S CONSENT

STATE OF _____)
) S.S.
COUNTY OF _____)

THE UNDERSIGNED, _____, HEREBY CERTIFIES THAT HE/SHE/THEY/IT IS THE HOLDER OF THE LEGAL TITLE OF ALL OF THE PROPERTY DESCRIBED HEREON AND THAT IT HAS CAUSED SAID PROPERTY TO BE SURVEYED AND SUBDIVIDED AS

SHOWN ON THE PLAT HEREON DRAWN. THIS IS TO ALSO CERTIFY THAT _____

AS OWNER OF THE PROPERTY DESCRIBED AS _____ AND LEGALLY DESCRIBED ON THE PLAT OF THE SAME NAME, HAVE DETERMINED TO THE BEST OF OUR KNOWLEDGE THE SCHOOL DISTRICT IN WHICH EACH OF THE FOLLOWING LOTS LIE.

LOT NUMBER(S) SCHOOL DISTRICT

ALL GRADE SCHOOL DISTRICT NO. 70 (LIBERTYVILLE)
HIGH SCHOOL DISTRICT NO. 128 (LIBERTYVILLE)
JUNIOR COLLEGE DISTRICT NO. C03532 (LAKE CO. COMM. COLL.)

DATED THIS _____ DAY OF _____, A.D. 20____.

BY: _____

BY: _____

NOTARY PUBLIC

STATE OF _____)
) S.S.
COUNTY OF _____)

I, _____, A NOTARY PUBLIC IN AND FOR THE COUNTY AND STATE

AFORESaid, DO HEREBY CERTIFY THAT _____ AND _____

OF _____ WHO IS/ARE PERSONALLY KNOWN TO ME TO BE THE SAME WHOSE NAME(S) IS/ARE SUBSCRIBED TO THE FOREGOING CERTIFICATE, APPEARED BEFORE ME THIS DAY IN PERSON AND ACKNOWLEDGED THAT HE/SHE/THEY DID SIGN AND DELIVER THIS INSTRUMENT AS A FREE AND VOLUNTARY ACT FOR THE USES AND PURPOSES HEREIN SET FORTH.

GIVEN UNDER MY HAND AND NOTORIAL SEAL THIS _____ DAY OF _____, A.D. 20____.

NOTARY PUBLIC

COMMONWEALTH EDISON AND SBC EASEMENT PROVISIONS

AN EASEMENT FOR SERVING THE SUBDIVISION AND OTHER PROPERTY WITH ELECTRIC AND COMMUNICATION SERVICE IS HEREBY RESERVED FOR AND GRANTED TO:

COMMONWEALTH EDISON COMPANY AND
SBC ILLINOIS, A.K.A. AMERITECH ILLINOIS,
A.K.A. ILLINOIS BELL TELEPHONE COMPANY, GRANTEEES,

THEIR RESPECTIVE LICENSEES, SUCCESSORS, AND ASSIGNS, JOINTLY AND SEVERALLY, TO CONSTRUCT, OPERATE, REPAIR, MAINTAIN, MODIFY, RECONSTRUCT, REPLACE, SUPPLEMENT, RELOCATE AND REMOVE, FROM TIME TO TIME, POLES, GUYS, ANCHORS, WIRES, CABLES, CONDUITS, MANHOLES, TRANSFORMERS, PEDESTALS, EQUIPMENT CABINETS OR OTHER FACILITIES USED IN CONNECTION WITH OVERHEAD AND UNDERGROUND TRANSMISSION AND DISTRIBUTION OF ELECTRICITY COMMUNICATIONS, SOUNDS AND SIGNALS IN, OVER, UNDER, ACROSS, ALONG AND UPON THE SURFACE OF THE PROPERTY SHOWN WITHIN THE DASHED OR DOTTED LINES (OR SIMILAR DESIGNATION) ON THE PLAT AND MARKED "EASEMENT", "UTILITY EASEMENT", "PUBLIC UTILITY EASEMENT", "P.U.E." (OR SIMILAR DESIGNATION), THE PROPERTY DESIGNATED IN THE DECLARATION OF CONDOMINIUM AND/OR ON THIS PLAT AS "COMMON ELEMENTS", AND THE PROPERTY DESIGNATED ON THE PLAT AS "COMMON AREA OR AREAS", AND THE PROPERTY DESIGNATED ON THE PLAT FOR STREETS AND ALLEYS, WHETHER PUBLIC OR PRIVATE, TOGETHER WITH THE RIGHT TO INSTALL REQUIRED SERVICE CONNECTIONS OVER OR UNDER THE SURFACE OF EACH LOT AND COMMON AREA OR AREAS TO SERVE IMPROVEMENTS THEREON, OR ON ADJACENT LOTS, AND COMMON AREA OR AREAS, THE RIGHT TO CUT, TRIM OR REMOVE TREES, BUSHES, ROOTS AND SAPLINGS AND TO CLEAR OBSTRUCTIONS FROM THE SURFACE AND SUBSURFACE AS MAY BE REASONABLY REQUIRED INCIDENT TO THE RIGHTS HEREIN GIVEN, AND THE RIGHT TO ENTER UPON THE SUBDIVIDED PROPERTY FOR ALL SUCH PURPOSES. OBSTRUCTIONS SHALL NOT BE PLACED OVER GRANTEEES' FACILITIES OR IN, UPON OR OVER THE PROPERTY WITHIN THE DASHED OR DOTTED LINES (OR SIMILAR DESIGNATION) MARKED "EASEMENT", "UTILITY EASEMENT", "PUBLIC UTILITY EASEMENT", "P.U.E." (OR SIMILAR DESIGNATION), WITHOUT THE PRIOR WRITTEN CONSENT OF GRANTEEES. AFTER INSTALLATION OF ANY SUCH FACILITIES, THE GRADE OF THE SUBDIVIDED PROPERTY SHALL NOT BE ALTERED IN A MANNER SO AS TO INTERFERE WITH THE PROPER OPERATION AND MAINTENANCE THEREOF.

THE TERM "COMMON ELEMENTS" SHALL HAVE THE MEANING SET FORTH IN SUCH TERM IN THE "CONDOMINIUM PROPERTY ACT", CHAPTER 765 ILCS 605/2, AS AMENDED FROM TIME TO TIME. THE TERM "COMMON AREA OR AREAS" IS DEFINED AS A LOT, PARCEL OR AREA OF REAL PROPERTY, THE BENEFICIAL USE AND ENJOYMENT OF WHICH IS RESERVED IN WHOLE AS AN APPORTIONMENT TO THE SEPARATELY OWNED LOTS, PARCEL OR AREAS WITHIN THE PLANNED DEVELOPMENT, EVEN THOUGH SUCH BE OTHERWISE DESIGNATED ON THE PLAT BY TERMS SUCH AS "OUTLOTS", "COMMON ELEMENTS", "OPEN SPACE", "OPEN AREA", "COMMON GROUND", "PARKING" AND "COMMON AREA". THE TERM "COMMON AREA OR AREAS", AND "COMMON ELEMENTS" INCLUDES REAL PROPERTY SURFACED WITH INTERIOR DRIVEWAYS AND WALKWAYS, BUT EXCLUDES REAL PROPERTY PHYSICALLY OCCUPIED BY A BUILDING, SERVICE BUSINESS DISTRICT OR STRUCTURES SUCH AS A POOL OR RETENTION POND OR MECHANICAL EQUIPMENT. RELOCATION OF FACILITIES WILL BE DONE BY GRANTEEES AT COST OF GRANTOR/LOT OWNER, UPON WRITTEN REQUEST.

VILLAGE ENGINEER/PLAT OFFICER CERTIFICATE

STATE OF ILLINOIS)
) S.S.
COUNTY OF LAKE)

I, _____ VILLAGE PLAT OFFICER/ENGINEER OF THE VILLAGE OF LIBERTYVILLE, DO HEREBY CERTIFY THAT ALL PROVISIONS PERTAINING TO THE LIBERTYVILLE SUBDIVISION ORDINANCE, INsofar AS THEY PERTAIN TO THE ACCOMPANYING PLAT, HAVE BEEN SATISFACTORILY COMPLIED WITH.

ATTESTED TO THIS _____ DAY OF _____, AD 20____.

VILLAGE PLAT OFFICER/ENGINEER VILLAGE OF LIBERTYVILLE

VILLAGE BOARD CERTIFICATE

STATE OF ILLINOIS)
) S.S.
COUNTY OF LAKE)

APPROVED BY THE PRESIDENT AND BOARD OF TRUSTEES OF THE VILLAGE OF LIBERTYVILLE, LAKE COUNTY, ILLINOIS AT A MEETING, HELD THIS _____ DAY OF _____, AD 20____.

VILLAGE PRESIDENT

PRINTED NAME

VILLAGE CLERK

PRINTED NAME

PLAN COMMISSION CERTIFICATE

STATE OF ILLINOIS)
) S.S.
COUNTY OF LAKE)

APPROVED BY THE PLAN COMMISSION OF THE VILLAGE OF LIBERTYVILLE, LAKE COUNTY, ILLINOIS AT A MEETING, HELD THIS _____ DAY OF _____, AD 20____.

CHAIRMAN

PRINTED NAME

SECRETARY

PRINTED NAME

PERPETUAL EASEMENT

A PERPETUAL EASEMENT APPURTENANT IS HEREBY GRANTED TO THE VILLAGE OF LIBERTYVILLE, ITS SUCCESSORS AND ASSIGNS, OVER, UPON, ACROSS, THROUGH AND UNDER THOSE PORTIONS OF THE ABOVE DESCRIBED REAL ESTATE DESIGNATED AS PUBLIC UTILITY AND/OR DRAINAGE EASEMENT (P.U. & D.E.) ON THIS PLAT FOR THE PURPOSE OF INSTALLING, LAYING, CONSTRUCTING, OPERATING, MAINTAINING, REPAIRING, RENEWING AND REPLACING WATER MAINS, SANITARY SEWER LINES, FORCE MAIN LINES, STORM SEWER LINES, PIPES, STREET LIGHT POWER CABLES, DITCHES, SWALES, STORM WATER DETENTION FACILITIES, AND ANY OTHER VILLAGE UTILITIES, TOGETHER WITH ALL APPURTENANT STRUCTURES, INCLUDING, BUT NOT LIMITED TO, MANHOLES, WET WELLS, LIFT STATIONS, FIRE HYDRANTS, VALVE VAULTS, STREET LIGHTING EQUIPMENT AND ANY AND ALL OTHER FIXTURES AND EQUIPMENT REQUIRED FOR THE PURPOSE OF SERVING THE ABOVE DESCRIBED REAL ESTATE WITH WATER SERVICE, SANITARY SEWER SERVICE, STORM WATER MANAGEMENT, STREET LIGHTING AND OTHER MUNICIPAL SERVICES AND FOR THE PURPOSE OF PROVIDING INGRESS TO AND EGRESS FROM ALL OF THE LOTS IN THE SUBDIVISION FOR EMERGENCY VEHICLES OF ANY AND ALL TYPES WHATSOEVER. IN NO EVENT SHALL ANY PERMANENT BUILDING BE PLACED UPON THE SAID EASEMENT AREAS, BUT THEY MAY BE USED FOR GARDENS, SHRUBS, LANDSCAPING AND SUCH OTHER PURPOSES THAT DO NOT, AND WILL NOT IN THE FUTURE, INTERFERE UNREASONABLY WITH THE EASEMENT RIGHTS HEREIN GRANTED.

NORTHERN ILLINOIS GAS COMPANY EASEMENT PROVISIONS

AN EASEMENT IS HEREBY RESERVED FOR AND GRANTED TO NORTHERN ILLINOIS GAS COMPANY, ITS SUCCESSORS AND ASSIGNS ("N-GAS") TO INSTALL, OPERATE, MAINTAIN, REPAIR, REPLACE AND REMOVE, FACILITIES USED IN CONNECTION WITH THE TRANSMISSION AND DISTRIBUTION OF NATURAL GAS IN, OVER, UNDER, ACROSS, ALONG AND UPON THE SURFACE OF THE PROPERTY SHOWN ON THIS PLAT MARKED "EASEMENT", "COMMON AREA OR AREAS" AND STREETS AND ALLEYS, WHETHER PUBLIC OR PRIVATE, AND THE PROPERTY DESIGNATED IN THE DECLARATION OF CONDOMINIUM AND/OR ON THIS PLAT AS "COMMON ELEMENTS", TOGETHER WITH THE RIGHT TO INSTALL REQUIRED SERVICE CONNECTIONS OVER OR UNDER THE SURFACE OF EACH LOT AND COMMON AREA OR AREAS TO SERVE IMPROVEMENTS THEREON, OR ON ADJACENT LOTS, AND COMMON AREA OR AREAS, AND THE RIGHT TO REMOVE OBSTRUCTIONS, INCLUDING BUT NOT LIMITED TO, TREES, BUSHES, ROOTS AND FENCES AS MAY BE REASONABLY REQUIRED INCIDENT TO THE RIGHTS HEREIN GIVEN, AND THE RIGHT TO ENTER UPON THE PROPERTY FOR ALL SUCH PURPOSES. OBSTRUCTIONS SHALL NOT BE PLACED OVER N-GAS FACILITIES OR IN, UPON OR OVER THE PROPERTY IDENTIFIED ON THIS PLAT FOR UTILITY PURPOSES WITHOUT THE PRIOR WRITTEN CONSENT OF N-GAS. AFTER INSTALLATION OF ANY SUCH FACILITIES, THE GRADE OF THE SUBDIVIDED PROPERTY SHALL NOT BE ALTERED IN A MANNER SO AS TO INTERFERE WITH THE PROPER OPERATION AND MAINTENANCE THEREOF.

THE TERM "COMMON ELEMENTS" SHALL HAVE THE MEANING SET FORTH FOR SUCH TERM IN THE "CONDOMINIUM PROPERTY ACT", CHAPTER 765 ILCS 605/2, AS AMENDED FROM TIME TO TIME.

THE TERM "COMMON AREA OR AREAS" IS DEFINED AS A LOT, PARCEL OR AREA OF REAL PROPERTY, INCLUDING REAL PROPERTY SURFACED WITH INTERIOR DRIVEWAYS AND WALKWAYS, THE BENEFICIAL USE AND ENJOYMENT OF WHICH IS RESERVED IN WHOLE AS AN APPURTENANCE TO THE SEPARATELY OWNED LOTS, PARCELS OR AREAS WITHIN THE PROPERTY, EVEN THOUGH SUCH BE OTHERWISE DESIGNATED ON THE PLAT BY TERMS.

PARCEL 1:

THAT PART OF THE NORTHEAST QUARTER OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE SOUTH LINE OF AND 6.82 CHAINS WEST FROM THE SOUTHEAST CORNER OF SAID NORTHEAST QUARTER; THENCE WEST ALONG SAID SOUTH LINE, 2229.68 FEET TO THE SOUTHWEST CORNER OF SAID NORTHEAST QUARTER, THENCE NORTH ALONG THE WEST LINE OF SAID NORTHEAST QUARTER, 1410.30 FEET; THENCE EAST 2230.38 FEET TO A POINT WHICH IS 6.82 CHAINS WEST FROM THE EAST LINE OF SAID NORTHEAST QUARTER AND 1405.2 FEET NORTH FROM THE SOUTH LINE OF SAID NORTHEAST QUARTER; THENCE SOUTH TO THE PLACE OF BEGINNING (EXCEPTING THEREFROM THE FOLLOWING: (A) THE EAST 1920.68 FEET THEREOF; (B) THAT PART THEREOF, IF ANY, FALLING IN BROOKHILL PARK, A SUBDIVISION OF PARTS OF SECTIONS 8 AND 9, TOWNSHIP AND RANGE AFORESAID, RECORDED APRIL 24, 1925, AS DOCUMENT 256105, IN BOOK "M" OF PLATS, PAGE 100; AND ALSO (C) EXCEPTING THEREFROM THAT PART CONVEYED BY WARRANTY DEED DATED SEPTEMBER 25, 1967, TO THE STATE OF ILLINOIS FOR THE USE OF DEPARTMENT OF PUBLIC WORKS AND BUILDINGS DESCRIBED AS FOLLOWS: PART OF THE NORTHEAST QUARTER OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, BOUNDED AND FULLY DESCRIBED AS FOLLOWS: BEGINNING AT THE SOUTHWEST CORNER OF SAID NORTHEAST QUARTER, THENCE EASTERLY ALONG THE SOUTH LINE OF SAID NORTHEAST QUARTER, 309.00 FEET; THENCE NORTHERLY PARALLEL TO THE EAST LINE OF SAID NORTHEAST QUARTER, 74.81 FEET; THENCE WESTERLY TOWARD A POINT ON THE WEST LINE OF SAID NORTHEAST QUARTER, SAID POINT BEING 75.00 FEET NORTHERLY FROM THE SOUTHWEST CORNER OF SAID NORTHEAST QUARTER, 217.17 FEET TO A POINT OF CURVE TOWARD THE SOUTH; THENCE ON SAID CURVE TO THE SOUTH, THE RADIUS OF WHICH IS 17,263.74 FEET, 91.83 FEET TO A POINT ON THE WEST LINE OF SAID NORTHEAST QUARTER, THIS POINT BEING 74.76 FEET NORTHERLY FROM THE SOUTHWEST CORNER OF SAID NORTHEAST QUARTER; THENCE SOUTHERLY ON THE WEST LINE OF SAID NORTHEAST QUARTER, 74.76 FEET TO SAID POINT OF BEGINNING), IN LAKE COUNTY, ILLINOIS.

PARCEL 2:

THE EAST 462.0 FEET OF THE SOUTH 1410.30 FEET (AS MEASURED ALONG THE EAST AND SOUTH LINES, RESPECTIVELY) OF THE NORTH WEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, (EXCEPT THE SOUTH 1079.60 FEET THEREOF), ALL IN LAKE COUNTY, ILLINOIS, AND THE WEST 420.5 FEET OF THE EAST 882.50 FEET OF THE SOUTH 1410.30 FEET (AS MEASURED ALONG THE EAST AND SOUTH LINES, RESPECTIVELY) OF THE NORTH WEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN (EXCEPT THAT PART DESCRIBED AS FOLLOWS, TO-WIT: BEGINNING AT THE SOUTH EAST CORNER OF THE SAID WEST 420.5 FEET OF THE EAST 882.50 FEET OF THE SOUTH 1410.30 FEET OF THE NORTH WEST 1/4 OF SECTION 8; THENCE NORTHERLY ALONG THE EAST LINE OF THE SAID WEST 420.5 FEET OF THE EAST 882.50 FEET OF THE SOUTH 1410.30 FEET OF THE NORTH WEST 1/4 OF SECTION 8 FOR A DISTANCE OF 898.19 FEET; THENCE WESTERLY ALONG A LINE TO THE WEST LINE OF THE SAID EAST 882.50 FEET OF THE NORTH WEST 1/4 OF SECTION 8 TO A POINT WHICH IS 896.80 FEET NORTHERLY OF THE SOUTH LINE OF THE SAID NORTH WEST 1/4 OF SECTION 8 (AS MEASURED ALONG THE SAID WEST LINE OF THE EAST 882.50 FEET); THENCE SOUTHERLY ALONG THE SAID WEST LINE OF THE EAST 882.50 FEET OF THE SOUTH 1410.30 FEET OF THE NORTH WEST 1/4 OF SECTION 8 FOR A DISTANCE OF 896.80 FEET TO THE SAID SOUTH LINE OF THE NORTH WEST 1/4 OF SECTION 8; THENCE EASTERLY ALONG THE SAID SOUTH LINE OF THE NORTH WEST 1/4 OF SECTION 8 FOR A DISTANCE OF 420.5 FEET TO THE POINT OF BEGINNING OF THIS EXCEPTION), IN LAKE COUNTY, ILLINOIS.

PARCEL 3:

THAT PART OF THE EAST 1/2 OF THE NORTH WEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS BEGINNING AT A POINT ON THE WEST LINE OF THE EAST 1/2 OF SAID NORTH WEST 1/4 WHICH IS 1755 FEET SOUTH OF THE NORTH WEST CORNER THEREOF; THENCE EAST ALONG THE SOUTH LINE OF THE NORTH 1755 FEET OF SAID EAST 1/2 OF THE NORTH WEST 1/4, A DISTANCE OF 453.98 FEET TO THE WEST LINE OF THE EAST 882.50 FEET OF THE NORTH WEST 1/4 OF SAID SECTION 8; THENCE NORTH ALONG SAID WEST LINE OF THE EAST 882.50 FEET, A DISTANCE OF 512.66 FEET TO THE NORTH LINE OF THE SOUTH 1410.30 FEET OF THE NORTH WEST 1/4 OF SAID SECTION 8; THENCE WEST ALONG SAID NORTH LINE OF THE SOUTH 1410.30 FEET OF THE NORTH WEST 1/4, 454.51 FEET TO THE WEST LINE OF THE EAST 1/2 OF THE NORTH WEST 1/4 OF SAID SECTION 8; THENCE SOUTH ALONG SAID WEST LINE OF THE EAST 1/2 OF SAID NORTH WEST 1/4, A DISTANCE OF 513.26 FEET TO THE POINT OF BEGINNING, IN LAKE COUNTY, ILLINOIS.

PARCEL 4:

THE WEST 432 FEET OF THE EAST 462 FEET OF THE SOUTH 701 FEET OF THE NORTHWEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, EXCEPT THEREFROM THAT PART OF THE NORTHWEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, BOUNDED AND FULLY DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE EAST LINE OF SAID NORTHWEST 1/4 SAID POINT BEING 74.76 FEET NORTH OF THE SOUTHEAST CORNER THEREOF; THENCE SOUTH ON THE EAST LINE OF SAID NORTHWEST 1/4, 74.76 FEET TO THE SOUTHEAST CORNER THEREOF; THENCE WESTERLY ON THE SOUTH LINE OF SAID NORTHWEST 1/4, 462.00 FEET; THENCE NORTHERLY PARALLEL WITH THE EAST LINE OF SAID NORTHWEST 1/4, 66.61 FEET; THENCE EASTERLY ON A CURVE VARYING TOWARD THE SOUTH, THE RADIUS OF WHICH IS 17,263.74 FEET TO SAID POINT OF BEGINNING), IN LAKE COUNTY, ILLINOIS.

PARCEL 5:

THE WEST 420.5 FEET OF THE EAST 882.5 FEET OF THAT PART OF THE EAST 1/2 OF THE NORTH WEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING SOUTH OF THE NORTH 1755 FEET THEREOF, (EXCEPT THAT PART THEREOF DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE EAST LINE OF SAID PARCEL, 66.61 FEET NORTHERLY FROM THE SOUTH LINE OF SAID NORTH WEST 1/4; THENCE SOUTHERLY ALONG SAID EAST LINE, 66.61 FEET TO THE SOUTH LINE OF SAID NORTH WEST 1/4; THENCE WESTERLY ON THE SOUTH LINE OF SAID NORTH WEST 1/4, 420.50 FEET; THENCE NORTHERLY PARALLEL TO THE EAST LINE OF SAID NORTH WEST 1/4, 47.99 FEET; THENCE EASTERLY ON A CURVE VARYING TOWARD THE SOUTH, THE RADIUS OF WHICH IS 17,263.74 FEET TO THE POINT OF BEGINNING) IN LAKE COUNTY, ILLINOIS.

PARCEL 6:

THE EAST 462.0 FEET OF THE SOUTH 1079.6 FEET OF THE NORTHWEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, (EXCEPT THE WEST 432.0 FEET OF THE SOUTH 701.0 FEET THEREOF AND ALSO EXCEPTING ANY PART THEREOF FALLING WITHIN THE RIGHT OF WAY OF FEDERAL AID ROUTE 22 (ILLINOIS ROUTE 137)), IN LAKE COUNTY, ILLINOIS.

ILLINOIS DEPARTMENT OF TRANSPORTATION CERTIFICATE

THIS PLAT HAS BEEN APPROVED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION WITH RESPECT TO ROADWAY ACCESS PURSUANT TO PARAGRAPH 2 OF "AN ACT TO REVISE THE LAW IN RELATION TO PLATS" AS AMENDED, A PLAN THAT MEETS THE REQUIREMENTS CONTAINED IN THE DEPARTMENTS "POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS" WILL BE REQUIRED BY THE DEPARTMENT.

DIRECTOR OF HIGHWAYS
REGION ONE ENGINEER



BASIS OF BEARINGS

North American Datum of 1983 (2011)
Illinois State Plane East Zone (1201)

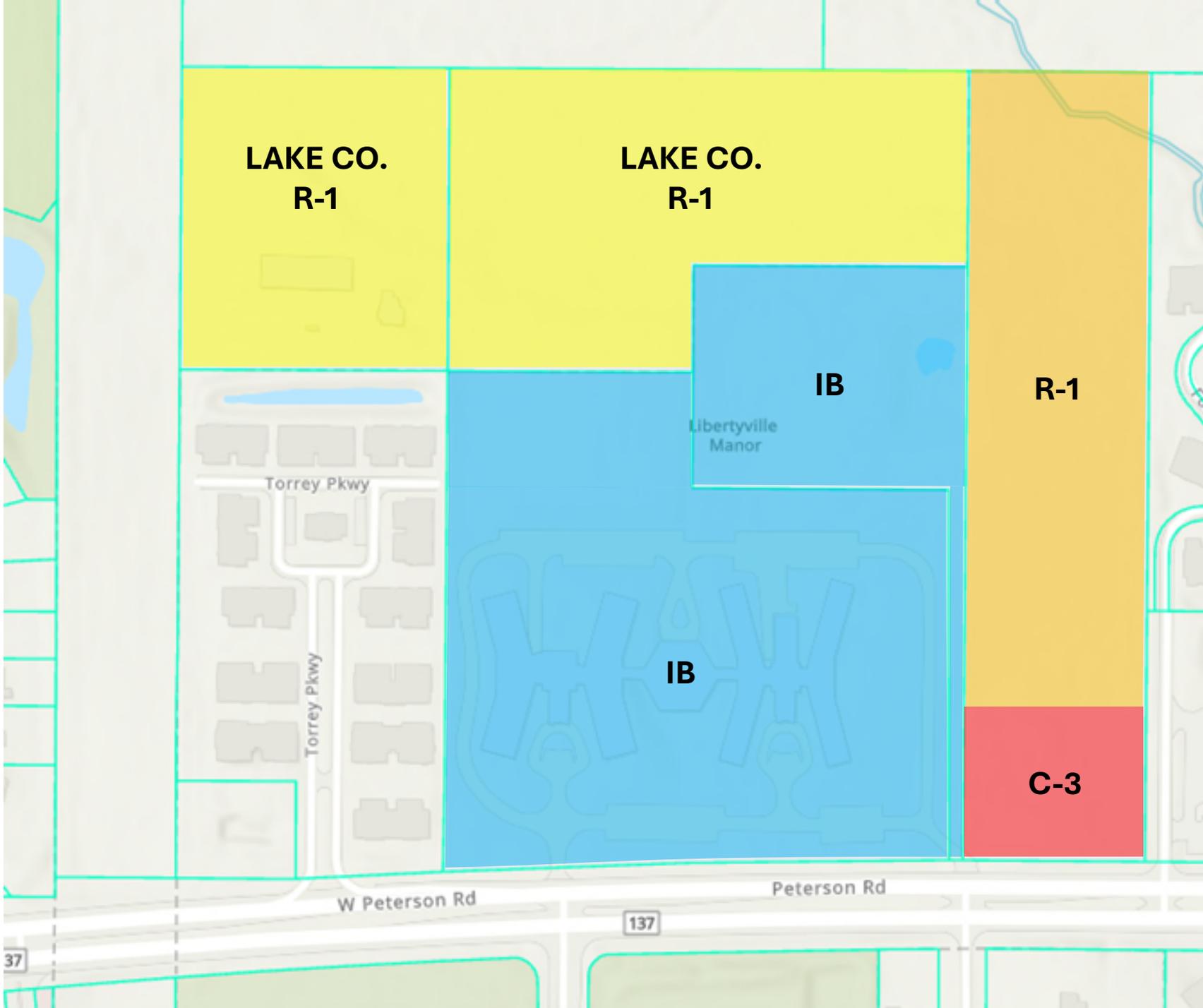


Table with 6 columns: No., DATE, REVISION DESCRIPTION, Scale, Drawn by, Checked by, Date, Project No., Sheet No. Row 1: 1, 3/31/25, REVISED OUTLOT GEOMETRY, 1\"/>



4201 Winfield Road
Warrenville, Illinois 60555
DESIGN FIRM # 184002012-0006
Tel. No. (630) 487-5550
www.kimley-horn.com

EXHIBIT D
ZONING EXHIBIT



**LAKE CO.
R-1**

**LAKE CO.
R-1**

IB

Libertyville
Manor

R-1

IB

C-3

Torrey Pkwy

Torrey Pkwy

W Peterson Rd

Peterson Rd

137

37

Traffic Impact Study

Proposed Greenway Chase Development

Libertyville, Illinois



Prepared For:



May 15, 2025

1. Executive Summary

This report summarizes the methodologies, results, and findings of a traffic impact study conducted by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA, Inc.) for a proposed residential development in Libertyville, Illinois. The site, which currently contains the Liberty Manor Extended Care facility, is located on the north side of Peterson Road (Illinois Route 137) opposite Elderberry Drive and Sunnyview Road. As proposed, the site will be redeveloped with a residential development consisting of 64 single-family homes and 70 age-restricted duplex units. Access to the development will be provided via the two existing full movement access roads off Peterson Road that align with Elderberry Drive and Sunnyview Road.

The objectives of the traffic study are as follows:

- Examine background traffic conditions including vehicular, pedestrian, bicycle, and public transportation conditions in the study area to establish a base condition.
- Assess the impact that the proposed development will have on traffic conditions in the area.
- Determine if any roadway or access improvements are necessary to accommodate traffic generated by the proposed development.

Vehicle, pedestrian, and bicycle counts were conducted during the weekday morning and evening peak periods at four intersections in the vicinity of the site in order to determine the existing traffic volumes during the general peak hours of commuter activity within each of these time periods. The intersections of Peterson Road with Forest Creek Lane, Sunnyview Road/Liberty Manor Extended Care east drive, Elderberry Lane/Liberty Manor Extended Care west drive, and Butterfield Square signalized access drive were included in this study.

With the addition of development-generated traffic as well as background growth, the following conclusions and improvements will be made:

- Outbound movements from the two access roads will be under stop sign control
- The eastern access road will provide one inbound lane and one outbound lane.
- The western access road will provide one inbound lane and two outbound lanes striped for an exclusive right-turn lane and a shared through/left-turn lane.
- The existing eastbound to northbound and westbound to southbound left-turn lanes on Peterson Road at its intersection with the west access road/Elderberry Drive will be buried within the existing median and extended to provide 215 feet of storage and 220 feet of taper consistent with IDOT's *Bureau of Design and Environment* (BDE) Manual requirements.

- An exclusive westbound to northbound right-turn lane on Peterson Road at its intersection with the west access road providing 215 feet of storage and 220 feet of taper.
- An exclusive eastbound to northbound left-turn lane on Peterson Road at its intersection with the east access road/Sunnyview Road will be buried within the existing median providing 215 feet of storage and 220 feet of taper.
- An exclusive westbound to southbound left-turn lane on Peterson Road at its intersection with the east access road/Sunnyview Road will be buried within the existing median. Given there is not enough distance to provide the required storage and taper, the exclusive left-turn lane will provide 150 feet of storage and 100 feet of taper.
- An exclusive westbound to northbound right-turn lane on Peterson Road at its intersection with the east access road. Given there is not enough frontage to provide the required storage and taper, the exclusive right-turn lane is proposed to provide 150 feet of storage and 95 feet of taper.
 - The two access roads that will serve the proposed development will help disperse the development-generated traffic, provide alternatives for the site traffic to enter and exit the development and improve emergency access.
 - Based on the results of the capacity analyses and taking into consideration the proposed geometric improvements, the traffic that will be generated by the proposed development will be accommodated by the area roadway system.

2. Introduction

This report summarizes the methodologies, results, and findings of a traffic impact study conducted by Kenig, Lindgren, O’Hara, Aboona, Inc. (KLOA, Inc.) for a proposed residential development in Libertyville, Illinois. The site, which currently contains the Liberty Manor Extended Care facility, is located on the north side of Peterson Road (Illinois Route 137) opposite Elderberry Drive and Sunnyview Road. As proposed, the site will be redeveloped with a residential development consisting of 64 single-family homes and 70 age-restricted duplex units. Access to the development will be provided via the two existing full movement access roads off Peterson Road that align with Elderberry Drive and Sunnyview Road.

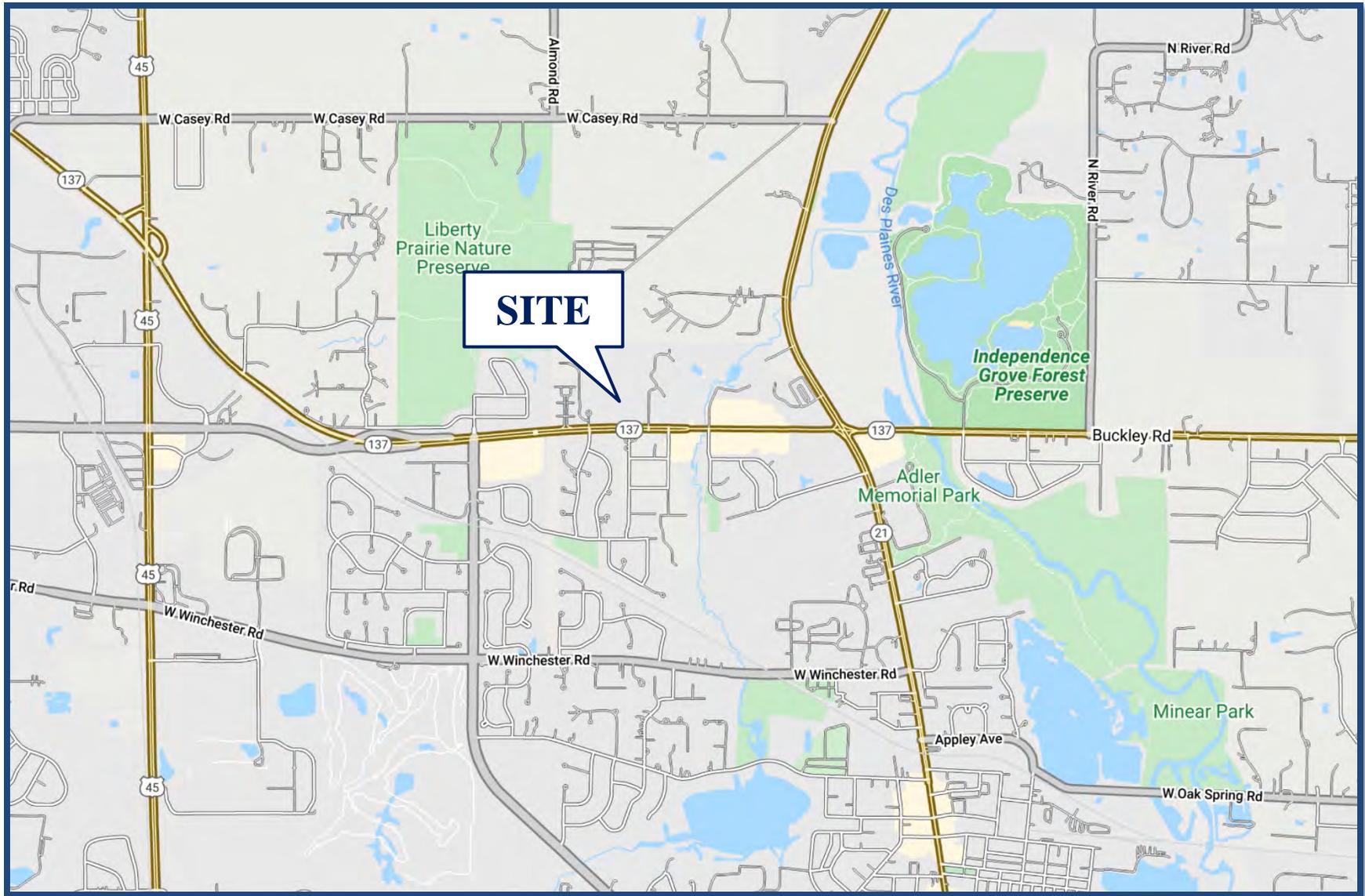
The purpose of this study was to examine background traffic conditions, assess the impact that the proposed development will have on traffic conditions in the area, and determine if any roadway or access improvements are necessary to accommodate traffic generated by the proposed development. **Figure 1** shows the location of the site in relation to the area roadway system. **Figure 2** shows an aerial view of the site.

The sections of this report present the following:

- Existing roadway conditions
- A description of the proposed development
- Directional distribution of the development traffic
- Vehicle trip generation for the development
- Future traffic conditions including access to the development
- Traffic analyses for the weekday morning and evening peak hours
- Recommendations with respect to adequacy of the site access and adjacent roadway system

Traffic capacity analyses were conducted for the weekday morning and evening peak hours for the following conditions:

1. Existing Traffic Conditions – Analyzes the capacity of the existing roadway system using peak hour traffic volumes from traffic counts conducted in 2024.
2. Year 2031 No-Build Conditions – Analyzes the capacity of the existing roadway system using existing traffic volumes increased by an ambient area growth factor not attributable to any particular development as well as the traffic to be generated by the remaining units within the Liberty Junction residential development located in the southeast quadrant of the intersection of Peterson Road with Milwaukee Avenue and the traffic to be generated by an affordable housing development to be located on the north side of Peterson Road opposite Bell Lane.
3. Year 2031 Total Projected Conditions – Analyzes the capacity of the future roadway system using the projected traffic volumes that include the Year 2031 no-build volumes and the traffic estimated to be generated by the proposed development.



Site Location

Figure 1



Aerial View of Site

Figure 2

*Proposed Greenway Chase Development
Libertyville, Illinois*

3. Existing Conditions

Existing transportation conditions in the vicinity of the site were documented based on field visits conducted by KLOA, Inc. in order to obtain a database for projecting future conditions. The following provides a description of the geographical location of the site, physical characteristics of the area roadway system including lane usage and traffic control devices, and existing peak hour traffic volumes.

Development Site

The site, which is currently occupied by the Liberty Manor Extended Care facility, is bounded by vacant land to the north, the Libertyville Tile & Carpet retail store to the east, Peterson Road to the south, and the Victoria Park townhome development to the west. Land uses in the vicinity of the site are primarily residential and retail. It should be noted that access to the site is currently served via two full movement access drives off Peterson Road located opposite Elderberry Lane and Sunnyview Road.

Existing Roadway System Characteristics

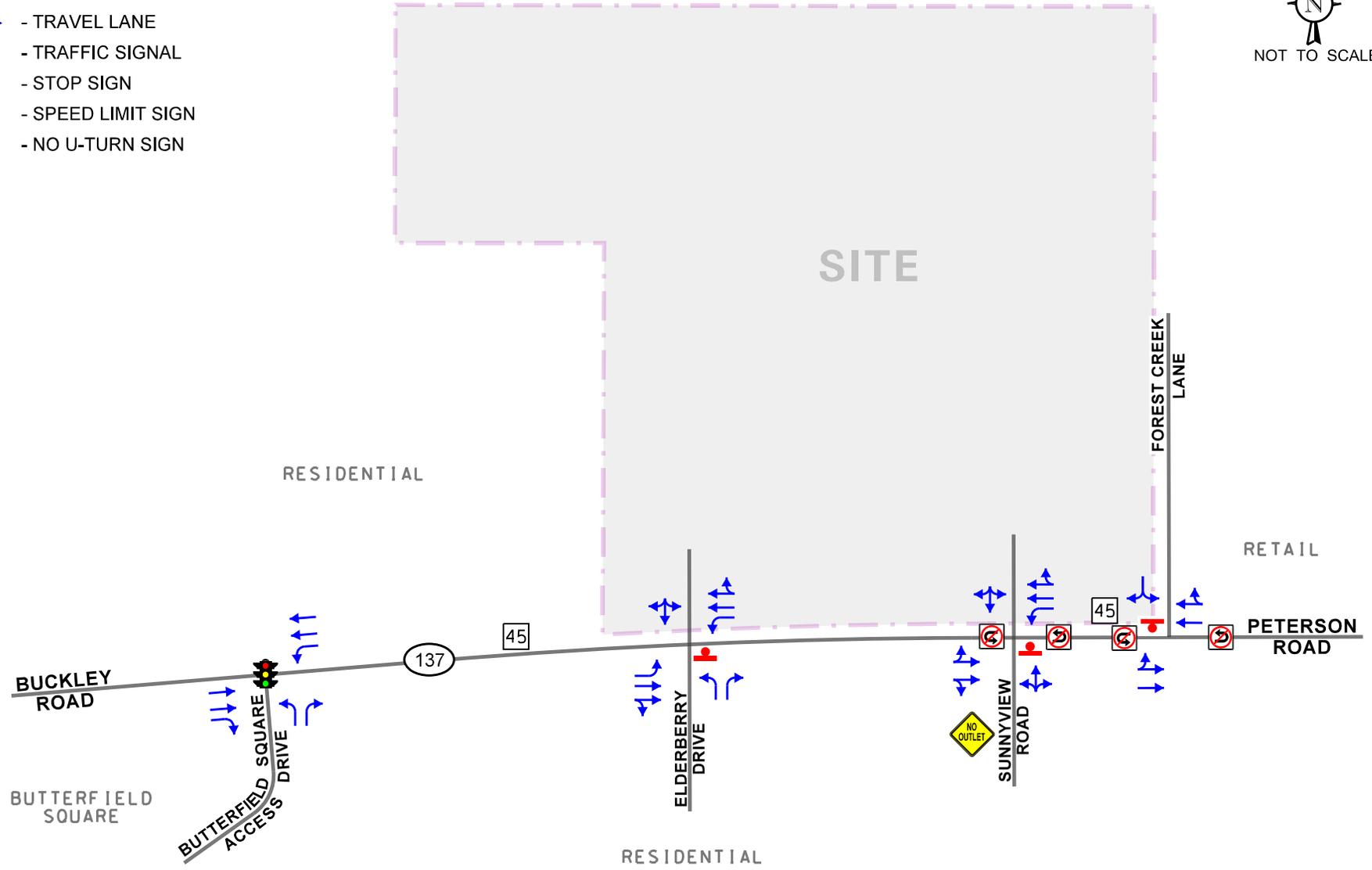
The characteristics of the existing roadways near the development are described below and illustrated in **Figure 3**.

Peterson Road (Illinois Route 137) is an east-west, principal arterial roadway that provides two lanes in each direction generally divided by a landscape median. At its unsignalized intersection with Forest Creek Lane, Peterson Road provides a through lane and a shared through/right-turn lane on the westbound approach and a shared through left-turn lane and a through lane on the eastbound approach. At its unsignalized intersection with Sunnyview Road/Liberty Manor Extended Care east access drive, Peterson Road provides a shared left/through and a through/right-turn lane on the eastbound approach. The westbound approach provides an exclusive left-turn lane, a through lane and a shared through/right-turn lane. At its unsignalized intersection with Elderberry Lane/Liberty Manor Extended Care west access drive, Peterson Road provides an exclusive left-turn lane, a through lane and a shared through/right-turn lane on both approaches. At its signalized intersection with the Butterfield Square full access drive, Peterson Road provides two through lanes and an exclusive right-turn lane on the eastbound approach and an exclusive left-turn lane and two through lanes on the westbound approach. Peterson Road is under the jurisdiction of the Illinois Department of Transportation (IDOT), is designated as a Strategic Regional Arterial (SRA) in the vicinity of the site and carries an Annual Average Daily Traffic (AADT) volume of 28,800 vehicles (IDOT 2023). Peterson Road has a posted speed limit of 45 mph.

Forest Creek Lane is a north-south, private roadway that extends north from Peterson Road and provides one lane in each direction. At its unsignalized intersection with Peterson Road, Forest Creek Lane provides one lane on the southbound approach is under stop sign control. Forest Creek Lane has a posted speed limit of 15 mph.

LEGEND

-  - TRAVEL LANE
-  - TRAFFIC SIGNAL
-  - STOP SIGN
-  - SPEED LIMIT SIGN
-  - NO U-TURN SIGN



Proposed
Greenway Chase
Libertyville, Illinois

Existing Roadway Characteristics

KLOA
Kenig, Lindgren, O'Hara, Aboona, Inc.
Job No: 24-280 Figure: 3

Sunnyview Road is a north-south, local roadway that extends south from Peterson Road for approximately 1,225 feet where the road is gated and provides one lane in each direction. At its unsignalized intersection with Peterson Road, Sunnyview Road provides one inbound lane and one outbound lane under stop sign control. Opposite Sunnyview Road is the Liberty Manor Extended care east access drive which provides one inbound lane and one outbound lane. Sunnyview Road is under the jurisdiction of Libertyville Township. South of the gate, the road is under the jurisdiction of the Village of Libertyville.

Elderberry Lane is a north-south, local roadway that extends south from Peterson Road to New Castle Drive and provides one lane in each direction. At its unsignalized intersection with Peterson Road, Sunnyview Road provides one inbound lane and one outbound lane under stop sign control. Opposite Sunnyview Road is the Liberty Manor Extended care west access drive which provides one inbound lane and one outbound lane. Sunnyview Road is under the jurisdiction of the Village of Libertyville.

Butterfield Square East Access Drive is a north-south access drive serving the Butterfield Square shopping center. At its signalized intersection with Peterson Road, the access drive provides one inbound lane and two outbound lanes striped for an exclusive left-turn lane and an exclusive right-turn lane.

Existing Traffic Volumes

In order to determine current traffic conditions in the vicinity of the site, KLOA, Inc. conducted peak period traffic counts using Miovision Scout Video Collection Units on Tuesday, November 12, 2024 during the weekday morning (7:00 A.M. to 9:00 A.M.) and weekday evening (4:00 P.M. to 6:00 P.M.) peak periods at the following intersections:

- Peterson Road with Forest Creek Lane
- Peterson Road with Sunnyview Road/Liberty Manor Extended Care east drive
- Peterson Road with Elderberry Lane/Liberty Manor Extended Care west drive
- Peterson Road with Butterfield Square signalized access drive

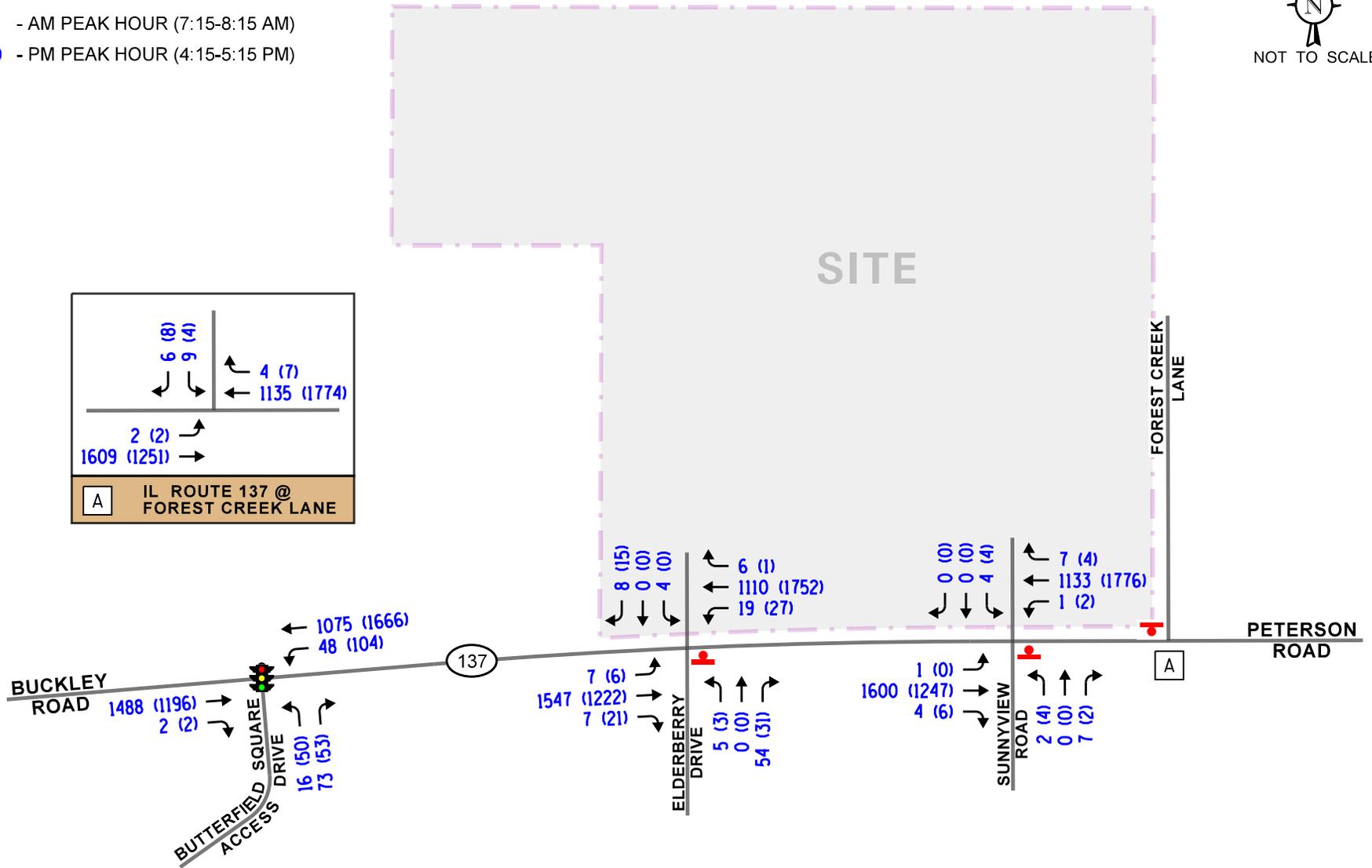
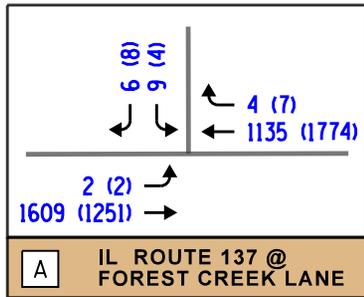
The results of the traffic counts indicated that the weekday morning peak hour of traffic occurs from 7:15 A.M. to 8:15 A.M. and the weekday evening peak hour of traffic occurs from 4:15 P.M. to 5:15 P.M. Copies of the traffic count summary sheets are included in the Appendix. **Figure 4** illustrates the existing peak hour vehicle traffic volumes.

LEGEND

- 00** - AM PEAK HOUR (7:15-8:15 AM)
- (00)** - PM PEAK HOUR (4:15-5:15 PM)



NOT TO SCALE



Proposed
Greenway Chase
Libertyville, Illinois

Existing Traffic Volumes



Job No: 24-280

Figure: 4

Crash Summary

KLOA, Inc. obtained crash data for the most recent available past five years (2019 to 2023) at the intersections of Peterson Road with Forest Creek Lane, Sunnyview Road, and Elderberry Drive. A review of the crash data revealed no crashes occurred at the intersection of Peterson Road with Sunnyview Road during the review period. Further, no fatalities were reported at any of the study area intersections during the review period. A summary of the crash data for the intersections of Peterson Road with Forest Creek Lane and Elderberry Drive are shown in **Tables 1** and **2**.¹

Table 1

PETERSON ROAD WITH FOREST CREEK LANE- CRASH SUMMARY

Year	Type of Crash Frequency						Total
	Angle	Object	Rear End	Sideswipe	Turning	Other	
2019	0	0	0	0	0	0	0
2020	0	0	0	0	0	0	0
2021	0	0	1	0	0	0	1
2022	0	0	0	0	0	0	0
2023	0	0	0	0	0	0	0
Total	0	0	1	0	0	0	1
Average/Year	0	0	<1.0	0	0	0	<1.0

Table 2

PETERSON ROAD WITH ELDERBERRY DRIVE/LIBERTY MANOR WEST DRIVE - CRASH SUMMARY

Year	Type of Crash Frequency						Total
	Angle	Object	Rear End	Sideswipe	Turning	Other	
2019	0	0	0	0	0	0	0
2020	0	0	0	0	0	0	0
2021	0	0	0	0	1	0	1
2022	0	0	1	0	1	0	2
2023	0	0	0	0	1	1	2
Total	0	0	1	0	3	1	5
Average/Year	0	0	<1.0	0	<1.0	<1.0	1.0

¹ IDOT DISCLAIMER: The motor vehicle crash data referenced herein was provided by the Illinois Department of Transportation. Any conclusions drawn from analysis of the aforementioned data are the sole responsibility of the data recipient(s).

4. Traffic Characteristics of the Proposed Development

In order to properly evaluate future traffic conditions in the surrounding area, it was necessary to determine the traffic characteristics of the proposed development, including the directional distribution and volumes of traffic that it will generate.

Proposed Site and Development Plan

As proposed, the site will be redeveloped with a residential development consisting of 64 single-family homes and 70 age-restricted duplex units with construction is anticipated to commence on the Year 2026. Access to the development will be provided via the two existing full movement access roads located on Peterson Road opposite Elderberry Drive and Sunnyview Road. The east access road will provide one inbound lane and one outbound lane with outbound movements under stop sign control. The west access drive will provide one inbound lane and two outbound lanes striped for a shared left/through lane and an exclusive right-turn lane. Outbound movements will be under stop sign control. As part of the development, the following geometric improvements on Peterson Road will be constructed.

- The existing eastbound to northbound and westbound to southbound left-turn lanes on Peterson Road at its intersection with the west access road/Elderberry Drive will be buried within the existing median and extended to provide 215 feet of storage and 220 feet of taper consistent with IDOT's *Bureau of Design and Environment* (BDE) Manual requirements.
- An exclusive westbound to northbound right-turn lane on Peterson Road at its intersection with the west access road providing 215 feet of storage and 220 feet of taper.
- An exclusive eastbound to northbound left-turn lane on Peterson Road at its intersection with the east access road/Sunnyview Road will be buried within the existing median providing 215 feet of storage and 220 feet of taper.
- An exclusive westbound to southbound left-turn lane on Peterson Road at its intersection with the east access road/Sunnyview Road will be buried within the existing median. Given there is not enough distance to provide the required storage and taper, the exclusive left-turn lane will provide 150 feet of storage and 100 feet of taper.
- An exclusive westbound to northbound right-turn lane on Peterson Road at its intersection with the east access road. Given there is not enough frontage to provide the required storage and taper, the exclusive right-turn lane is proposed to provide 150 feet of storage and 95 feet of taper.

A copy of the preliminary site plan/geometric plan is included in the Appendix.

Directional Distribution

The directions from which employees and trucks will approach and depart the site were estimated based on existing travel patterns, as determined from the traffic counts. **Figure 5** illustrates the directional distribution of the site-generated traffic. Figure 5 also shows the distance, in feet, between the existing and proposed access intersections.

Development-Generated Traffic Volumes

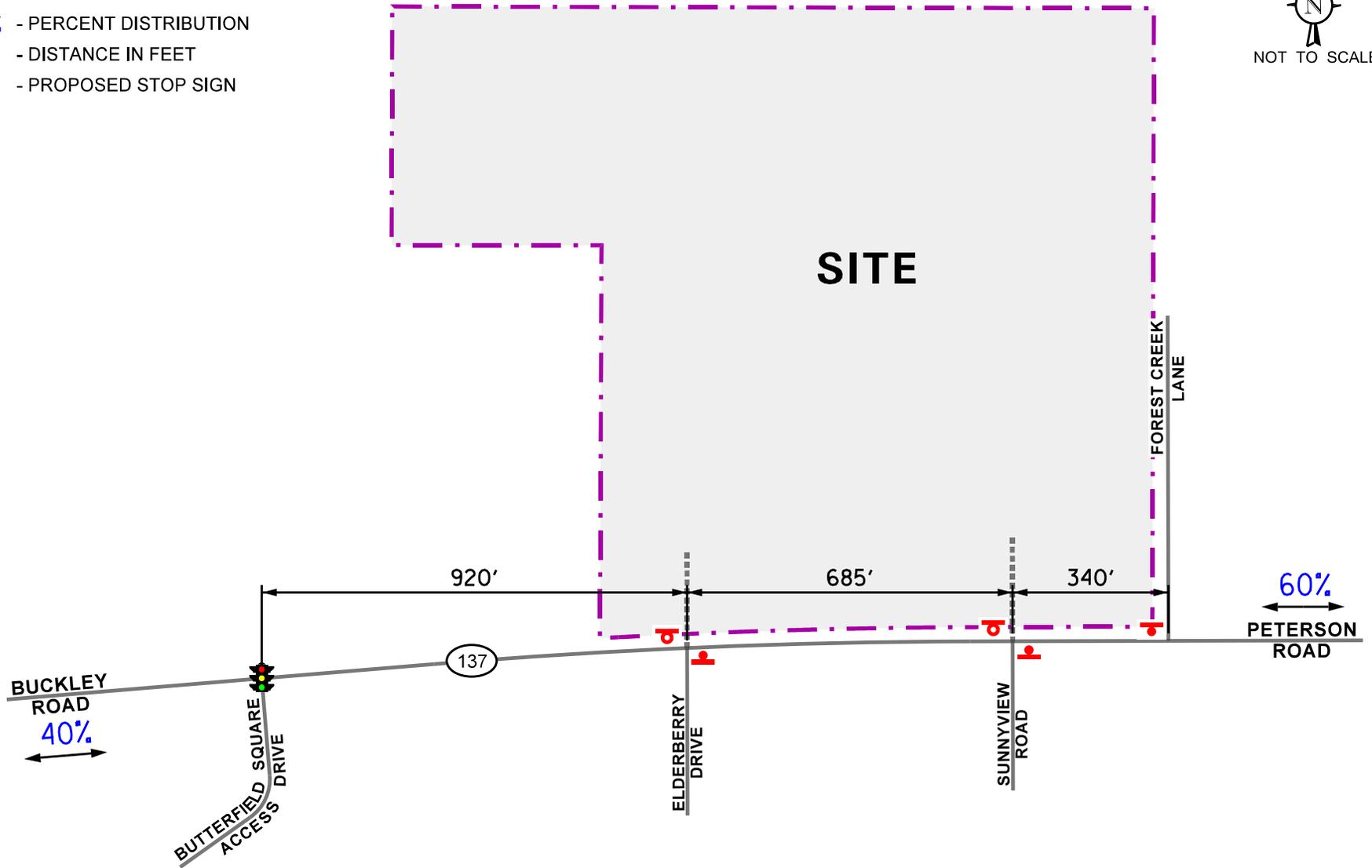
The number of peak hour trips estimated to be generated by the proposed residential development was based on vehicle trip generation rates contained in *Trip Generation Manual*, 11th Edition, published by the Institute of Transportation Engineers (ITE). The “Single-Family Detached Housing” (Land-Use Code 210) and the “Senior Adult Housing – Single-Family” (Land Use Code 251) rates were used to determine the traffic to be generated by the development. The fitted curve equation was utilized to calculate the estimated number of trips. **Table 3** summarizes the trips projected to be generated by the development during the peak hours and on a daily basis. Copies of the ITE trip generation sheets are included in the Appendix.

Table 3
ESTIMATED PEAK HOUR AND DAILY TRIP GENERATION

ITE Land-Use Code	Type/Size	Weekday Morning Peak Hour			Weekday Evening Peak Hour			Daily Trips		
		In	Out	Total	In	Out	Total	In	Out	Total
210	Single-Family Detached (64 Units)	12	38	50	41	24	65	335	335	670
251	Senior Adult Housing – Single-Family (70 Units)	10	20	30	20	14	34	219	219	438
	Total	22	58	80	61	38	99	554	554	1,108

LEGEND

- 00% - PERCENT DISTRIBUTION
- 00' - DISTANCE IN FEET
-  - PROPOSED STOP SIGN



Proposed
Greenway Chase
Libertyville, Illinois

Directional Distribution



Job No: 24-280

Figure: 5

5. Projected Traffic Conditions

The total projected traffic volumes include the existing traffic volumes, increase in background traffic due to growth, and the traffic estimated to be generated by the proposed subject development.

Development Traffic Assignment

The estimated weekday morning and evening peak hour traffic volumes that will be generated by the proposed development were assigned to the roadway system in accordance with the previously described directional distribution (Figure 5). The total new traffic assignment for the development is illustrated in **Figure 6**.

Background (No-Build) Traffic Conditions

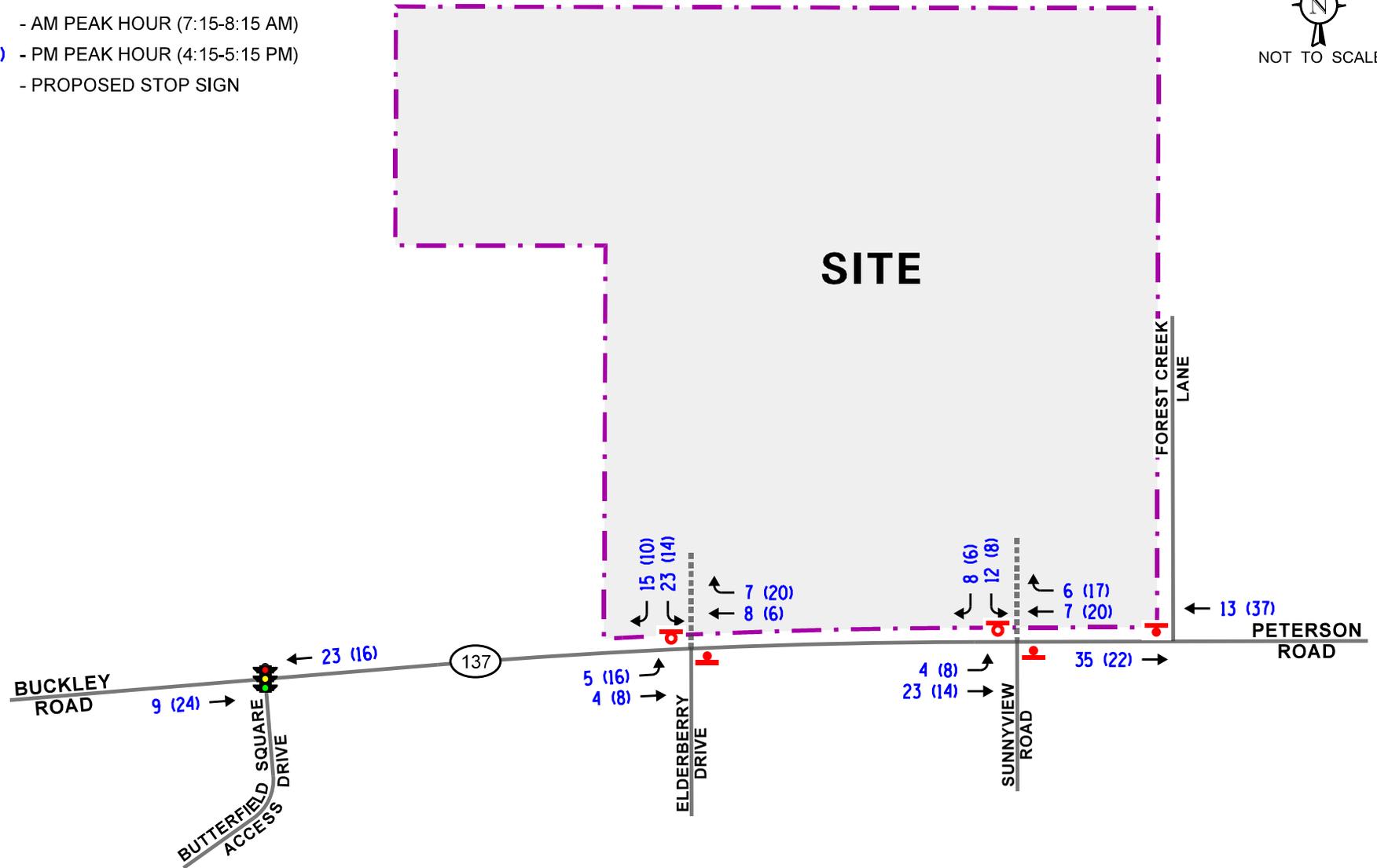
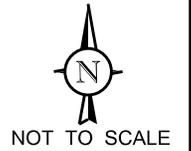
In the addition to the traffic to be generated by the proposed development, the study also considered the following additional regional growth and other developments proposed in the area:

- The existing traffic volumes (Figure 4) were increased by a regional growth factor to account for the increase in existing traffic related to regional growth in the area (i.e., not attributable to any planned development). Based on AADT projections provided by the Chicago Metropolitan Agency for Planning (CMAP), the existing traffic volumes were increased by an annually compounded growth rate of 0.3 percent per year for seven years for a total of three (3) percent. A copy of the CMAP projections letter is included in the Appendix.
- The remaining units to be sold and occupied (approximately 20 units) of the currently under construction residential development located in the southwest quadrant of the intersection of Peterson Road with Milwaukee Avenue (Illinois Route 21). This additional traffic was based on the traffic impact study for the development dated October 11, 2021 prepared by KLOA, Inc.
- The traffic volumes to be generated by a proposed residential development that will contain 34 affordable housing residential units for senior active adults and approximately 10,500 square feet of office space to be located on the north side of Peterson Road opposite Bell Lane. The traffic to be generated by this development was based on the traffic study dated December 16, 2022 prepared by KLOA, Inc.

Copies of the site traffic assignment for the two aforementioned developments are included in the Appendix. The Year 2031 no-build traffic volumes are illustrated in **Figure 7**.

LEGEND

- 00** - AM PEAK HOUR (7:15-8:15 AM)
- (00)** - PM PEAK HOUR (4:15-5:15 PM)
-  - PROPOSED STOP SIGN



Proposed
Greenway Chase
Libertyville, Illinois

Site-Generated Traffic Volumes



Job No: 24-280

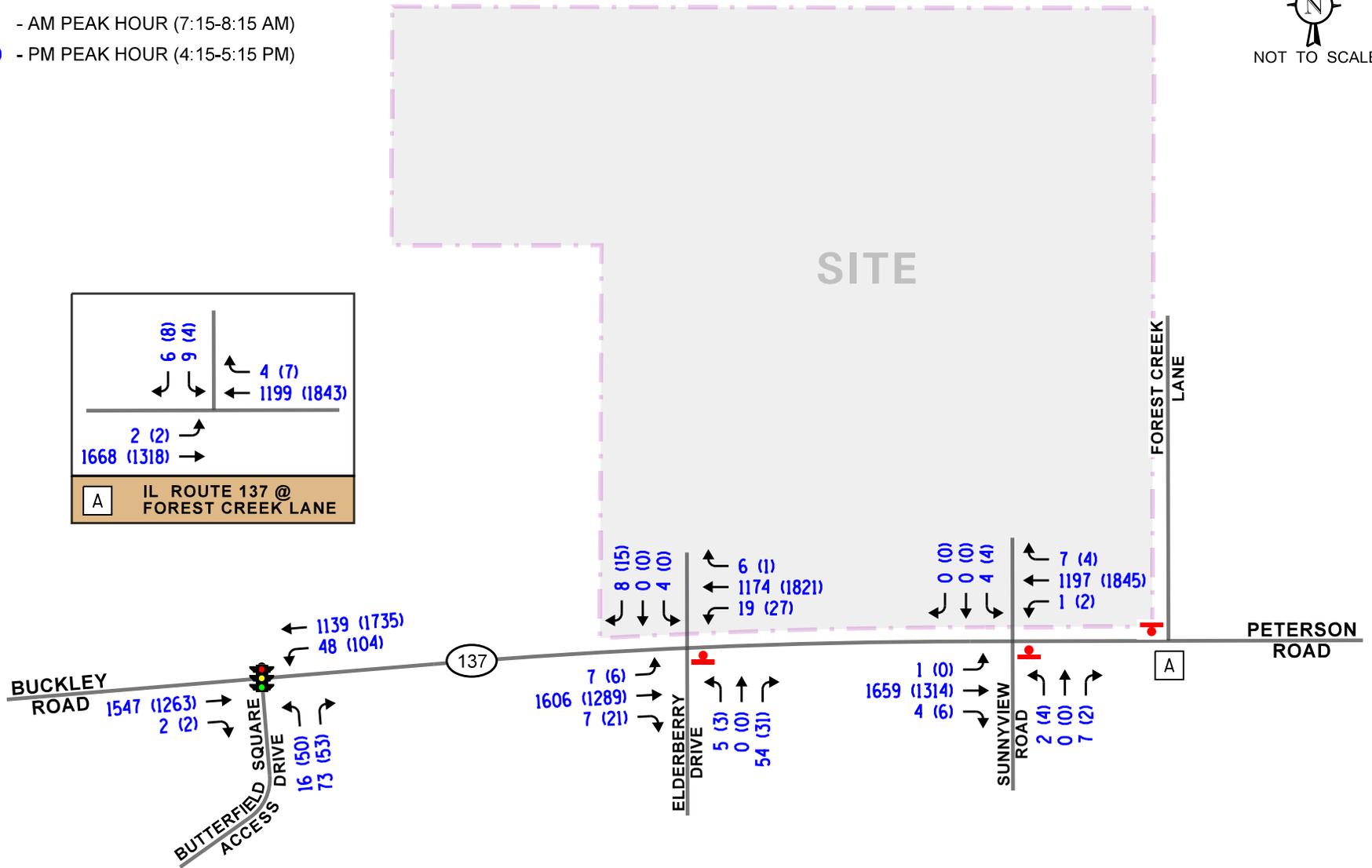
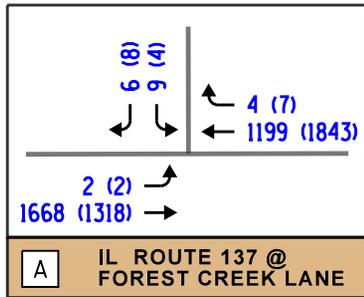
Figure: 6

LEGEND

- 00** - AM PEAK HOUR (7:15-8:15 AM)
- (00)** - PM PEAK HOUR (4:15-5:15 PM)



NOT TO SCALE



Proposed
Greenway Chase
Libertyville, Illinois

Year 2031 No-Build Traffic Volumes



Job No: 24-280

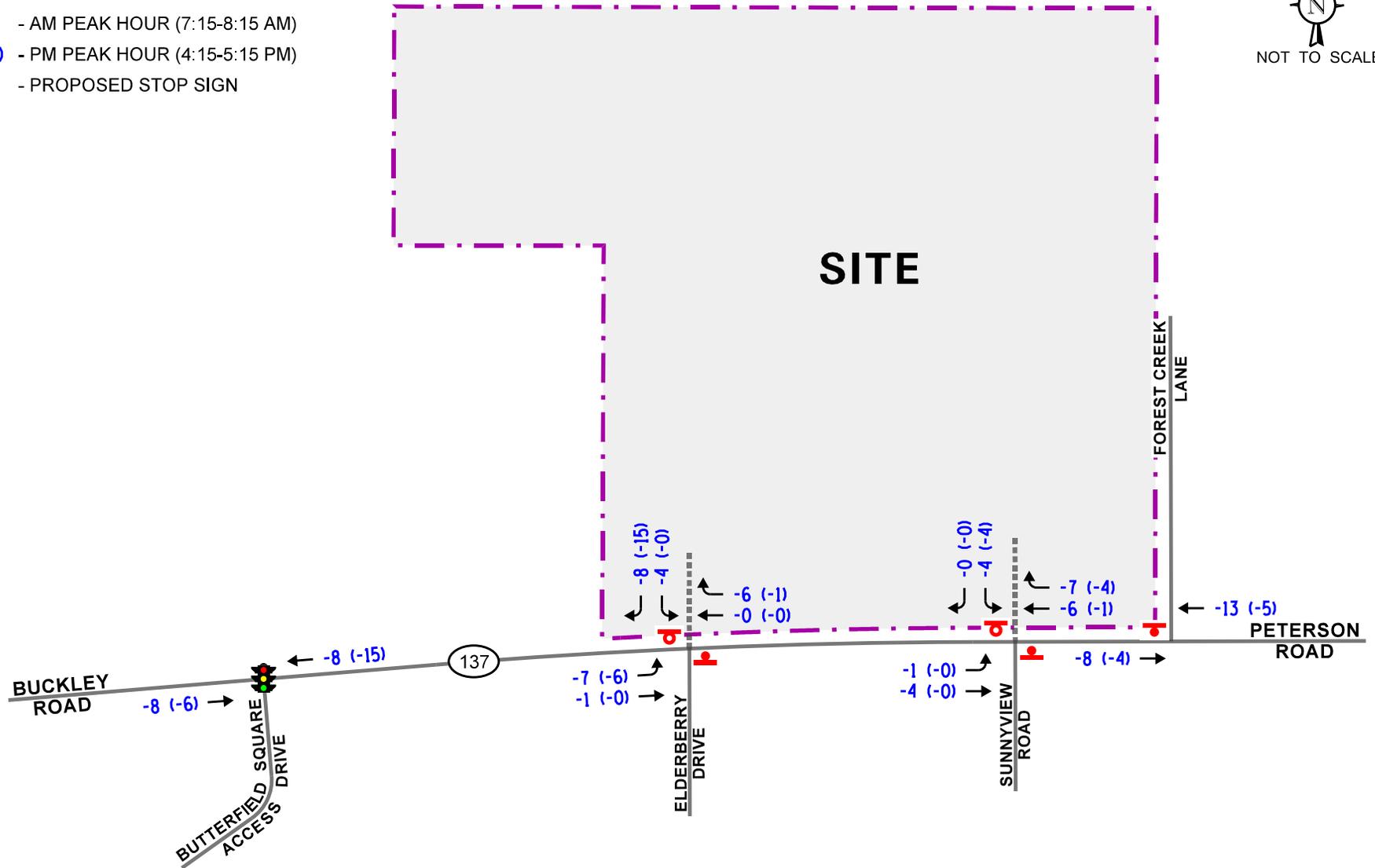
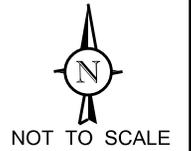
Figure: 7

Total Projected Traffic Volumes

It is important to note that the traffic volumes generated by the existing Liberty Manor Extended Care facility will be eliminated as a result of the development. A figure depicting the removal of the existing traffic volumes generated by the extended care facility is shown in **Figure 8**. The development-generated traffic (Figure 6) was added to the Year 2031 no-build traffic volumes (Figure 7) and to Figure 8 (removal of existing site-generated traffic volumes) to determine the Year 2031 total projected traffic volumes, as shown in **Figure 9**.

LEGEND

- 00** - AM PEAK HOUR (7:15-8:15 AM)
- (00)** - PM PEAK HOUR (4:15-5:15 PM)
-  - PROPOSED STOP SIGN



Proposed
Greenway Chase
Libertyville, Illinois

Existing Extended Care Traffic Generation Removal



Job No: 24-280

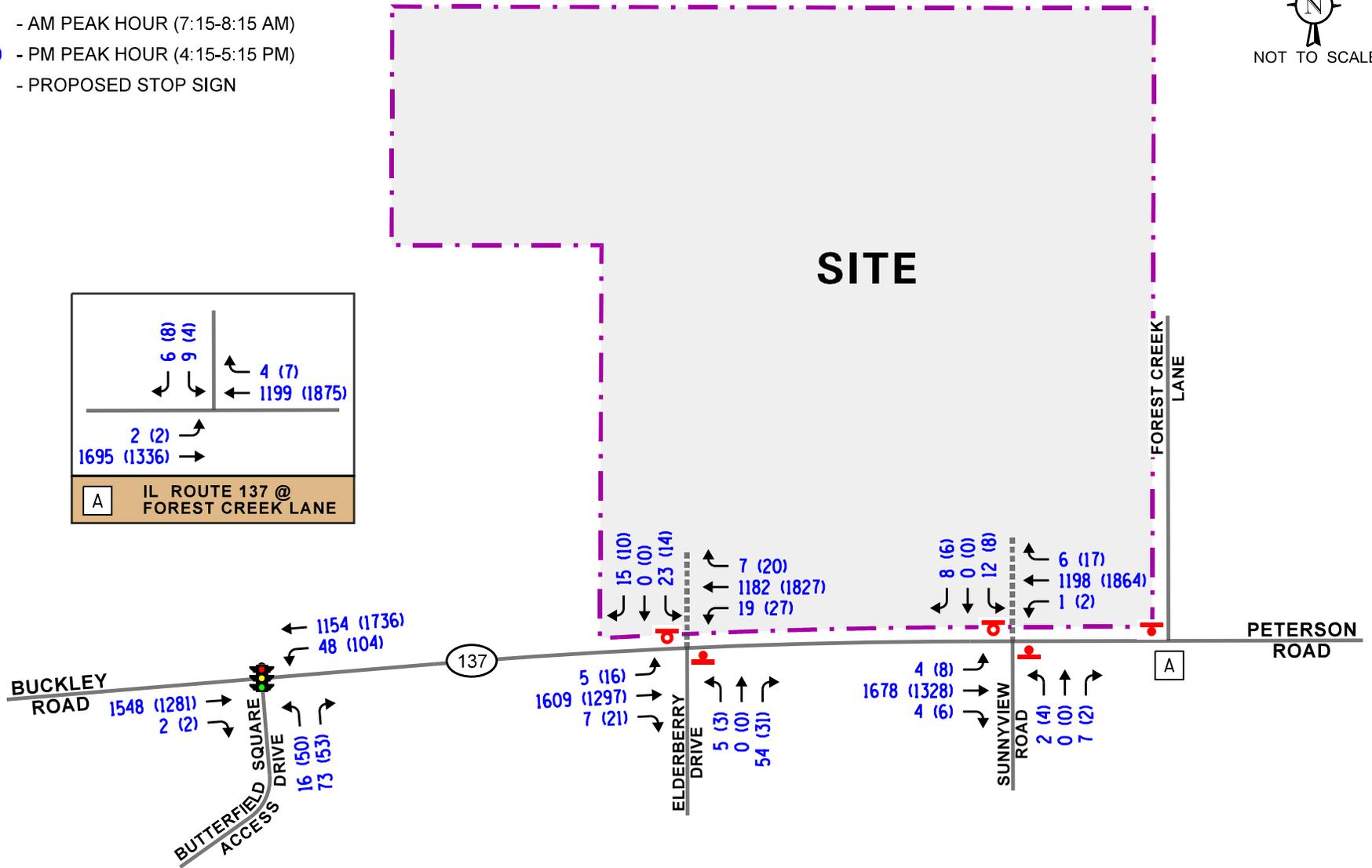
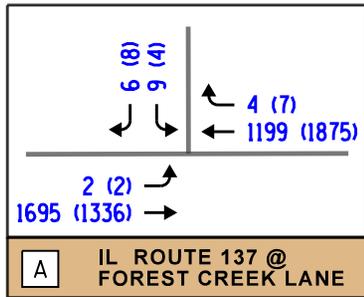
Figure: 8

LEGEND

- 00 - AM PEAK HOUR (7:15-8:15 AM)
- (00) - PM PEAK HOUR (4:15-5:15 PM)
-  - PROPOSED STOP SIGN



NOT TO SCALE



Proposed
Greenway Chase
Libertyville, Illinois

Year 2031 Total Traffic Volumes



Job No: 24-280

Figure: 9

6. Traffic Analysis and Recommendations

The following provides an evaluation conducted for the weekday morning and evening peak hours. The analysis includes conducting capacity analyses to determine how well the roadway system and access drives are projected to operate and whether any roadway improvements or modifications are required.

Traffic Analyses

Roadway and adjacent or nearby intersection analyses were performed for the weekday morning and weekday evening peak hours for the existing, Year 2031 no-build, and Year 2031 total projected traffic volumes.

The traffic analyses were performed using the methodologies outlined in the Transportation Research Board's *Highway Capacity Manual (HCM)*, 7th Edition and analyzed using Synchro/SimTraffic 12 software. The analysis for the traffic-signal controlled intersections were accomplished using field-measured cycle lengths and phasings to determine the average overall vehicle delay and levels of service.

The analyses for the unsignalized intersections determine the average control delay to vehicles at an intersection. Control delay is the elapsed time from a vehicle joining the queue at a stop sign (includes the time required to decelerate to a stop) until its departure from the stop sign and resumption of free flow speed. The methodology analyzes each intersection approach controlled by a stop sign and considers traffic volumes on all approaches and lane characteristics.

The ability of an intersection to accommodate traffic flow is expressed in terms of level of service, which is assigned a letter from A to F based on the average control delay experienced by vehicles passing through the intersection. The *Highway Capacity Manual* definitions for levels of service and the corresponding control delay for signalized intersections and unsignalized intersections are included in the Appendix of this report.

Summaries of the traffic analysis results showing the level of service and overall intersection delay (measured in seconds) for the existing, Year 2031 no-build, and Year 2031 total projected conditions are presented in **Tables 4** through **7**. A discussion of each intersection follows. Summary sheets for the capacity analyses are included in the Appendix.

Table 4

PETERSON ROAD WITH BUTTERFIELD SQUARE ACCESS DRIVE – SIGNALIZED

	Peak Hour	Eastbound		Westbound		Northbound		Overall
		T	R	L	T	L	R	
Existing Conditions	Weekday Morning	B 13.1	A 1.0	E 76.6	A 5.2	E 55.1	B 14.1	B 11.4
		B – 13.1		A – 8.3		C – 21.4		
	Weekday Evening	A 9.6	A 2.0	E 75.0	A 3.9	E 72.8	C 20.2	A 9.9
		A – 9.6		A – 8.1		D – 45.8		
2031 No-Build Conditions	Weekday Morning	B 13.6	A 1.0	E 76.6	A 5.4	E 55.1	B 14.1	B 11.6
		B – 13.6		A – 8.3		C – 21.4		
	Weekday Evening	A 10.0	A 2.0	E 75.0	A 4.1	E 72.8	C 20.2	B 10.1
		A – 10.0		A – 8.1		D – 45.8		
2031 Projected Conditions	Weekday Morning	B 13.6	A 1.0	E 76.6	A 5.5	E 55.1	B 14.1	B 11.6
		B – 13.6		A – 8.3		C – 21.4		
	Weekday Evening	B 10.1	A 2.0	E 75.0	A 4.1	E 72.8	C 20.2	B 10.1
		B – 10.1		A – 8.1		D – 45.8		
Letter denotes Level of Service L – Left Turn R – Right Turn Delay is measured in seconds. T – Through								

Table 5

CAPACITY ANALYSIS RESULTS – EXISTING CONDITIONS – UNSIGNALIZED

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
Peterson Road with Forest Creek Lane				
• Eastbound Left/Through	B	14.8	C	16.6
• Southbound Approach	C	23.8	D	30.9
Peterson Road with Sunnyview Road/East Access Drive				
• Eastbound Left/Through	B	11.1	A	0.0
• Westbound Left Turn	B	14.3	B	12.0
• Northbound Approach	D	25.6	D	32.7
• Southbound Approach	D	34.0	F	71.9
Peterson Road with Elderberry Drive/West Access Drive				
• Eastbound Left Turn	B	12.0	C	16.4
• Westbound Left Turn	C	15.9	B	12.3
• Northbound Approach	C	22.1	C	17.5
• Southbound Approach	C	23.0	C	19.3
LOS = Level of Service Delay is Measured in Seconds				

Table 6

CAPACITY ANALYSIS RESULTS – 2031 NO BUILD CONDITIONS – UNSIGNALIZED

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
Peterson Road with Forest Creek Lane				
• Eastbound Left/Through	C	15.5	C	17.4
• Southbound Approach	D	25.4	D	33.3
Peterson Road with Sunnyview Road/East Access Drive				
• Eastbound Left/Through	B	11.4	A	0.0
• Westbound Left Turn	B	14.8	B	12.4
• Northbound Approach	D	27.1	D	35.4
• Southbound Approach	E	36.8	F	80.1
Peterson Road with Elderberry Drive/West Access Drive				
• Eastbound Left Turn	B	12.5	C	17.2
• Westbound Left Turn	C	16.6	B	12.8
• Northbound Approach	C	23.3	C	18.4
• Southbound Approach	C	24.6	C	20.2
LOS = Level of Service Delay is Measured in Seconds				

Table 7

CAPACITY ANALYSIS RESULTS – 2031 FUTURE CONDITIONS – UNSIGNALIZED

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
Peterson Road with Forest Creek Lane				
• Eastbound Left/Through	C	15.5	C	17.7
• Southbound Approach	D	25.5	D	34.4
Peterson Road with Sunnyview Road/East Access Drive				
• Eastbound Left Turn	B	11.5	C	18.0
• Westbound Left Turn	B	15.0	B	12.5
• Northbound Approach	D	27.8	E	37.8
• Southbound Approach	D	30.0	F	63.3
Peterson Road with Elderberry Drive/West Access Drive				
• Eastbound Left Turn	B	12.5	C	18.0
• Westbound Left Turn	C	16.6	B	12.9
• Northbound Approach	C	23.4	C	18.8
• Southbound Approach	E	38.6	F	77.3
LOS = Level of Service Delay is Measured in Seconds				

Discussion and Recommendations

The following summarizes how the intersections are projected to operate and identifies any roadway and traffic control improvements necessary to accommodate the development-generated traffic.

Peterson Road with Butterfield Square Access Drive

The results of the capacity analyses indicate that the intersection currently operates at an overall Level of Service (LOS) B during the weekday morning peak hour and LOS A during the weekday evening peak hour. Further, all turning movements operate at LOS E or better during both peak hours and Peterson Road through movements operate at LOS A. Under Year 2031 no-build conditions, this intersection is projected to continue to operate at the same overall LOS as under existing conditions during both peak hours.

Under Year 2031 total projected conditions, this intersection is projected to operate at an overall LOS B during the weekday morning and weekday evening peak hour with increases in delay of less than one second over existing and no-build conditions. Overall, the proposed development is projected to increase the volume of traffic traversing this intersection by less than one percent. As such, the intersection has sufficient reserve capacity to accommodate the traffic to be generated by the development and no roadway improvements or traffic control modifications are required.

Peterson Road with Forest Creek Lane

The results of the capacity analyses indicate that the southbound approach at this intersection operates at LOS C during the weekday morning peak hour and LOS D during the weekday evening peak hour. Eastbound left-turn movements currently operate at LOS C or better during the peak hours.

Under Year 2031 no build and total projected conditions, the southbound approach is projected to continue operating at LOS D with minimal increases in delay. Further, the eastbound left-turn movement is projected to operate at LOS C during both peak hours. As such, the traffic estimated to be generated by the proposed development will have a limited impact on the operations of this intersection.

Peterson Road with Sunnyview Road and the East Access Road

The results of the capacity analysis indicate that under existing conditions, the critical turning movements and the northbound and southbound approaches are operating at acceptable LOS with the exception of the southbound approach during the weekday evening peak hour which operates at LOS F. It is important to note that the LOS F is typical and expected when a minor road intersects a major arterial such as Peterson Road. Furthermore, it is important to note that Peterson Road provides a 28-foot-wide median break that can accommodate northbound and southbound two-part left turns.

Under Year 2031 no build conditions, the northbound approach will continue operating at the same LOS and the southbound approach will operate at LOS E and F during the weekday morning and evening peak hours respectively.

As indicated earlier and as part of the development, an eastbound left turn lane will be provided within the existing median to accommodate the projected traffic volumes. Furthermore, an exclusive westbound right-turn lane will also be provided. Based on the results of the capacity analyses for the Year 2031 total projected conditions, the eastbound and westbound left-turn movements will operate at LOS C or better. The northbound approach will operate at LOS D and E during the weekday morning and evening peak hours, respectively. The southbound approach will operate at LOS D and F during the weekday morning and evening peak hours, respectively. This level of service is typical and expected at the unsignalized intersection of an access drive or local roadway with a major arterial roadway such as Peterson Road. While outbound vehicles may experience some delay, this approach is projected to operate with a volume to capacity (v/c) ratio of less than one and 95th percentile queues of less than one vehicle indicating that vehicles will be able to turn onto Peterson Road. In addition, the results of the capacity analyses do not take into consideration gaps created in the Peterson Road traffic stream by the signalized intersection of Peterson Road with the Butterfield Square access road to the west. As such, this intersection has sufficient reserve capacity to accommodate the traffic to be generated by the development and no additional roadway improvements or traffic control modifications are required.

Peterson Road with Elderberry Drive and the West Access Road

The results of the capacity analysis indicate that under existing conditions, the critical turning movements and the northbound and southbound approaches are operating at acceptable LOS.

Under Year 2031 no build conditions, all of the critical turning movements as well as the north and south approaches will continue operating at the same LOS as under existing conditions.

As indicated earlier and as part of the development, the existing eastbound left-turn lane will be extended to meet the requirements set forth in the BDE Manual and an exclusive westbound right-turn lane will also be provided. Based on the results of the capacity analyses for the Year 2031 total projected conditions, the eastbound and westbound left turn movements from Peterson Road will operate at LOS C or better. The northbound approach will operate at LOS C during the weekday morning and evening peak hours. The southbound approach will operate at LOS E and F during the weekday morning and evening peak hours, respectively. As discussed previously, this level of service is typical and expected at the unsignalized intersection of an access drive or local roadway with a major arterial roadway such as Peterson Road. While outbound vehicles may experience some delay, this approach is projected to operate with a v/c ratio of less than one and 95th percentile queues of one vehicle or less indicating that vehicles will be able to turn onto Peterson Road. In addition, the results of the capacity analyses do not take into consideration gaps created in the Peterson Road traffic stream by the signalized intersection of Peterson Road with the Butterfield Square access road to the west. As such, this intersection has sufficient reserve capacity to accommodate the traffic to be generated by the development and no additional roadway improvements or traffic control modifications are required.

7. Conclusion

Based on the preceding analyses and recommendations, the following conclusions have been made:

- As proposed, the site will be redeveloped with a residential development consisting of 64 single-family homes and 70 age-restricted duplex units.
- Access to the redevelopment will be provided via the two existing full movement access roads on Peterson Road opposite Elderberry Drive and Sunnyview Road.

As part of the development, the following geometric improvements on Peterson Road will be constructed.

- The existing eastbound to northbound and westbound to southbound left-turn lanes on Peterson Road at its intersection with the west access road/Elderberry Drive will be buried within the existing median and extended to provide 215 feet of storage and 220 feet of taper consistent with IDOT's *Bureau of Design and Environment* (BDE) Manual requirements.
- An exclusive westbound to northbound right-turn lane on Peterson Road at its intersection with the west access road providing 215 feet of storage and 220 feet of taper.
- An exclusive eastbound to northbound left-turn lane on Peterson Road at its intersection with the east access road/Sunnyview Road will be buried within the existing median providing 215 feet of storage and 220 feet of taper.
- An exclusive westbound to southbound left-turn lane on Peterson Road at its intersection with the east access road/Sunnyview Road will be buried within the existing median. Given there is not enough distance to provide the required storage and taper, the exclusive left-turn lane will provide 150 feet of storage and 100 feet of taper.
- An exclusive westbound to northbound right-turn lane on Peterson Road at its intersection with the east access road. Given there is not enough frontage to provide the required storage and taper, the exclusive right-turn lane is proposed to provide 150 feet of storage and 95 feet of taper.
 - The two access roads that will serve the proposed development will help disperse the development-generated traffic, provide alternatives for the site traffic to enter and exit the development and improve emergency access.
 - Based on the results of the capacity analyses and taking into consideration the proposed geometric improvements, the traffic that will be generated by the proposed development will be accommodated by the area roadway system.

Appendix

Traffic Count Summary Sheets
Site Plan/Geometric Plan
ITE Trip Generation Sheets
CMAP Projections Letter
Site Traffic Assignment Figures of Adjacent
Developments (From Original Traffic Study)
Level of Service Criteria
Capacity Analysis Summary Sheets

Traffic Count Summary Sheets

Peterson Road with Access Drive TMC - TMC

Tue Nov 12, 2024

Full Length (7 AM-9 AM, 4 PM-6 PM)

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245690, Location: 42.305872, -87.97968



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400,
Rosemont, IL, 60018, US

Leg Direction	Peterson Road Westbound					Access Drive Northbound					Peterson Road Eastbound					Int
	T	L	U	App	Ped*	R	L	U	App	Ped*	R	T	U	App	Ped*	
2024-11-12 7:00AM	215	6	1	222	0	16	3	0	19	0	1	330	0	331	0	572
7:15AM	265	17	0	282	0	19	4	0	23	0	1	378	0	379	0	684
7:30AM	309	7	0	316	0	18	4	0	22	0	0	373	0	373	0	711
7:45AM	259	14	0	273	0	15	5	0	20	0	0	366	0	366	0	659
Hourly Total	1048	44	1	1093	0	68	16	0	84	0	2	1447	0	1449	0	2626
8:00AM	231	10	0	241	0	21	3	0	24	0	1	363	0	364	0	629
8:15AM	210	15	2	227	0	20	6	0	26	0	1	327	1	329	0	582
8:30AM	187	10	2	199	0	9	4	0	13	0	2	362	0	364	0	576
8:45AM	160	19	3	182	0	17	6	0	23	0	1	352	0	353	0	558
Hourly Total	788	54	7	849	0	67	19	0	86	0	5	1404	1	1410	0	2345
4:00PM	389	20	0	409	0	21	11	0	32	0	1	314	0	315	0	756
4:15PM	414	20	2	436	0	15	10	0	25	0	0	297	0	297	0	758
4:30PM	392	37	2	431	0	16	15	0	31	0	1	303	0	304	0	766
4:45PM	402	26	1	429	0	11	14	0	25	0	1	267	0	268	0	722
Hourly Total	1597	103	5	1705	0	63	50	0	113	0	3	1181	0	1184	0	3002
5:00PM	443	21	2	466	0	11	11	0	22	0	0	343	0	343	0	831
5:15PM	405	9	1	415	0	15	9	0	24	0	2	284	0	286	0	725
5:30PM	389	21	2	412	0	11	12	0	23	0	0	246	0	246	0	681
5:45PM	349	21	2	372	0	22	6	0	28	0	4	220	0	224	0	624
Hourly Total	1586	72	7	1665	0	59	38	0	97	0	6	1093	0	1099	0	2861
Total	5019	273	20	5312	0	257	123	0	380	0	16	5125	1	5142	0	10834
% Approach	94.5%	5.1%	0.4%	-	-	67.6%	32.4%	0%	-	-	0.3%	99.7%	0%	-	-	-
% Total	46.3%	2.5%	0.2%	49.0%	-	2.4%	1.1%	0%	3.5%	-	0.1%	47.3%	0%	47.5%	-	-
Lights	4756	268	20	5044	-	251	116	0	367	-	14	4883	1	4898	-	10309
% Lights	94.8%	98.2%	100%	95.0%	-	97.7%	94.3%	0%	96.6%	-	87.5%	95.3%	100%	95.3%	-	95.2%
Single-Unit Trucks	101	2	0	103	-	3	5	0	8	-	2	95	0	97	-	208
% Single-Unit Trucks	2.0%	0.7%	0%	1.9%	-	1.2%	4.1%	0%	2.1%	-	12.5%	1.9%	0%	1.9%	-	1.9%
Articulated Trucks	118	3	0	121	-	1	1	0	2	-	0	119	0	119	-	242
% Articulated Trucks	2.4%	1.1%	0%	2.3%	-	0.4%	0.8%	0%	0.5%	-	0%	2.3%	0%	2.3%	-	2.2%
Buses	44	0	0	44	-	2	1	0	3	-	0	28	0	28	-	75
% Buses	0.9%	0%	0%	0.8%	-	0.8%	0.8%	0%	0.8%	-	0%	0.5%	0%	0.5%	-	0.7%
Bicycles on Road	0	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0
% Bicycles on Road	0%	0%	0%	0%	-	0%	0%	0%	0%	-	0%	0%	0%	0%	-	0%
Pedestrians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

* Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Peterson Road with Access Drive TMC - TMC

Tue Nov 12, 2024

Full Length (7 AM-9 AM, 4 PM-6 PM)

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

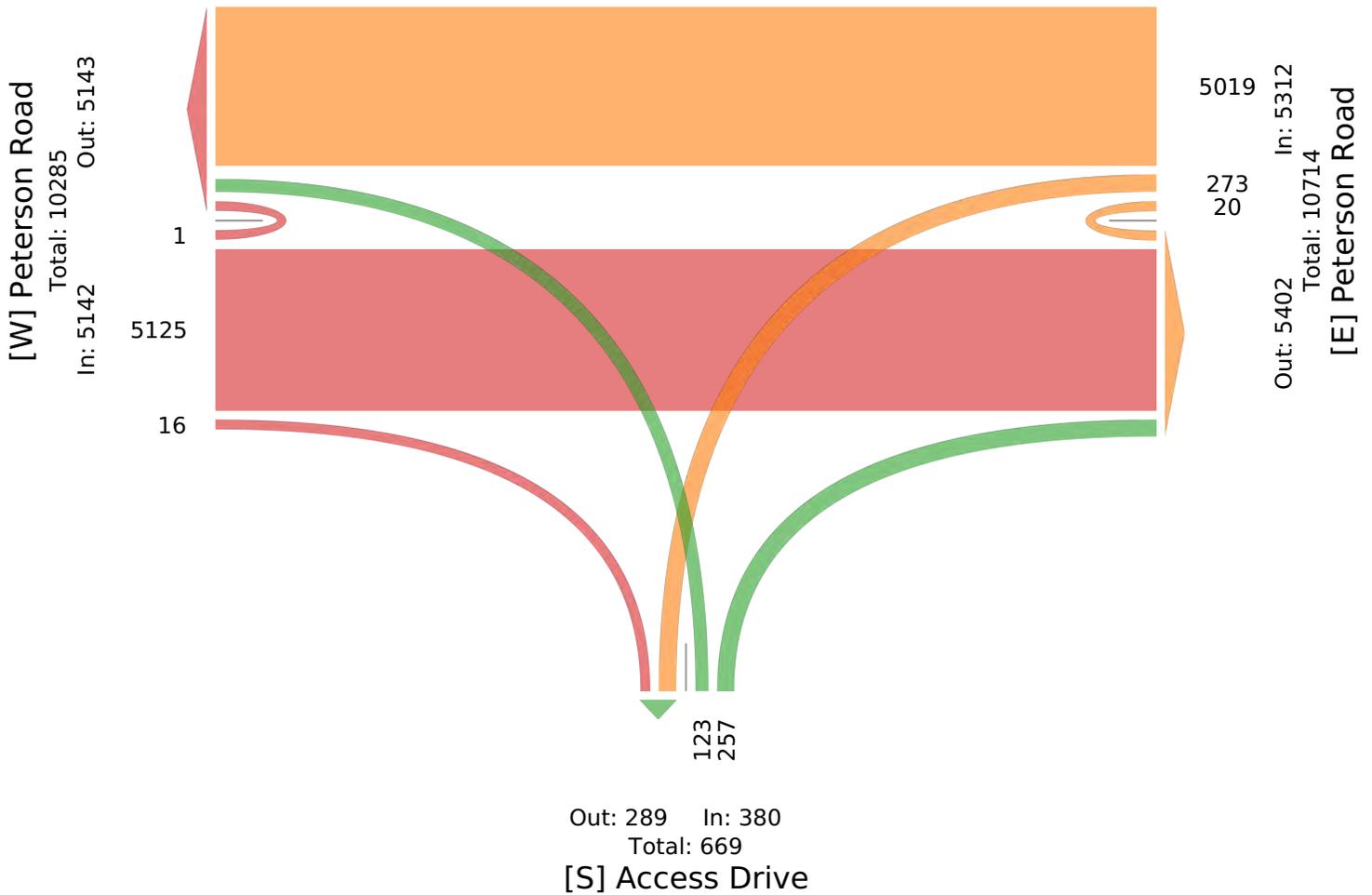
All Movements

ID: 1245690, Location: 42.305872, -87.97968



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400,
Rosemont, IL, 60018, US



Peterson Road with Access Drive TMC - TMC

Tue Nov 12, 2024

AM Peak (7:15 AM - 8:15 AM)

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245690, Location: 42.305872, -87.97968



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400,
Rosemont, IL, 60018, US

Leg Direction	Peterson Road Westbound					Access Drive Northbound					Peterson Road Eastbound					
Time	T	L	U	App	Ped*	R	L	U	App	Ped*	R	T	U	App	Ped*	Int
2024-11-12 7:15AM	265	17	0	282	0	19	4	0	23	0	1	378	0	379	0	684
7:30AM	309	7	0	316	0	18	4	0	22	0	0	373	0	373	0	711
7:45AM	259	14	0	273	0	15	5	0	20	0	0	366	0	366	0	659
8:00AM	231	10	0	241	0	21	3	0	24	0	1	363	0	364	0	629
Total	1064	48	0	1112	0	73	16	0	89	0	2	1480	0	1482	0	2683
% Approach	95.7%	4.3%	0%	-	-	82.0%	18.0%	0%	-	-	0.1%	99.9%	0%	-	-	-
% Total	39.7%	1.8%	0%	41.4%	-	2.7%	0.6%	0%	3.3%	-	0.1%	55.2%	0%	55.2%	-	-
PHF	0.861	0.706	-	0.880	-	0.869	0.800	-	0.927	-	0.500	0.979	-	0.978	-	0.943
Lights	988	46	0	1034	-	71	13	0	84	-	1	1380	0	1381	-	2499
% Lights	92.9%	95.8%	0%	93.0%	-	97.3%	81.3%	0%	94.4%	-	50.0%	93.2%	0%	93.2%	-	93.1%
Single-Unit Trucks	30	0	0	30	-	0	2	0	2	-	1	44	0	45	-	77
% Single-Unit Trucks	2.8%	0%	0%	2.7%	-	0%	12.5%	0%	2.2%	-	50.0%	3.0%	0%	3.0%	-	2.9%
Articulated Trucks	35	2	0	37	-	0	0	0	0	-	0	49	0	49	-	86
% Articulated Trucks	3.3%	4.2%	0%	3.3%	-	0%	0%	0%	0%	-	0%	3.3%	0%	3.3%	-	3.2%
Buses	11	0	0	11	-	2	1	0	3	-	0	7	0	7	-	21
% Buses	1.0%	0%	0%	1.0%	-	2.7%	6.3%	0%	3.4%	-	0%	0.5%	0%	0.5%	-	0.8%
Bicycles on Road	0	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0
% Bicycles on Road	0%	0%	0%	0%	-	0%	0%	0%	0%	-	0%	0%	0%	0%	-	0%
Pedestrians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	-	0
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

* Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Peterson Road with Access Drive TMC - TMC

Tue Nov 12, 2024

AM Peak (7:15 AM - 8:15 AM)

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

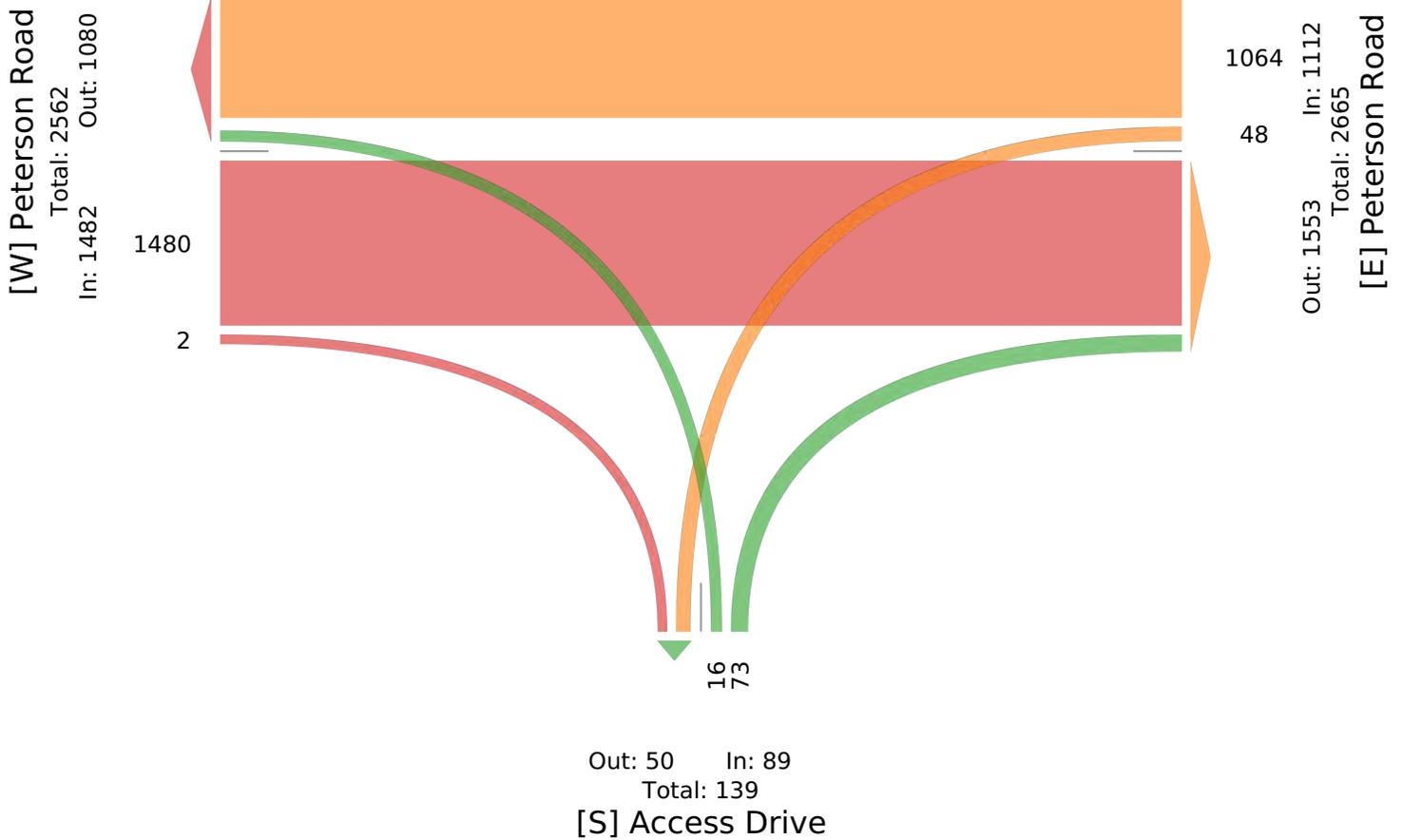
All Movements

ID: 1245690, Location: 42.305872, -87.97968



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400,
Rosemont, IL, 60018, US



Peterson Road with Access Drive TMC - TMC

Tue Nov 12, 2024

PM Peak (4:15 PM - 5:15 PM) - Overall Peak Hour

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245690, Location: 42.305872, -87.97968



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400,
Rosemont, IL, 60018, US

Leg Direction	Peterson Road Westbound					Access Drive Northbound					Peterson Road Eastbound					Int
	T	L	U	App	Ped*	R	L	U	App	Ped*	R	T	U	App	Ped*	
2024-11-12 4:15PM	414	20	2	436	0	15	10	0	25	0	0	297	0	297	0	758
4:30PM	392	37	2	431	0	16	15	0	31	0	1	303	0	304	0	766
4:45PM	402	26	1	429	0	11	14	0	25	0	1	267	0	268	0	722
5:00PM	443	21	2	466	0	11	11	0	22	0	0	343	0	343	0	831
Total	1651	104	7	1762	0	53	50	0	103	0	2	1210	0	1212	0	3077
% Approach	93.7%	5.9%	0.4%	-	-	51.5%	48.5%	0%	-	-	0.2%	99.8%	0%	-	-	-
% Total	53.7%	3.4%	0.2%	57.3%	-	1.7%	1.6%	0%	3.3%	-	0.1%	39.3%	0%	39.4%	-	-
PHF	0.932	0.703	0.875	0.945	-	0.828	0.833	-	0.831	-	0.500	0.882	-	0.883	-	0.926
Lights	1600	104	7	1711	-	53	50	0	103	-	2	1183	0	1185	-	2999
% Lights	96.9%	100%	100%	97.1%	-	100%	100%	0%	100%	-	100%	97.8%	0%	97.8%	-	97.5%
Single-Unit Trucks	25	0	0	25	-	0	0	0	0	-	0	12	0	12	-	37
% Single-Unit Trucks	1.5%	0%	0%	1.4%	-	0%	0%	0%	0%	-	0%	1.0%	0%	1.0%	-	1.2%
Articulated Trucks	21	0	0	21	-	0	0	0	0	-	0	13	0	13	-	34
% Articulated Trucks	1.3%	0%	0%	1.2%	-	0%	0%	0%	0%	-	0%	1.1%	0%	1.1%	-	1.1%
Buses	5	0	0	5	-	0	0	0	0	-	0	2	0	2	-	7
% Buses	0.3%	0%	0%	0.3%	-	0%	0%	0%	0%	-	0%	0.2%	0%	0.2%	-	0.2%
Bicycles on Road	0	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0
% Bicycles on Road	0%	0%	0%	0%	-	0%	0%	0%	0%	-	0%	0%	0%	0%	-	0%
Pedestrians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	-	0
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

* Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Peterson Road with Access Drive TMC - TMC

Tue Nov 12, 2024

PM Peak (4:15 PM - 5:15 PM) - Overall Peak Hour

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

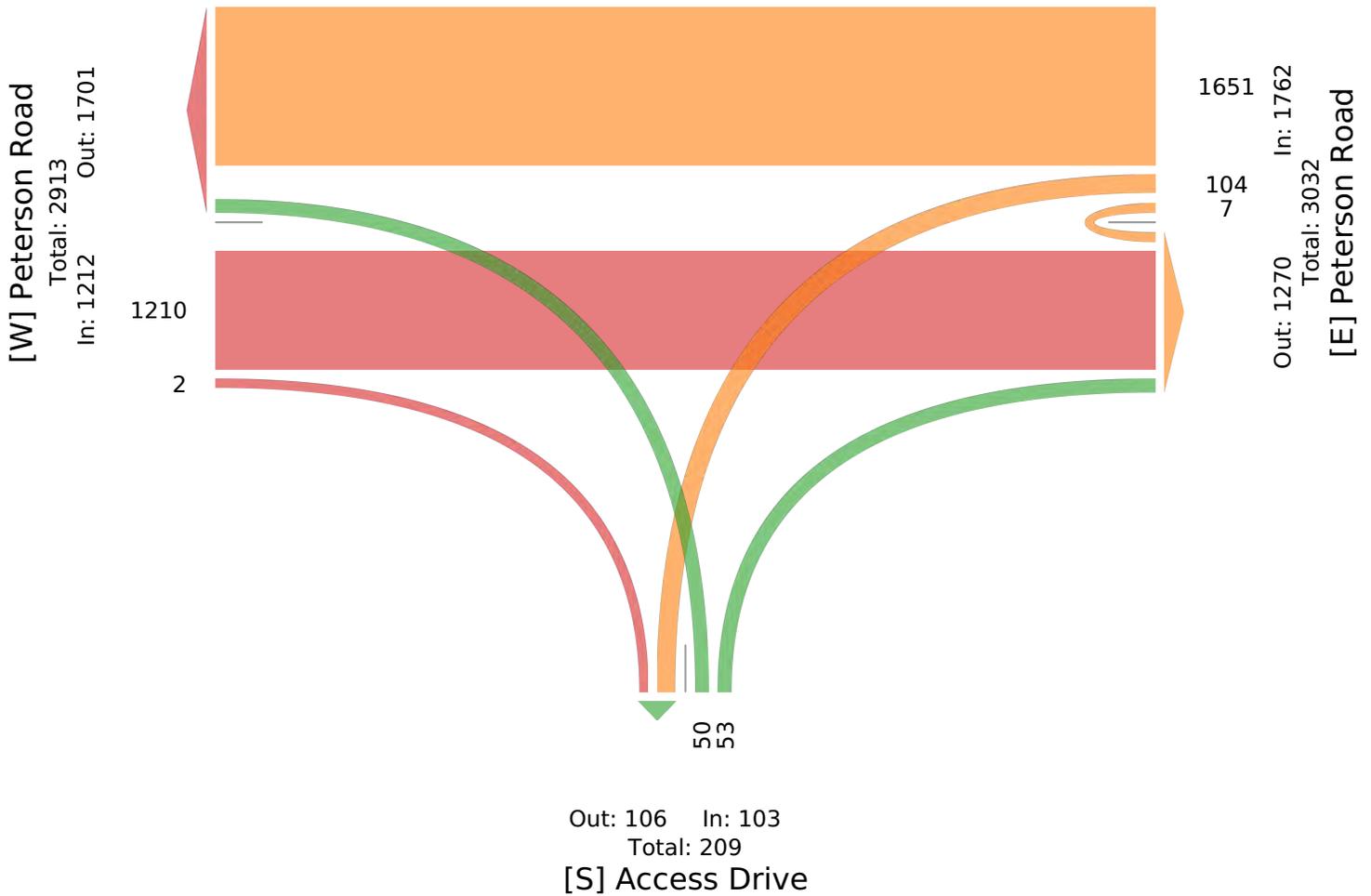
All Movements

ID: 1245690, Location: 42.305872, -87.97968



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400,
Rosemont, IL, 60018, US



Peterson Road with Elderberry Drive TMC - TMC

Tue Nov 12, 2024

Full Length (7 AM-9 AM, 4 PM-6 PM)

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245693, Location: 42.306019, -87.976305



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400, Rosemont, IL, 60018, US

Leg Direction	Access Drive Southbound						Peterson Road Westbound						Elderberry Drive Northbound						Peterson Road Eastbound						Int
	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	
2024-11-12																									
7:00AM	3	0	0	0	3	0	2	210	3	0	215	0	7	0	1	0	8	0	1	352	1	0	354	0	580
7:15AM	1	0	1	0	2	0	0	280	2	0	282	0	13	0	1	0	14	0	0	392	0	0	392	0	690
7:30AM	2	0	1	0	3	0	2	307	5	0	314	0	16	0	0	0	16	0	2	384	1	1	388	0	721
7:45AM	1	0	0	0	1	0	3	276	4	0	283	0	13	0	1	0	14	0	0	355	3	0	358	0	656
Hourly Total	7	0	2	0	9	0	7	1073	14	0	1094	0	49	0	3	0	52	0	3	1483	5	1	1492	0	2647
8:00AM	4	0	2	0	6	0	1	229	8	1	239	0	12	0	3	0	15	0	5	376	3	1	385	0	645
8:15AM	1	0	2	0	3	0	0	227	6	0	233	0	16	0	2	0	18	0	3	350	5	0	358	0	612
8:30AM	2	0	0	0	2	0	0	190	4	0	194	0	9	0	1	0	10	0	2	360	1	0	363	0	569
8:45AM	1	0	0	0	1	0	1	175	6	0	182	0	11	0	2	0	13	0	2	371	1	0	374	0	570
Hourly Total	8	0	4	0	12	0	2	821	24	1	848	0	48	0	8	0	56	0	12	1457	10	1	1480	0	2396
4:00PM	2	0	2	0	4	0	0	406	10	0	416	0	10	0	2	0	12	0	2	341	2	1	346	0	778
4:15PM	3	0	0	0	3	0	0	434	2	0	436	0	14	0	2	0	16	0	6	300	1	1	308	0	763
4:30PM	3	0	0	0	3	0	0	421	9	1	431	0	8	0	1	0	9	0	3	320	0	1	324	0	767
4:45PM	3	0	0	0	3	0	0	425	10	2	437	0	6	0	0	0	6	0	5	256	4	1	266	0	712
Hourly Total	11	0	2	0	13	0	0	1686	31	3	1720	0	38	0	5	0	43	0	16	1217	7	4	1244	0	3020
5:00PM	6	0	0	0	6	0	1	443	6	0	450	0	3	0	0	0	3	0	7	352	1	2	362	0	821
5:15PM	1	0	0	0	1	0	0	407	14	0	421	0	5	0	1	0	6	0	2	292	0	0	294	0	722
5:30PM	1	0	0	0	1	0	0	401	6	0	407	0	10	0	0	0	10	0	5	251	0	3	259	0	677
5:45PM	0	0	0	0	0	0	0	368	8	0	376	0	3	0	3	0	6	0	1	240	0	0	241	0	623
Hourly Total	8	0	0	0	8	0	1	1619	34	0	1654	0	21	0	4	0	25	0	15	1135	1	5	1156	0	2843
Total	34	0	8	0	42	0	10	5199	103	4	5316	0	156	0	20	0	176	0	46	5292	23	11	5372	0	10906
% Approach	81.0%	0%	19.0%	0%	-	-	0.2%	97.8%	1.9%	0.1%	-	-	88.6%	0%	11.4%	0%	-	-	0.9%	98.5%	0.4%	0.2%	-	-	-
% Total	0.3%	0%	0.1%	0%	0.4%	-	0.1%	47.7%	0.9%	0%	48.7%	-	1.4%	0%	0.2%	0%	1.6%	-	0.4%	48.5%	0.2%	0.1%	49.3%	-	-
Lights	31	0	8	0	39	-	10	4942	101	4	5057	-	150	0	20	0	170	-	46	5049	21	11	5127	-	10393
% Lights	91.2%	0%	100%	0%	92.9%	-	100%	95.1%	98.1%	100%	95.1%	-	96.2%	0%	100%	0%	96.6%	-	100%	95.4%	91.3%	100%	95.4%	-	95.3%
Single-Unit Trucks	0	0	0	0	0	-	0	87	0	0	87	-	1	0	0	0	1	-	0	109	0	0	109	-	197
% Single-Unit Trucks	0%	0%	0%	0%	0%	-	0%	1.7%	0%	0%	1.6%	-	0.6%	0%	0%	0%	0.6%	-	0%	2.1%	0%	0%	2.0%	-	1.8%
Articulated Trucks	0	0	0	0	0	-	0	132	1	0	133	-	0	0	0	0	0	-	0	102	0	0	102	-	235
% Articulated Trucks	0%	0%	0%	0%	0%	-	0%	2.5%	1.0%	0%	2.5%	-	0%	0%	0%	0%	0%	-	0%	1.9%	0%	0%	1.9%	-	2.2%
Buses	3	0	0	0	3	-	0	38	1	0	39	-	5	0	0	0	5	-	0	30	2	0	32	-	79
% Buses	8.8%	0%	0%	0%	7.1%	-	0%	0.7%	1.0%	0%	0.7%	-	3.2%	0%	0%	0%	2.8%	-	0%	0.6%	8.7%	0%	0.6%	-	0.7%
Bicycles on Road	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	2	0	0	2	-	2
% Bicycles on Road	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%
Pedestrians	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

* Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Peterson Road with Elderberry Drive TMC - TMC

Tue Nov 12, 2024

Full Length (7 AM-9 AM, 4 PM-6 PM)

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245693, Location: 42.306019, -87.976305



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400, Rosemont, IL, 60018, US

[N] Access Drive

Total: 75

In: 42 Out: 33

34
8

[W] Peterson Road
Total: 10636
In: 5372 Out: 5264

11
23
5292
46

10
5199
103
4
Out: 5460 In: 5316
Total: 10776

[E] Peterson Road

Out: 149 In: 176
Total: 325

[S] Elderberry Drive

20
156

Peterson Road with Elderberry Drive TMC - TMC

Tue Nov 12, 2024

AM Peak (7:15 AM - 8:15 AM)

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245693, Location: 42.306019, -87.976305



Provided by: Kenig Lindgren O'Hara Aboona, Inc.
9575 W. Higgins Rd., Suite 400,
Rosemont, IL, 60018, US

Leg Direction	Access Drive Southbound						Peterson Road Westbound						Elderberry Drive Northbound						Peterson Road Eastbound						Int
	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	
2024-11-12 7:15AM	1	0	1	0	2	0	0	280	2	0	282	0	13	0	1	0	14	0	0	392	0	0	392	0	690
7:30AM	2	0	1	0	3	0	2	307	5	0	314	0	16	0	0	0	16	0	2	384	1	1	388	0	721
7:45AM	1	0	0	0	1	0	3	276	4	0	283	0	13	0	1	0	14	0	0	355	3	0	358	0	656
8:00AM	4	0	2	0	6	0	1	229	8	1	239	0	12	0	3	0	15	0	5	376	3	1	385	0	645
Total	8	0	4	0	12	0	6	1092	19	1	1118	0	54	0	5	0	59	0	7	1507	7	2	1523	0	2712
% Approach	66.7%	0%	33.3%	0%	-	-	0.5%	97.7%	1.7%	0.1%	-	-	91.5%	0%	8.5%	0%	-	-	0.5%	98.9%	0.5%	0.1%	-	-	-
% Total	0.3%	0%	0.1%	0%	0.4%	-	0.2%	40.3%	0.7%	0%	41.2%	-	2.0%	0%	0.2%	0%	2.2%	-	0.3%	55.6%	0.3%	0.1%	56.2%	-	-
PHF	0.500	-	0.500	-	0.500	-	0.500	0.889	0.594	0.250	0.890	-	0.844	-0.417	-	0.922	-	0.350	0.962	0.583	0.500	0.973	-	0.941	
Lights	7	0	4	0	11	-	6	1015	17	1	1039	-	52	0	5	0	57	-	7	1404	6	2	1419	-	2526
% Lights	87.5%	0%	100%	0%	91.7%	-	100%	92.9%	89.5%	100%	92.9%	-	96.3%	0%	100%	0%	96.6%	-	100%	93.2%	85.7%	100%	93.2%	-	93.1%
Single-Unit Trucks	0	0	0	0	0	-	0	24	0	0	24	-	0	0	0	0	0	-	0	52	0	0	52	-	76
% Single-Unit Trucks	0%	0%	0%	0%	0%	-	0%	2.2%	0%	0%	2.1%	-	0%	0%	0%	0%	0%	-	0%	3.5%	0%	0%	3.4%	-	2.8%
Articulated Trucks	0	0	0	0	0	-	0	44	1	0	45	-	0	0	0	0	0	-	0	39	0	0	39	-	84
% Articulated Trucks	0%	0%	0%	0%	0%	-	0%	4.0%	5.3%	0%	4.0%	-	0%	0%	0%	0%	0%	-	0%	2.6%	0%	0%	2.6%	-	3.1%
Buses	1	0	0	0	1	-	0	9	1	0	10	-	2	0	0	0	2	-	0	10	1	0	11	-	24
% Buses	12.5%	0%	0%	0%	8.3%	-	0%	0.8%	5.3%	0%	0.9%	-	3.7%	0%	0%	0%	3.4%	-	0%	0.7%	14.3%	0%	0.7%	-	0.9%
Bicycles on Road	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	2	0	0	2	-	2
% Bicycles on Road	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%	0.1%	0%	0%	0.1%	-	0.1%
Pedestrians	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

* Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Peterson Road with Elderberry Drive TMC - TMC

Tue Nov 12, 2024

AM Peak (7:15 AM - 8:15 AM)

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245693, Location: 42.306019, -87.976305



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400,
Rosemont, IL, 60018, US

[N] Access Drive

Total: 25

In: 12 Out: 13

84

[W] Peterson Road

Total: 2630
In: 1523 Out: 1107

27
1507
7

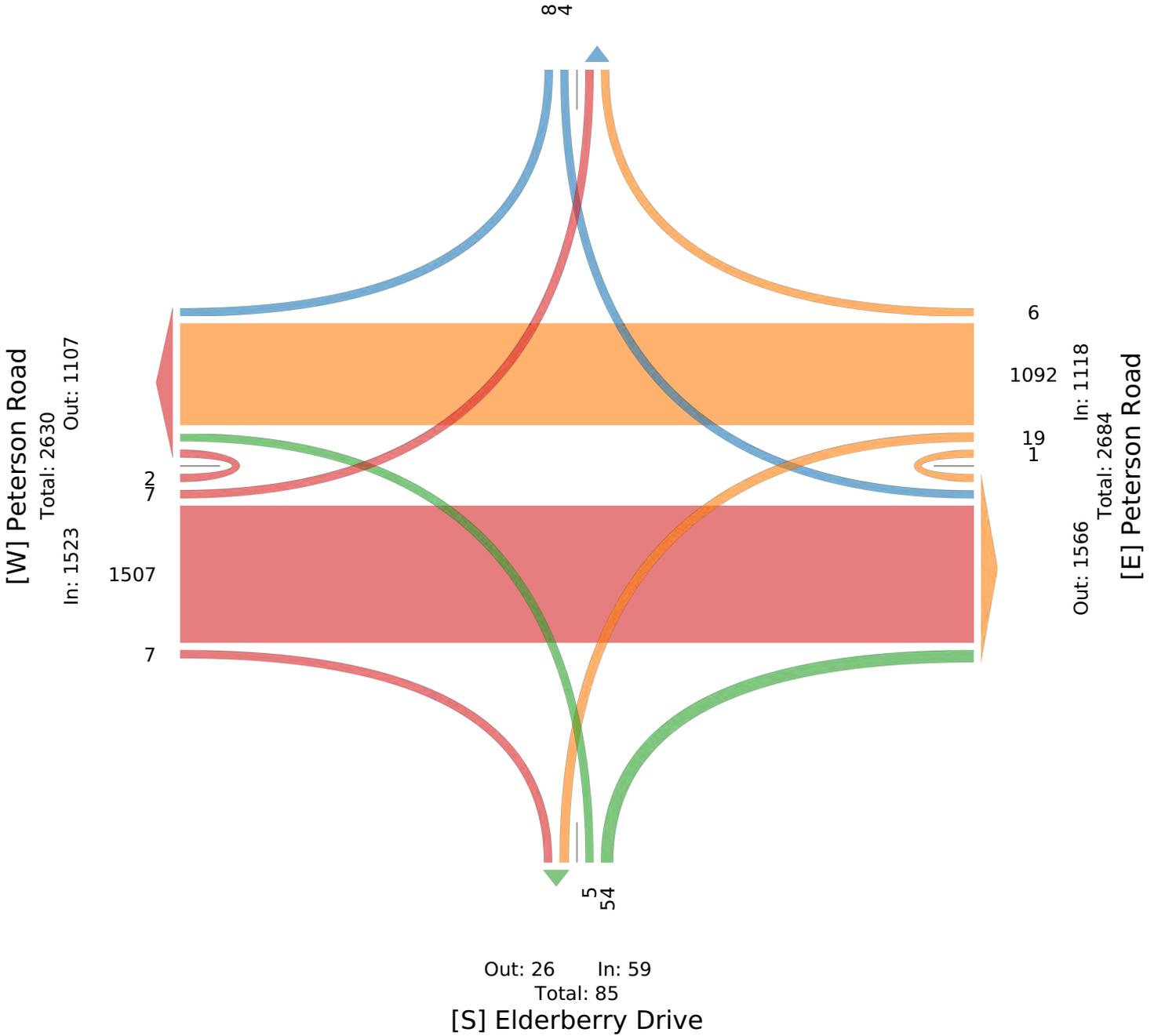
6
1092
19

[E] Peterson Road
Total: 2684
In: 1118 Out: 1566

Out: 26 In: 59
Total: 85

[S] Elderberry Drive

54



Peterson Road with Elderberry Drive TMC - TMC

Tue Nov 12, 2024

PM Peak (4:15 PM - 5:15 PM) - Overall Peak Hour

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245693, Location: 42.306019, -87.976305



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400,
Rosemont, IL, 60018, US

Leg Direction	Access Drive Southbound					Peterson Road Westbound					Elderberry Drive Northbound					Peterson Road Eastbound									
Time	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	Int
2024-11-12 4:15PM	3	0	0	0	3	0	0	434	2	0	436	0	14	0	2	0	16	0	6	300	1	1	308	0	763
4:30PM	3	0	0	0	3	0	0	421	9	1	431	0	8	0	1	0	9	0	3	320	0	1	324	0	767
4:45PM	3	0	0	0	3	0	0	425	10	2	437	0	6	0	0	0	6	0	5	256	4	1	266	0	712
5:00PM	6	0	0	0	6	0	1	443	6	0	450	0	3	0	0	0	3	0	7	352	1	2	362	0	821
Total	15	0	0	0	15	0	1	1723	27	3	1754	0	31	0	3	0	34	0	21	1228	6	5	1260	0	3063
% Approach	100%	0%	0%	0%	-	-	0.1%	98.2%	1.5%	0.2%	-	-	91.2%	0%	8.8%	0%	-	-	1.7%	97.5%	0.5%	0.4%	-	-	-
% Total	0.5%	0%	0%	0%	0.5%	-	0%	56.3%	0.9%	0.1%	57.3%	-	1.0%	0%	0.1%	0%	1.1%	-	0.7%	40.1%	0.2%	0.2%	41.1%	-	-
PHF	0.625	-	-	-	0.625	-	0.250	0.972	0.675	0.375	0.974	-	0.554	-	0.375	-	0.531	-	0.750	0.872	0.375	0.625	0.870	-	0.933
Lights	15	0	0	0	15	-	1	1678	27	3	1709	-	30	0	3	0	33	-	21	1201	6	5	1233	-	2990
% Lights	100%	0%	0%	0%	100%	-	100%	97.4%	100%	100%	97.4%	-	96.8%	0%	100%	0%	97.1%	-	100%	97.8%	100%	100%	97.9%	-	97.6%
Single-Unit Trucks	0	0	0	0	0	-	0	22	0	0	22	-	1	0	0	0	1	-	0	13	0	0	13	-	36
% Single-Unit Trucks	0%	0%	0%	0%	0%	-	0%	1.3%	0%	0%	1.3%	-	3.2%	0%	0%	0%	2.9%	-	0%	1.1%	0%	0%	1.0%	-	1.2%
Articulated Trucks	0	0	0	0	0	-	0	18	0	0	18	-	0	0	0	0	0	-	0	11	0	0	11	-	29
% Articulated Trucks	0%	0%	0%	0%	0%	-	0%	1.0%	0%	0%	1.0%	-	0%	0%	0%	0%	0%	-	0%	0.9%	0%	0%	0.9%	-	0.9%
Buses	0	0	0	0	0	-	0	5	0	0	5	-	0	0	0	0	0	-	0	3	0	0	3	-	8
% Buses	0%	0%	0%	0%	0%	-	0%	0.3%	0%	0%	0.3%	-	0%	0%	0%	0%	0%	-	0%	0.2%	0%	0%	0.2%	-	0.3%
Bicycles on Road	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0
% Bicycles on Road	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%
Pedestrians	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

* Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Peterson Road with Elderberry Drive TMC - TMC

Tue Nov 12, 2024

PM Peak (4:15 PM - 5:15 PM) - Overall Peak Hour

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245693, Location: 42.306019, -87.976305



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400, Rosemont, IL, 60018, US

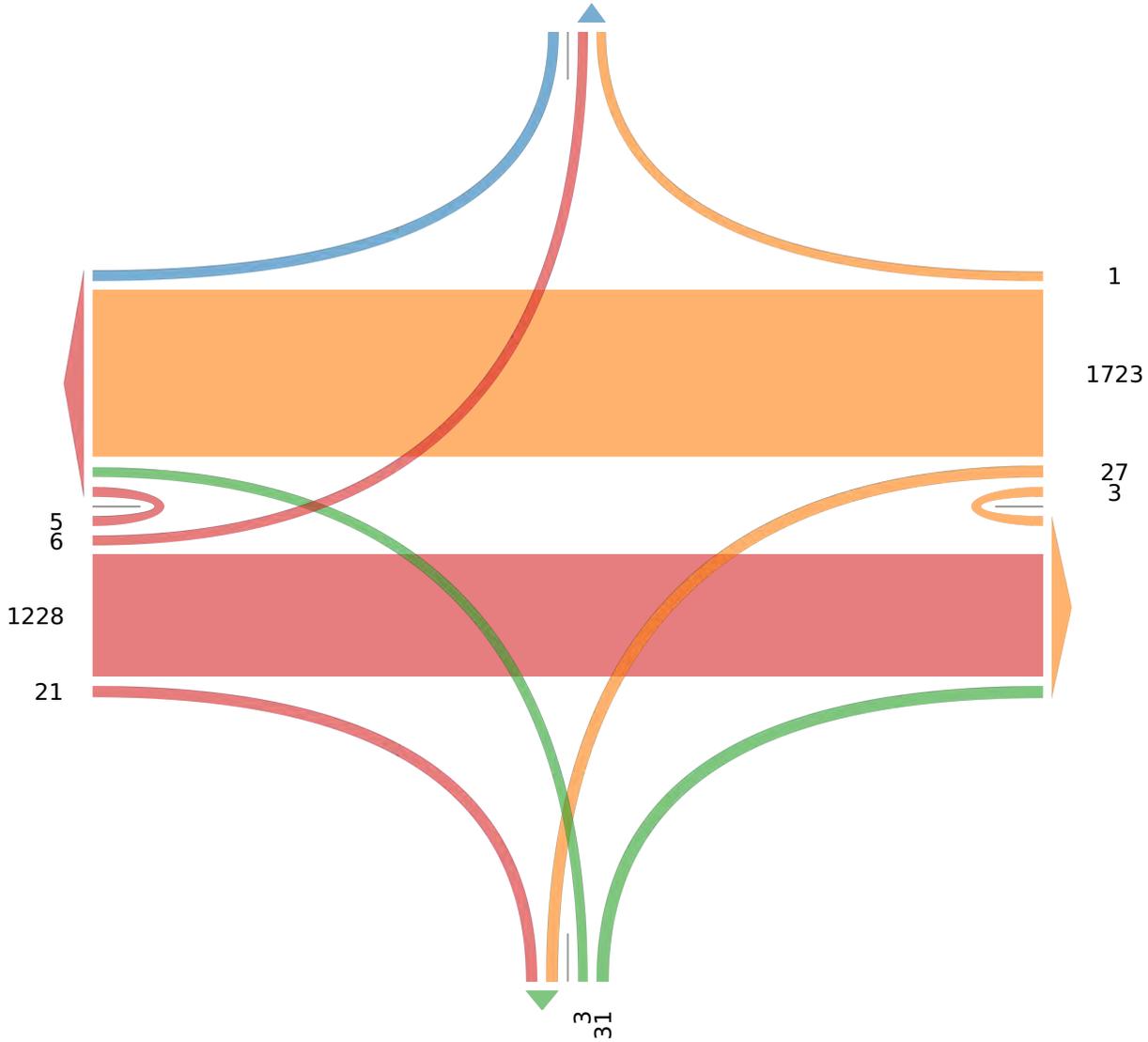
[N] Access Drive

Total: 22

In: 15 Out: 7

15

[W] Peterson Road
Total: 3006
In: 1260 Out: 1746



[E] Peterson Road
Total: 3016
In: 1754
Out: 1262

Out: 48 In: 34

Total: 82

[S] Elderberry Drive

Peterson Road with Forest Creek Lane TMC - TMC

Tue Nov 12, 2024

Full Length (7 AM-9 AM, 4 PM-6 PM)

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245695, Location: 42.306095, -87.972528



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400,
Rosemont, IL, 60018, US

Leg Direction	Forest Creek Lane Southbound					Peterson Road Westbound					Peterson Road Eastbound					Int
	R	L	U	App	Ped*	R	T	U	App	Ped*	T	L	U	App	Ped*	
2024-11-12 7:00AM	0	1	0	1	0	0	218	0	218	0	379	1	0	380	0	599
7:15AM	1	4	0	5	0	1	277	0	278	0	418	1	0	419	0	702
7:30AM	1	2	0	3	0	1	330	0	331	0	392	0	0	392	0	726
7:45AM	0	2	0	2	0	2	287	0	289	0	391	0	0	391	0	682
Hourly Total	2	9	0	11	0	4	1112	0	1116	0	1580	2	0	1582	0	2709
8:00AM	4	1	0	5	0	0	241	0	241	0	404	1	0	405	0	651
8:15AM	0	2	0	2	0	1	227	0	228	0	357	2	0	359	0	589
8:30AM	0	3	0	3	0	0	200	0	200	0	348	1	0	349	0	552
8:45AM	3	1	0	4	0	5	179	0	184	0	400	1	0	401	0	589
Hourly Total	7	7	0	14	0	6	847	0	853	0	1509	5	0	1514	0	2381
4:00PM	4	1	0	5	0	2	412	0	414	0	351	2	0	353	0	772
4:15PM	5	1	0	6	0	2	431	0	433	0	311	1	0	312	0	751
4:30PM	2	1	0	3	0	2	439	0	441	0	321	0	0	321	0	765
4:45PM	0	1	0	1	0	2	425	0	427	0	279	1	0	280	0	708
Hourly Total	11	4	0	15	0	8	1707	0	1715	0	1262	4	0	1266	0	2996
5:00PM	1	1	0	2	0	1	479	0	480	0	341	0	0	341	0	823
5:15PM	0	0	0	0	0	1	445	0	446	0	294	1	0	295	0	741
5:30PM	1	1	0	2	0	1	416	0	417	0	236	2	0	238	0	657
5:45PM	1	1	0	2	0	1	416	0	417	0	246	0	0	246	0	665
Hourly Total	3	3	0	6	0	4	1756	0	1760	0	1117	3	0	1120	0	2886
Total	23	23	0	46	0	22	5422	0	5444	0	5468	14	0	5482	0	10972
% Approach	50.0%	50.0%	0%	-	-	0.4%	99.6%	0%	-	-	99.7%	0.3%	0%	-	-	-
% Total	0.2%	0.2%	0%	0.4%	-	0.2%	49.4%	0%	49.6%	-	49.8%	0.1%	0%	50.0%	-	-
Lights	20	23	0	43	-	21	5173	0	5194	-	5246	12	0	5258	-	10495
% Lights	87.0%	100%	0%	93.5%	-	95.5%	95.4%	0%	95.4%	-	95.9%	85.7%	0%	95.9%	-	95.7%
Single-Unit Trucks	1	0	0	1	-	1	82	0	83	-	97	0	0	97	-	181
% Single-Unit Trucks	4.3%	0%	0%	2.2%	-	4.5%	1.5%	0%	1.5%	-	1.8%	0%	0%	1.8%	-	1.6%
Articulated Trucks	0	0	0	0	-	0	129	0	129	-	98	0	0	98	-	227
% Articulated Trucks	0%	0%	0%	0%	-	0%	2.4%	0%	2.4%	-	1.8%	0%	0%	1.8%	-	2.1%
Buses	2	0	0	2	-	0	38	0	38	-	27	2	0	29	-	69
% Buses	8.7%	0%	0%	4.3%	-	0%	0.7%	0%	0.7%	-	0.5%	14.3%	0%	0.5%	-	0.6%
Bicycles on Road	0	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0
% Bicycles on Road	0%	0%	0%	0%	-	0%	0%	0%	0%	-	0%	0%	0%	0%	-	0%
Pedestrians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

* Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Peterson Road with Forest Creek Lane TMC - TMC

Tue Nov 12, 2024

Full Length (7 AM-9 AM, 4 PM-6 PM)

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245695, Location: 42.306095, -87.972528



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400, Rosemont, IL, 60018, US

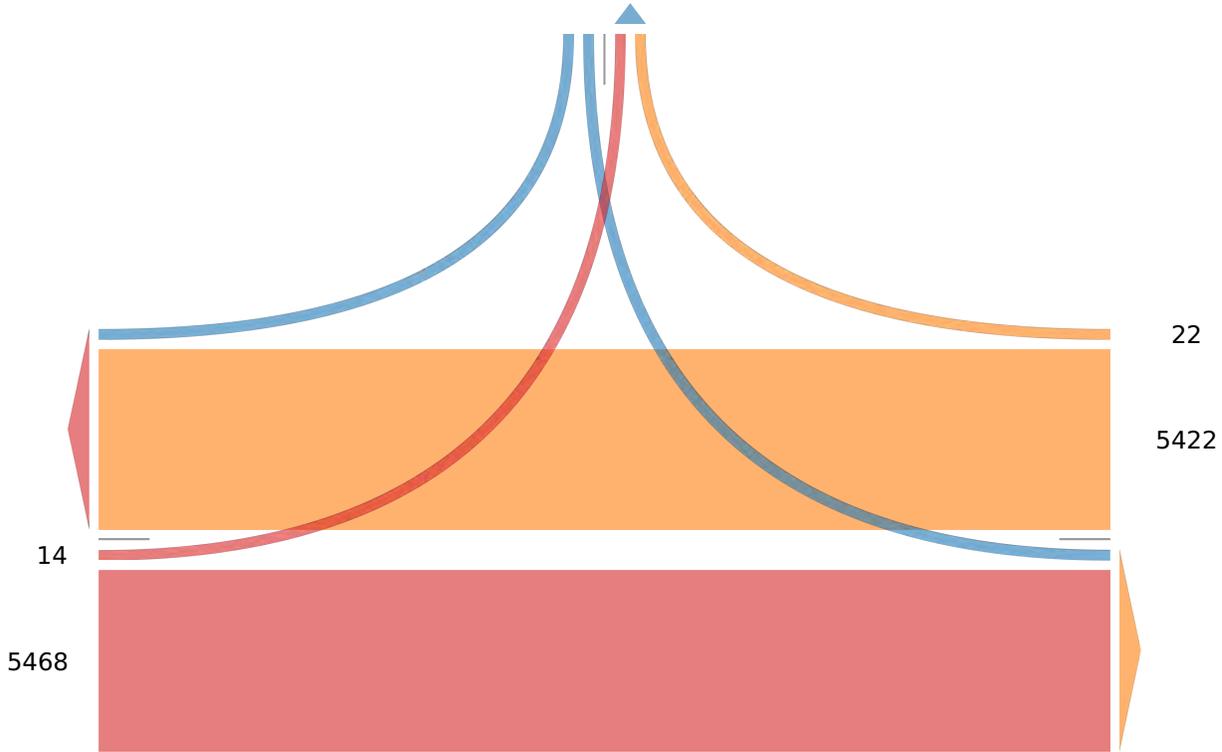
[N] Forest Creek Lane

Total: 82

In: 46 Out: 36

233

[W] Peterson Road
Total: 10927
In: 5482 Out: 5445



Out: 5491 In: 5444
Total: 10935
[E] Peterson Road

Peterson Road with Forest Creek Lane TMC - TMC

Tue Nov 12, 2024

AM Peak (7:15 AM - 8:15 AM)

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245695, Location: 42.306095, -87.972528



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400,
Rosemont, IL, 60018, US

Leg Direction	Forest Creek Lane Southbound					Peterson Road Westbound					Peterson Road Eastbound					Int
	R	L	U	App	Ped*	R	T	U	App	Ped*	T	L	U	App	Ped*	
2024-11-12 7:15AM	1	4	0	5	0	1	277	0	278	0	418	1	0	419	0	702
7:30AM	1	2	0	3	0	1	330	0	331	0	392	0	0	392	0	726
7:45AM	0	2	0	2	0	2	287	0	289	0	391	0	0	391	0	682
8:00AM	4	1	0	5	0	0	241	0	241	0	404	1	0	405	0	651
Total	6	9	0	15	0	4	1135	0	1139	0	1605	2	0	1607	0	2761
% Approach	40.0%	60.0%	0%	-	-	0.4%	99.6%	0%	-	-	99.9%	0.1%	0%	-	-	-
% Total	0.2%	0.3%	0%	0.5%	-	0.1%	41.1%	0%	41.3%	-	58.1%	0.1%	0%	58.2%	-	-
PHF	0.375	0.563	-	0.750	-	0.500	0.860	-	0.860	-	0.960	0.500	-	0.959	-	0.951
Lights	4	9	0	13	-	4	1065	0	1069	-	1520	1	0	1521	-	2603
% Lights	66.7%	100%	0%	86.7%	-	100%	93.8%	0%	93.9%	-	94.7%	50.0%	0%	94.6%	-	94.3%
Single-Unit Trucks	1	0	0	1	-	0	18	0	18	-	42	0	0	42	-	61
% Single-Unit Trucks	16.7%	0%	0%	6.7%	-	0%	1.6%	0%	1.6%	-	2.6%	0%	0%	2.6%	-	2.2%
Articulated Trucks	0	0	0	0	-	0	43	0	43	-	36	0	0	36	-	79
% Articulated Trucks	0%	0%	0%	0%	-	0%	3.8%	0%	3.8%	-	2.2%	0%	0%	2.2%	-	2.9%
Buses	1	0	0	1	-	0	9	0	9	-	7	1	0	8	-	18
% Buses	16.7%	0%	0%	6.7%	-	0%	0.8%	0%	0.8%	-	0.4%	50.0%	0%	0.5%	-	0.7%
Bicycles on Road	0	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0
% Bicycles on Road	0%	0%	0%	0%	-	0%	0%	0%	0%	-	0%	0%	0%	0%	-	0%
Pedestrians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

* Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Peterson Road with Forest Creek Lane TMC - TMC

Tue Nov 12, 2024

AM Peak (7:15 AM - 8:15 AM)

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245695, Location: 42.306095, -87.972528



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400, Rosemont, IL, 60018, US

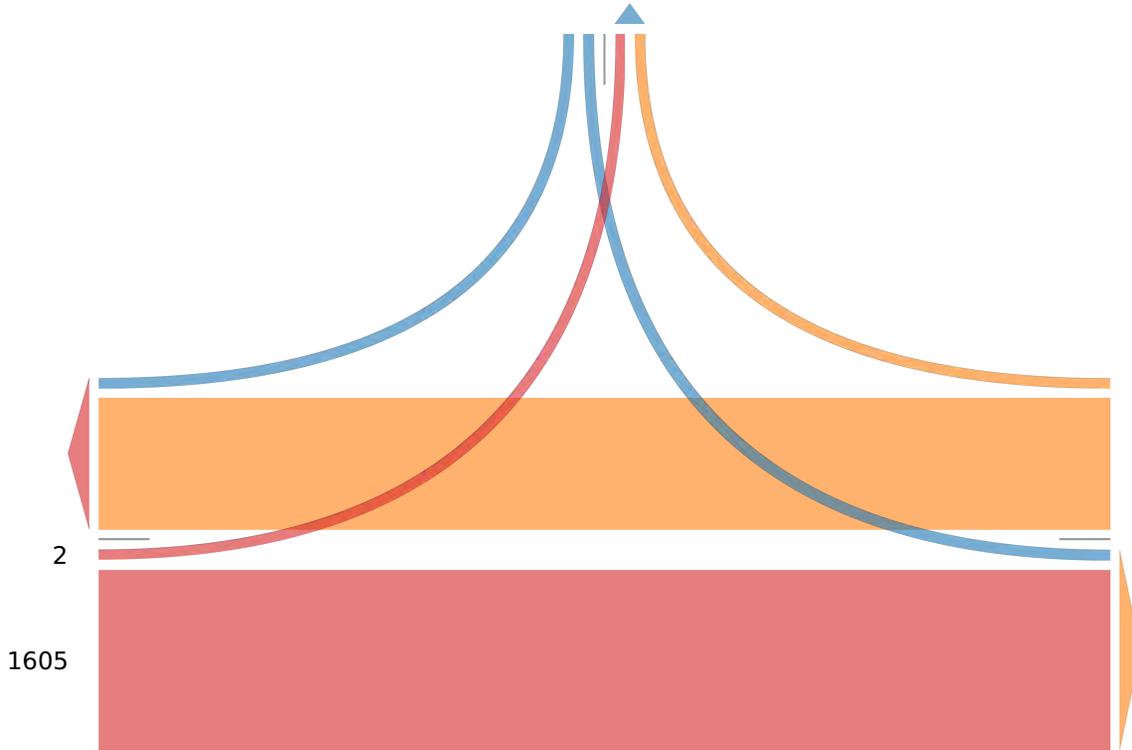
[N] Forest Creek Lane

Total: 21

In: 15 Out: 6

69

[W] Peterson Road
Total: 2748
In: 1607 Out: 1141



Out: 1614 In: 1139
Total: 2753
[E] Peterson Road

Peterson Road with Forest Creek Lane TMC - TMC

Tue Nov 12, 2024

PM Peak (4:15 PM - 5:15 PM) - Overall Peak Hour

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245695, Location: 42.306095, -87.972528



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400,
Rosemont, IL, 60018, US

Leg Direction	Forest Creek Lane Southbound					Peterson Road Westbound					Peterson Road Eastbound					Int
	R	L	U	App	Ped*	R	T	U	App	Ped*	T	L	U	App	Ped*	
2024-11-12 4:15PM	5	1	0	6	0	2	431	0	433	0	311	1	0	312	0	751
4:30PM	2	1	0	3	0	2	439	0	441	0	321	0	0	321	0	765
4:45PM	0	1	0	1	0	2	425	0	427	0	279	1	0	280	0	708
5:00PM	1	1	0	2	0	1	479	0	480	0	341	0	0	341	0	823
Total	8	4	0	12	0	7	1774	0	1781	0	1252	2	0	1254	0	3047
% Approach	66.7%	33.3%	0%	-	-	0.4%	99.6%	0%	-	-	99.8%	0.2%	0%	-	-	-
% Total	0.3%	0.1%	0%	0.4%	-	0.2%	58.2%	0%	58.5%	-	41.1%	0.1%	0%	41.2%	-	-
PHF	0.400	1.000	-	0.500	-	0.875	0.926	-	0.928	-	0.918	0.500	-	0.919	-	0.926
Lights	8	4	0	12	-	7	1728	0	1735	-	1224	2	0	1226	-	2973
% Lights	100%	100%	0%	100%	-	100%	97.4%	0%	97.4%	-	97.8%	100%	0%	97.8%	-	97.6%
Single-Unit Trucks	0	0	0	0	-	0	23	0	23	-	12	0	0	12	-	35
% Single-Unit Trucks	0%	0%	0%	0%	-	0%	1.3%	0%	1.3%	-	1.0%	0%	0%	1.0%	-	1.1%
Articulated Trucks	0	0	0	0	-	0	19	0	19	-	14	0	0	14	-	33
% Articulated Trucks	0%	0%	0%	0%	-	0%	1.1%	0%	1.1%	-	1.1%	0%	0%	1.1%	-	1.1%
Buses	0	0	0	0	-	0	4	0	4	-	2	0	0	2	-	6
% Buses	0%	0%	0%	0%	-	0%	0.2%	0%	0.2%	-	0.2%	0%	0%	0.2%	-	0.2%
Bicycles on Road	0	0	0	0	-	0	0	0	0	-	0	0	0	0	-	0
% Bicycles on Road	0%	0%	0%	0%	-	0%	0%	0%	0%	-	0%	0%	0%	0%	-	0%
Pedestrians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

* Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Peterson Road with Forest Creek Lane TMC - TMC

Tue Nov 12, 2024

PM Peak (4:15 PM - 5:15 PM) - Overall Peak Hour

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245695, Location: 42.306095, -87.972528



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400,
Rosemont, IL, 60018, US

[N] Forest Creek Lane

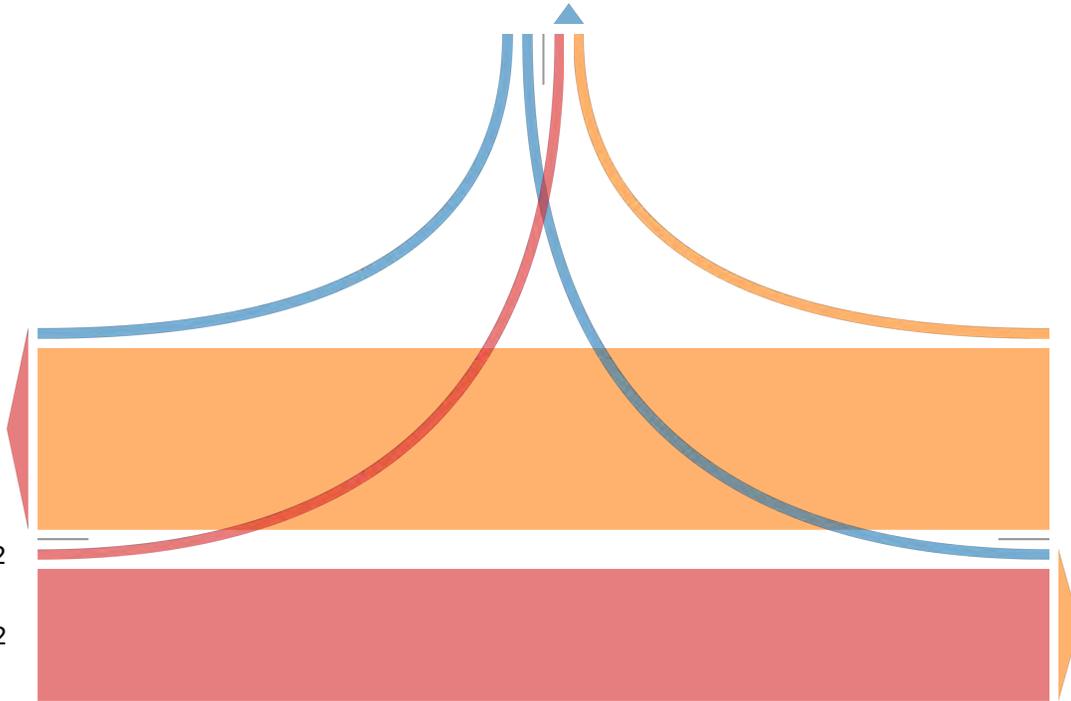
Total: 21

In: 12 Out: 9

84

[W] Peterson Road
Total: 3036
In: 1254 Out: 1782

2
1252



7

1774

Out: 1256 In: 1781
Total: 3037
[E] Peterson Road

Peterson Road with Sunnyview Road TMC - TMC

Tue Nov 12, 2024

Full Length (7 AM-9 AM, 4 PM-6 PM)

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245694, Location: 42.306071, -87.973747



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400, Rosemont, IL, 60018, US

Leg Direction	Access Drive Southbound						Peterson Road Westbound						Sunnyview Road Northbound						Peterson Road Eastbound						Int
	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	
2024-11-12																									
7:00AM	0	0	0	0	0	0	1	218	0	0	219	0	0	0	0	0	0	0	0	380	0	0	380	0	599
7:15AM	0	0	0	0	0	0	1	269	0	0	270	0	0	0	0	0	0	0	0	426	0	0	426	0	696
7:30AM	0	0	1	0	1	0	3	331	0	0	334	0	3	0	0	0	3	0	2	381	0	0	383	0	721
7:45AM	0	0	3	0	3	0	3	281	0	0	284	0	2	0	1	0	3	0	0	409	0	0	409	0	699
Hourly Total	0	0	4	0	4	0	8	1099	0	0	1107	0	5	0	1	0	6	0	2	1596	0	0	1598	0	2715
8:00AM	0	0	0	0	0	0	0	249	1	0	250	0	2	0	1	0	3	0	2	384	1	0	387	0	640
8:15AM	2	0	0	0	2	0	0	231	0	0	231	0	2	0	1	0	3	0	2	367	0	0	369	0	605
8:30AM	0	0	0	0	0	0	1	194	0	1	196	0	1	0	1	0	2	0	1	338	0	0	339	0	537
8:45AM	0	0	1	0	1	0	0	186	0	0	186	0	0	0	0	0	0	0	2	397	0	0	399	0	586
Hourly Total	2	0	1	0	3	0	1	860	1	1	863	0	5	0	3	0	8	0	7	1486	1	0	1494	0	2368
4:00PM	0	0	2	0	2	0	1	417	0	0	418	0	0	0	0	0	0	0	2	345	0	0	347	0	767
4:15PM	0	0	0	0	0	0	1	427	0	0	428	0	0	0	0	0	0	0	2	306	0	0	308	0	736
4:30PM	0	0	1	0	1	0	2	440	1	0	443	0	2	0	1	0	3	0	0	308	0	0	308	0	755
4:45PM	0	0	2	0	2	0	1	435	0	0	436	0	0	0	1	0	1	0	1	280	0	0	281	0	720
Hourly Total	0	0	5	0	5	0	5	1719	1	0	1725	0	2	0	2	0	4	0	5	1239	0	0	1244	0	2978
5:00PM	0	0	1	0	1	0	0	470	0	0	470	0	0	0	1	0	1	0	2	342	0	0	344	0	816
5:15PM	0	0	0	0	0	0	1	433	1	0	435	0	0	0	1	0	1	0	3	317	0	0	320	0	756
5:30PM	0	0	0	0	0	0	0	421	0	0	421	0	0	0	0	0	0	0	0	239	0	0	239	0	660
5:45PM	0	0	1	0	1	0	0	385	2	0	387	0	1	0	0	0	1	0	0	250	0	0	250	0	639
Hourly Total	0	0	2	0	2	0	1	1709	3	0	1713	0	1	0	2	0	3	0	5	1148	0	0	1153	0	2871
Total	2	0	12	0	14	0	15	5387	5	1	5408	0	13	0	8	0	21	0	19	5469	1	0	5489	0	10932
% Approach	14.3%	0%	85.7%	0%	-	-	0.3%	99.6%	0.1%	0%	-	-	61.9%	0%	38.1%	0%	-	-	0.3%	99.6%	0%	0%	-	-	-
% Total	0%	0%	0.1%	0%	0.1%	-	0.1%	49.3%	0%	0%	49.5%	-	0.1%	0%	0.1%	0%	0.2%	-	0.2%	50.0%	0%	0%	50.2%	-	-
Lights	1	0	12	0	13	-	13	5125	5	1	5144	-	11	0	8	0	19	-	12	5230	1	0	5243	-	10419
% Lights	50.0%	0%	100%	0%	92.9%	-	86.7%	95.1%	100%	100%	95.1%	-	84.6%	0%	100%	0%	90.5%	-	63.2%	95.6%	100%	0%	95.5%	-	95.3%
Single-Unit Trucks	0	0	0	0	0	-	0	93	0	0	93	-	1	0	0	0	1	-	2	104	0	0	106	-	200
% Single-Unit Trucks	0%	0%	0%	0%	0%	-	0%	1.7%	0%	0%	1.7%	-	7.7%	0%	0%	0%	4.8%	-	10.5%	1.9%	0%	0%	1.9%	-	1.8%
Articulated Trucks	0	0	0	0	0	-	0	133	0	0	133	-	0	0	0	0	0	-	0	105	0	0	105	-	238
% Articulated Trucks	0%	0%	0%	0%	0%	-	0%	2.5%	0%	0%	2.5%	-	0%	0%	0%	0%	0%	-	0%	1.9%	0%	0%	1.9%	-	2.2%
Buses	1	0	0	0	1	-	2	36	0	0	38	-	1	0	0	0	1	-	5	28	0	0	33	-	73
% Buses	50.0%	0%	0%	0%	7.1%	-	13.3%	0.7%	0%	0%	0.7%	-	7.7%	0%	0%	0%	4.8%	-	26.3%	0.5%	0%	0%	0.6%	-	0.7%
Bicycles on Road	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	2	0	0	2	-	2
% Bicycles on Road	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%
Pedestrians	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	-	0
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

*Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Peterson Road with Sunnyview Road TMC - TMC

Tue Nov 12, 2024

Full Length (7 AM-9 AM, 4 PM-6 PM)

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245694, Location: 42.306071, -87.973747



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400,
Rosemont, IL, 60018, US

[N] Access Drive

Total: 30

In: 14 Out: 16

12

[W] Peterson Road

Total: 10886
In: 5489 Out: 5397

1
5469
19

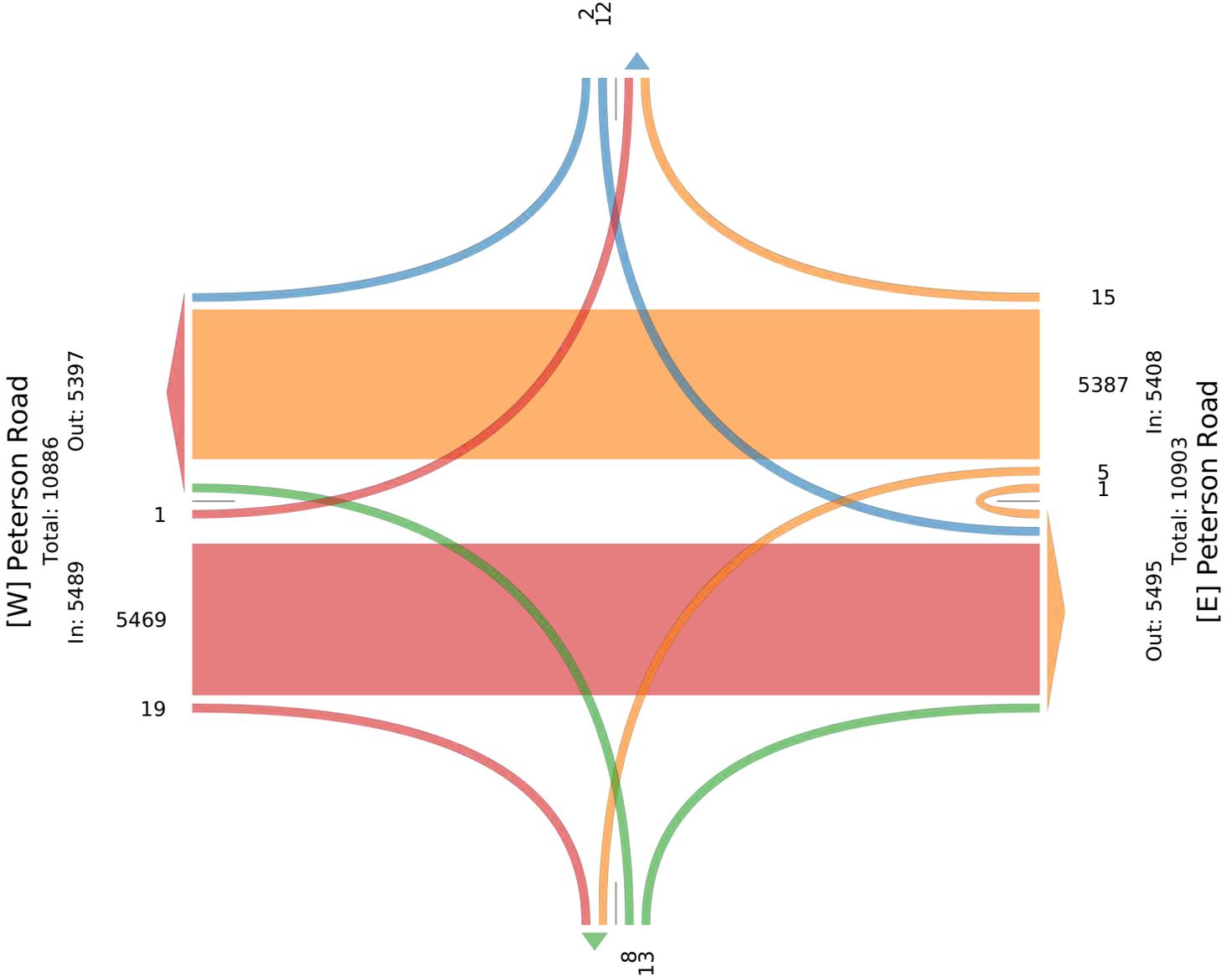
15
5387
15

Total: 10903
In: 5408
Out: 5495
[E] Peterson Road

Out: 24 In: 21
Total: 45

[S] Sunnyview Road

13



Peterson Road with Sunnyview Road TMC - TMC

Tue Nov 12, 2024

AM Peak (7:15 AM - 8:15 AM)

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245694, Location: 42.306071, -87.973747



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Leg Direction	Access Drive Southbound						Peterson Road Westbound						Sunnyview Road Northbound						Peterson Road Eastbound						Int
	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	
2024-11-12 7:15AM	0	0	0	0	0	0	1	269	0	0	270	0	0	0	0	0	0	0	0	426	0	0	426	0	696
7:30AM	0	0	1	0	1	0	3	331	0	0	334	0	3	0	0	0	3	0	2	381	0	0	383	0	721
7:45AM	0	0	3	0	3	0	3	281	0	0	284	0	2	0	1	0	3	0	0	409	0	0	409	0	699
8:00AM	0	0	0	0	0	0	0	249	1	0	250	0	2	0	1	0	3	0	2	384	1	0	387	0	640
Total	0	0	4	0	4	0	7	1130	1	0	1138	0	7	0	2	0	9	0	4	1600	1	0	1605	0	2756
% Approach	0%	0%	100%	0%	-	-	0.6%	99.3%	0.1%	0%	-	-	77.8%	0%	22.2%	0%	-	-	0.2%	99.7%	0.1%	0%	-	-	-
% Total	0%	0%	0.1%	0%	0.1%	-	0.3%	41.0%	0%	0%	41.3%	-	0.3%	0%	0.1%	0%	0.3%	-	0.1%	58.1%	0%	0%	58.2%	-	-
PHF	-	-	0.333	-	0.333	-	0.583	0.853	0.250	-	0.852	-	0.583	-	0.500	-	0.750	-	0.500	0.939	0.250	-	0.942	-	0.956
Lights	0	0	4	0	4	-	7	1052	1	0	1060	-	6	0	2	0	8	-	1	1502	1	0	1504	-	2576
% Lights	0%	0%	100%	0%	100%	-	100%	93.1%	100%	0%	93.1%	-	85.7%	0%	100%	0%	88.9%	-	25.0%	93.9%	100%	0%	93.7%	-	93.5%
Single-Unit Trucks	0	0	0	0	0	-	0	23	0	0	23	-	0	0	0	0	0	-	1	49	0	0	50	-	73
% Single-Unit Trucks	0%	0%	0%	0%	0%	-	0%	2.0%	0%	0%	2.0%	-	0%	0%	0%	0%	0%	-	25.0%	3.1%	0%	0%	3.1%	-	2.6%
Articulated Trucks	0	0	0	0	0	-	0	45	0	0	45	-	0	0	0	0	0	-	0	41	0	0	41	-	86
% Articulated Trucks	0%	0%	0%	0%	0%	-	0%	4.0%	0%	0%	4.0%	-	0%	0%	0%	0%	0%	-	0%	2.6%	0%	0%	2.6%	-	3.1%
Buses	0	0	0	0	0	-	0	10	0	0	10	-	1	0	0	0	1	-	2	8	0	0	10	-	21
% Buses	0%	0%	0%	0%	0%	-	0%	0.9%	0%	0%	0.9%	-	14.3%	0%	0%	0%	11.1%	-	50.0%	0.5%	0%	0%	0.6%	-	0.8%
Bicycles on Road	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0
% Bicycles on Road	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%
Pedestrians	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	0
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

*Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Peterson Road with Sunnyview Road TMC - TMC

Tue Nov 12, 2024

AM Peak (7:15 AM - 8:15 AM)

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All Movements

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[N] Access Drive

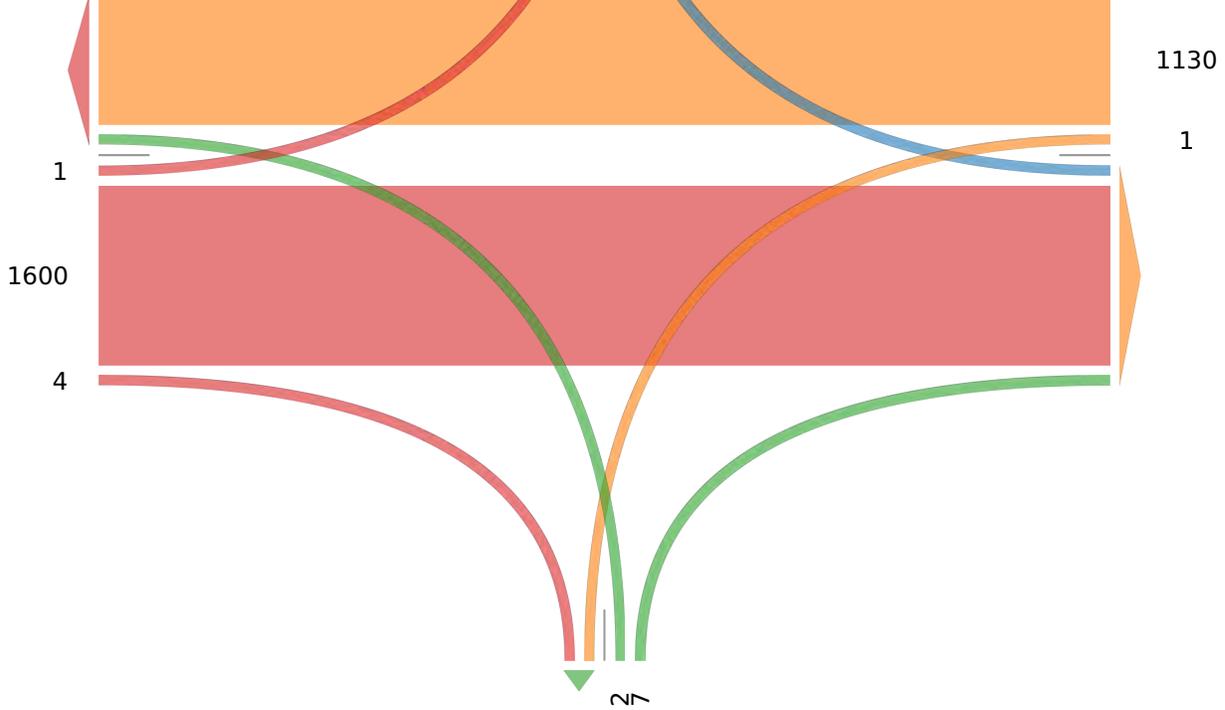
Total: 12

In: 4 Out: 8

4

[W] Peterson Road

Total: 2737
In: 1605 Out: 1132



[E] Peterson Road
In: 1138
Out: 1611
Total: 2749

Out: 5 In: 9

Total: 14

[S] Sunnyview Road

Peterson Road with Sunnyview Road TMC - TMC

Tue Nov 12, 2024

PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

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Leg Direction	Access Drive Southbound						Peterson Road Westbound						Sunnyview Road Northbound						Peterson Road Eastbound						Int
	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	
2024-11-12 4:30PM	0	0	1	0	1	0	2	440	1	0	443	0	2	0	1	0	3	0	0	308	0	0	308	0	755
4:45PM	0	0	2	0	2	0	1	435	0	0	436	0	0	0	1	0	1	0	1	280	0	0	281	0	720
5:00PM	0	0	1	0	1	0	0	470	0	0	470	0	0	0	1	0	1	0	2	342	0	0	344	0	816
5:15PM	0	0	0	0	0	0	1	433	1	0	435	0	0	0	1	0	1	0	3	317	0	0	320	0	756
Total	0	0	4	0	4	0	4	1778	2	0	1784	0	2	0	4	0	6	0	6	1247	0	0	1253	0	3047
% Approach	0%	0%	100%	0%	-	-	0.2%	99.7%	0.1%	0%	-	-	33.3%	0%	66.7%	0%	-	-	0.5%	99.5%	0%	0%	-	-	-
% Total	0%	0%	0.1%	0%	0.1%	-	0.1%	58.4%	0.1%	0%	58.5%	-	0.1%	0%	0.1%	0%	0.2%	-	0.2%	40.9%	0%	0%	41.1%	-	-
PHF	-	-	0.500	-	0.500	-	0.500	0.946	0.500	-	0.949	-	0.250	-	1.000	-	0.500	-	0.500	0.912	-	-	0.911	-	0.934
Lights	0	0	4	0	4	-	4	1738	2	0	1744	-	2	0	4	0	6	-	6	1216	0	0	1222	-	2976
% Lights	0%	0%	100%	0%	100%	-	100%	97.8%	100%	0%	97.8%	-	100%	0%	100%	0%	100%	-	100%	97.5%	0%	0%	97.5%	-	97.7%
Single-Unit Trucks	0	0	0	0	0	-	0	23	0	0	23	-	0	0	0	0	0	-	0	12	0	0	12	-	35
% Single-Unit Trucks	0%	0%	0%	0%	0%	-	0%	1.3%	0%	0%	1.3%	-	0%	0%	0%	0%	0%	-	0%	1.0%	0%	0%	1.0%	-	1.1%
Articulated Trucks	0	0	0	0	0	-	0	15	0	0	15	-	0	0	0	0	0	-	0	17	0	0	17	-	32
% Articulated Trucks	0%	0%	0%	0%	0%	-	0%	0.8%	0%	0%	0.8%	-	0%	0%	0%	0%	0%	-	0%	1.4%	0%	0%	1.4%	-	1.1%
Buses	0	0	0	0	0	-	0	2	0	0	2	-	0	0	0	0	0	-	0	2	0	0	2	-	4
% Buses	0%	0%	0%	0%	0%	-	0%	0.1%	0%	0%	0.1%	-	0%	0%	0%	0%	0%	-	0%	0.2%	0%	0%	0.2%	-	0.1%
Bicycles on Road	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0
% Bicycles on Road	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%
Pedestrians	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	0
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

* Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Peterson Road with Sunnyview Road TMC - TMC

Tue Nov 12, 2024

PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour

All Classes (Lights, Single-Unit Trucks, Articulated Trucks, Buses, Pedestrians, Bicycles on Road)

All Movements

ID: 1245694, Location: 42.306071, -87.973747



Provided by: Kenig Lindgren O'Hara Aboona, Inc.

9575 W. Higgins Rd., Suite 400,
Rosemont, IL, 60018, US

[N] Access Drive

Total: 8
In: 4 Out: 4

4

[W] Peterson Road
Total: 3035
In: 1253 Out: 1782

1247

6

[E] Peterson Road
In: 1784
Out: 1253
Total: 3037

1778

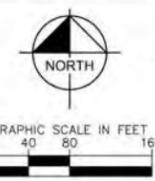
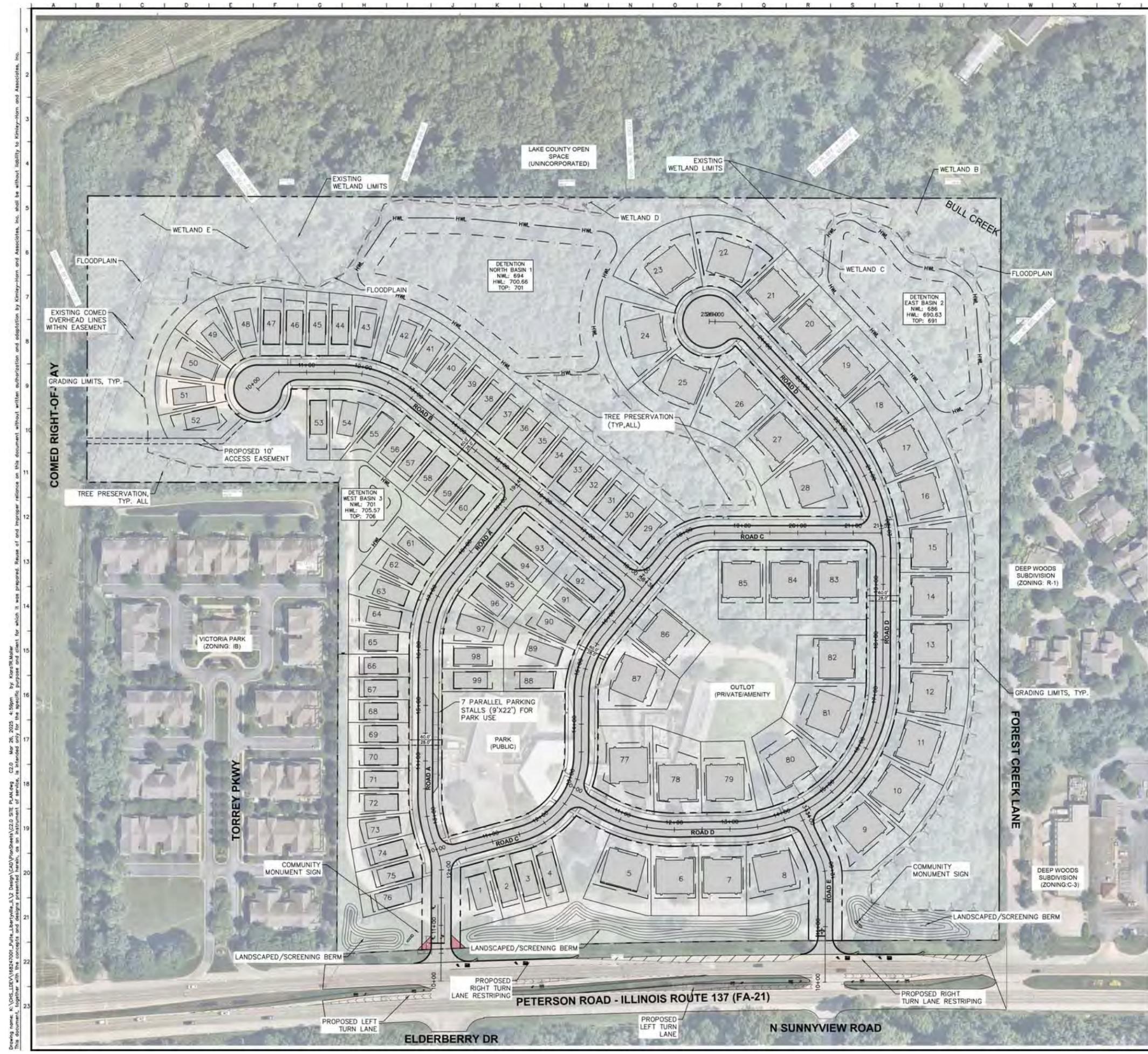
2

Out: 8 In: 6
Total: 14

[S] Sunnyview Road

4 2

Site Plan/Geometric Plan



Call Before You Dig
TOLIE
 1-800-892-0123

GENERAL NOTES

1. ALL DIMENSIONS REFER TO THE BACK OF CURB UNLESS OTHERWISE NOTED.
2. BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
3. REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.
4. REFER TO LANDSCAPE ARCHITECT PLANS FOR MONUMENT SIGN DETAILS.

PROPERTY SUMMARY TABLE

SITE AREA	= 42.11 ACRES
R/W DEDICATION	= 6.55 ACRES
LOT AREA (CUMULATIVE)	= 17.69 ACRES
PARK DISTRICT DEDICATION	= 1.14 ACRES
PRIVATE AMENITY SPACE	= 1.41 ACRES
OPEN SPACE/DETENTION	= 2.92 ACRES
OPEN SPACE/UNDISTURBED AREA	= 12.40 ACRES
TOTAL LOTS PROVIDED	= 99 LOTS
UNIT COUNT	= 134 UNITS

ZONING SUMMARY:
 EXISTING ZONING DISTRICT: (B/R)/C3/UNINCORPORATED
 PROPOSED ZONING DISTRICT: R-7 PUD

SINGLE FAMILY (41' X 110')
 GREENWAY CHASE SPRINGS
 FRONT YARD SETBACK: 20 FEET
 REAR YARD SETBACK: 20 FEET
 CORNER SIDE YARD SETBACK: 15 FEET
 SIDE YARD SETBACK: 5 FEET*
 *(MINIMUM 10 FT BUILDING SEPARATION)

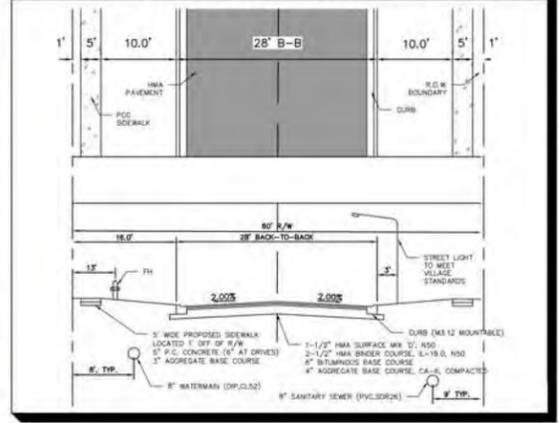
DUPLEXES (86' X 125')
 GREENWAY CHASE LANDINGS
 FRONT YARD SETBACK: 20 FEET
 REAR YARD SETBACK: 30 FEET
 CORNER SIDE YARD SETBACK: 15 FEET
 SIDE YARD SETBACK: 6 FEET*
 *(MINIMUM 12 FT BUILDING SEPARATION)

MAXIMUM BUILDING COVERAGE SINGLE FAMILY = 45%
MAXIMUM IMPERVIOUS COVERAGE SINGLE FAMILY = 50%
MAXIMUM BUILDING COVERAGE DUPLEX = 50%
MAXIMUM IMPERVIOUS COVERAGE DUPLEX = 55%

FEMA NOTE

PER FLOOD INSURANCE RATE MAP PANEL NO. 1709700161K EFFECTIVE DATE 9/19/2013, A PORTION OF THE SITE IS LOCATED IN ZONE "AE" WITH A VARYING BASE FLOOD ELEVATION RANGING FROM 701.0 ON THE WEST SIDE TO 684.0 ON THE EAST SIDE, AS NOTED HEREIN. THE REMAINDER OF THE SITE IS LOCATED WITHIN ZONE "X" (AREA OF MINIMAL FLOOD HAZARD).

TYPICAL ROADWAY CROSS-SECTION



NO.	DATE	BY	REVISIONS
1	03/28/25	DC	VILLAGE RESUBMITTAL
2	03/07/25	DC	ARC RE-SUBMITTAL
3			
4			

Kimley-Horn
 © 2024 KIMLEY-HORN AND ASSOCIATES, INC.
 570 LAKE COOK ROAD, SUITE 200
 NORTON MASSACHUSETTS 01946
 PHONE: 978-260-7804
 WWW.KIMLEY-HORN.COM

SCALE: AS NOTED
 DESIGNED BY: NS
 DRAWN BY: KTRM
 CHECKED BY: RNM

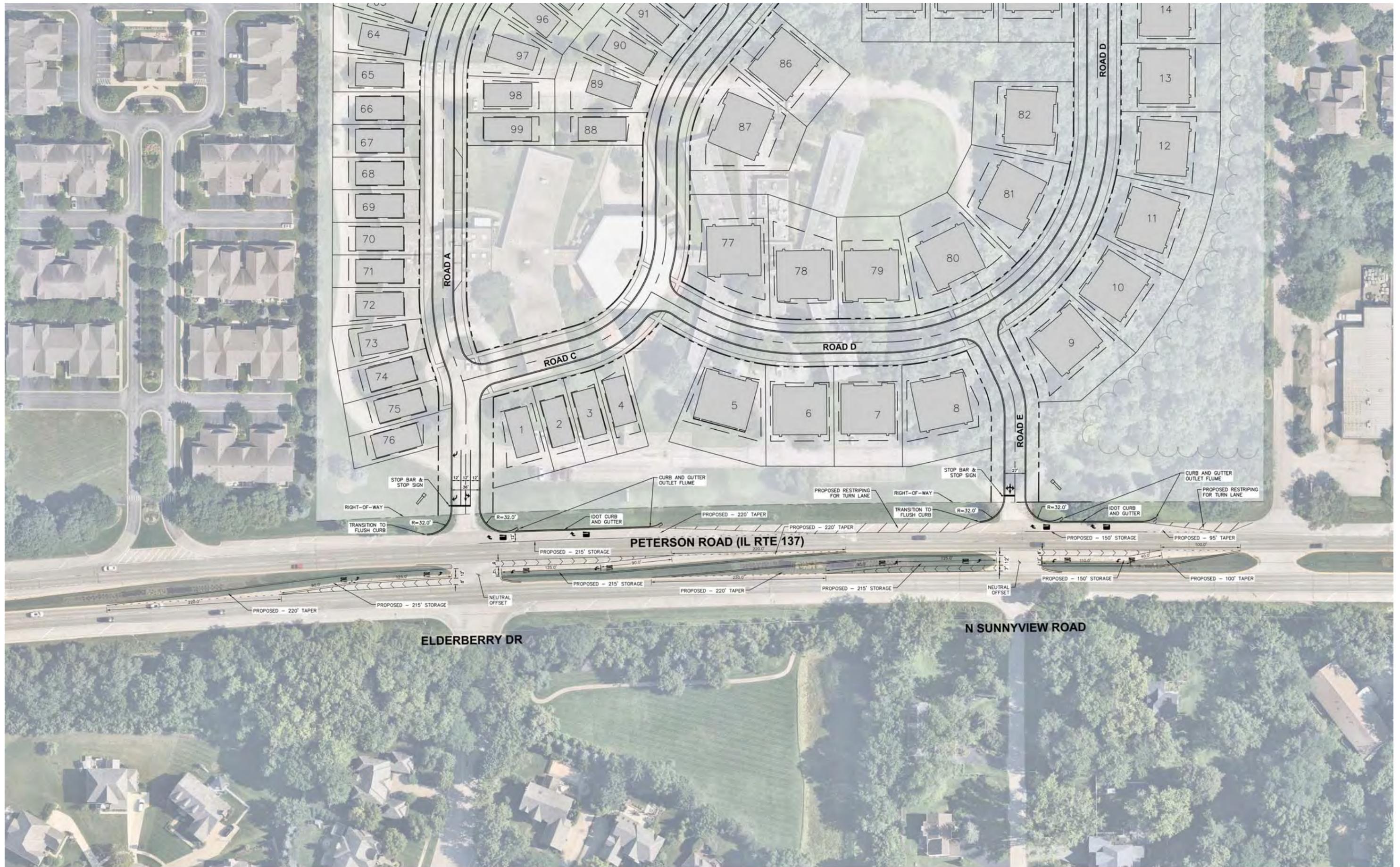
PULTE HOME COMPANY, LLC

OVERALL SITE PLAN

GREENWAY CHASE
 610 PETERSON ROAD
 LIBERTYVILLE, IL 60048

ORIGINAL ISSUE: 01/03/2025
 KHA PROJECT NO. 168247001
 SHEET NUMBER C2.0

Drawing name: \\S:\LDS\168247001_Pulte_Libertyville_IL\33_Design\CAD\Sheet\KHA\02_C2_Site_Plan.dwg
 C2.0 - Mar 26, 2025, 4:58pm - by Kimley-Horn
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ITE Trip Generation Sheets

Land Use: 210

Single-Family Detached Housing

Description

A single-family detached housing site includes any single-family detached home on an individual lot. A typical site surveyed is a suburban subdivision.

Specialized Land Use

Data have been submitted for several single-family detached housing developments with homes that are commonly referred to as patio homes. A patio home is a detached housing unit that is located on a small lot with little (or no) front or back yard. In some subdivisions, communal maintenance of outside grounds is provided for the patio homes. The three patio home sites total 299 dwelling units with overall weighted average trip generation rates of 5.35 vehicle trips per dwelling unit for weekday, 0.26 for the AM adjacent street peak hour, and 0.47 for the PM adjacent street peak hour. These patio home rates based on a small sample of sites are lower than those for single-family detached housing (Land Use 210), lower than those for single-family attached housing (Land Use 251), and higher than those for senior adult housing -- single-family (Land Use 251). Further analysis of this housing type will be conducted in a future edition of *Trip Generation Manual*.

Additional Data

The technical appendices provide supporting information on time-of-day distributions for this land use. The appendices can be accessed through either the ITETripGen web app or the trip generation resource page on the ITE website (<https://www.ite.org/technical-resources/topics/trip-and-parking-generation/>).

For 30 of the study sites, data on the number of residents and number of household vehicles are available. The overall averages for the 30 sites are 3.6 residents per dwelling unit and 1.5 vehicles per dwelling unit.

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in Arizona, California, Connecticut, Delaware, Illinois, Indiana, Kentucky, Maryland, Massachusetts, Minnesota, Montana, New Jersey, North Carolina, Ohio, Ontario (CAN), Oregon, Pennsylvania, South Carolina, South Dakota, Tennessee, Vermont, Virginia, and West Virginia.

Source Numbers

100, 105, 114, 126, 157, 167, 177, 197, 207, 211, 217, 267, 275, 293, 300, 319, 320, 356, 357, 367, 384, 387, 407, 435, 522, 550, 552, 579, 598, 601, 603, 614, 637, 711, 716, 720, 728, 735, 868, 869, 903, 925, 936, 1005, 1007, 1008, 1010, 1033, 1066, 1077, 1078, 1079

Single-Family Detached Housing (210)

Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban

Number of Studies: 174

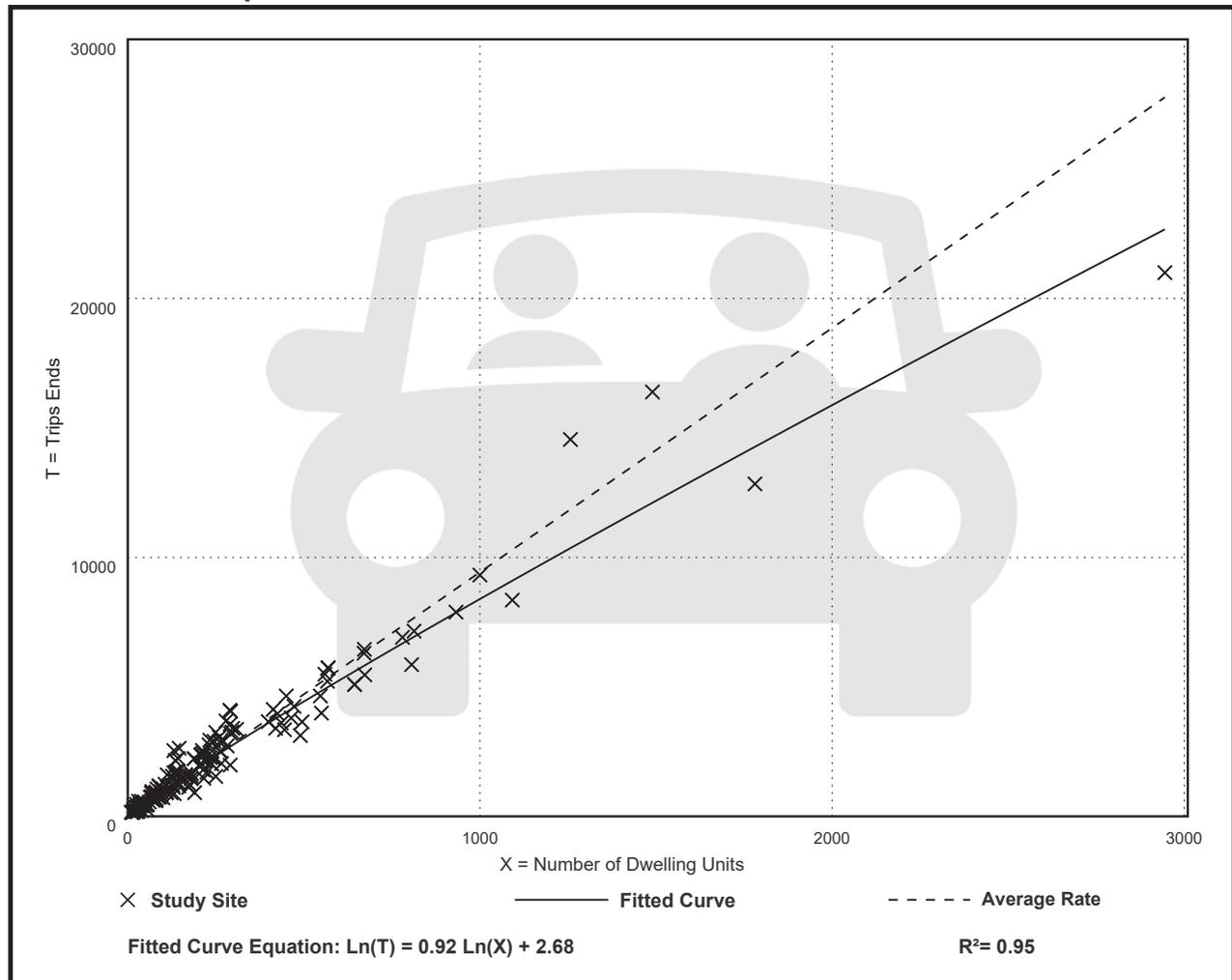
Avg. Num. of Dwelling Units: 246

Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
9.43	4.45 - 22.61	2.13

Data Plot and Equation



Single-Family Detached Housing (210)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 192

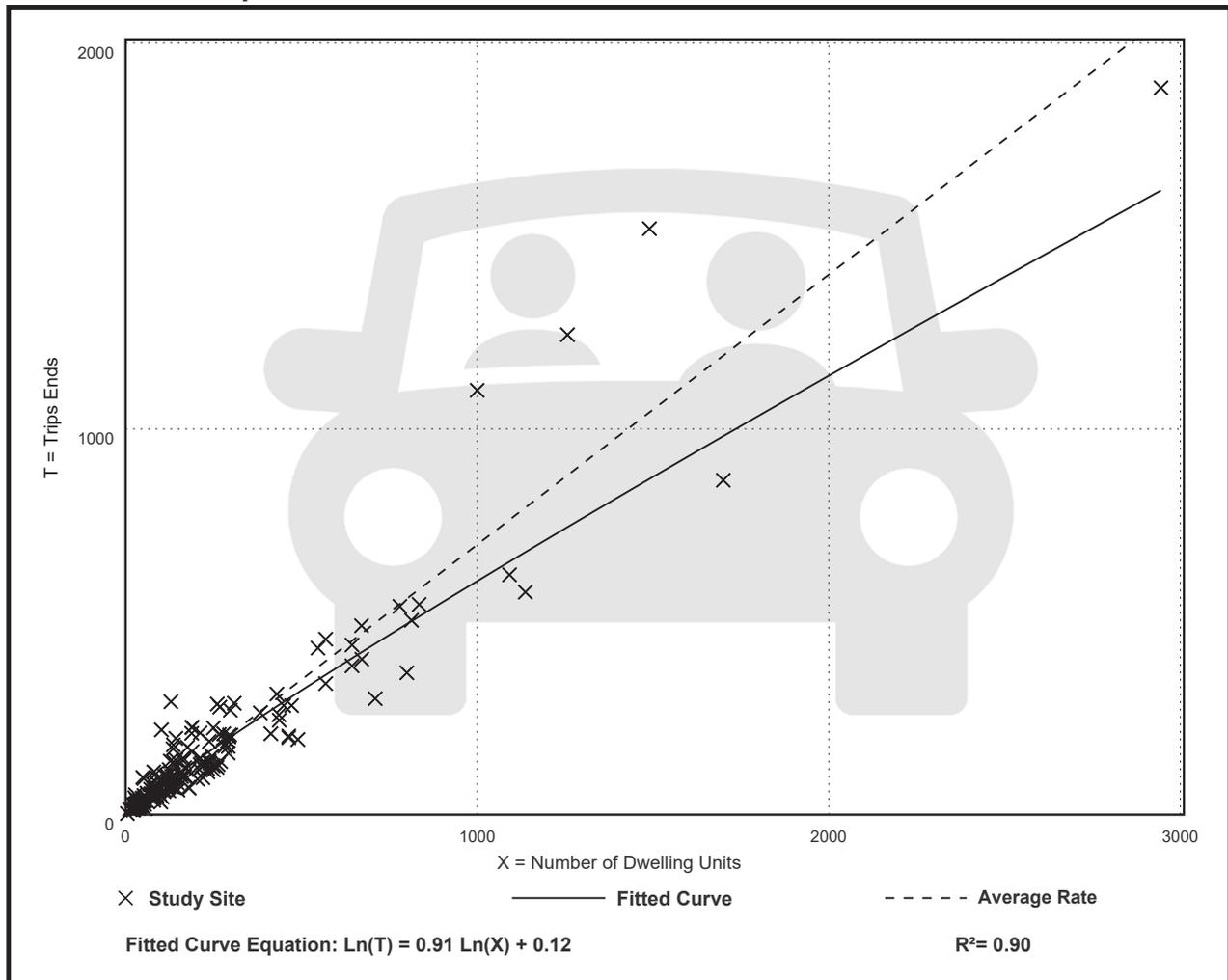
Avg. Num. of Dwelling Units: 226

Directional Distribution: 26% entering, 74% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.70	0.27 - 2.27	0.24

Data Plot and Equation



Single-Family Detached Housing (210)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 208

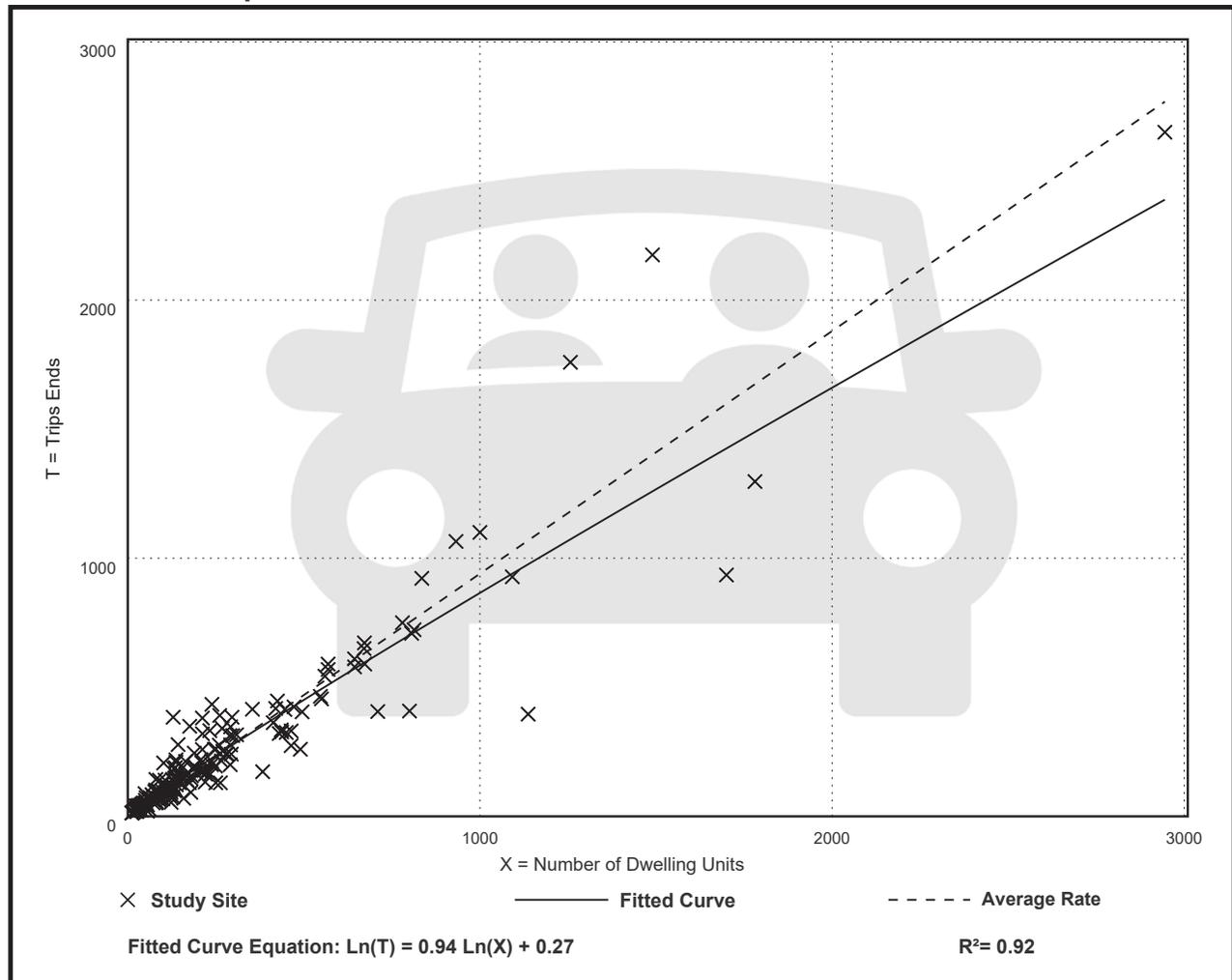
Avg. Num. of Dwelling Units: 248

Directional Distribution: 63% entering, 37% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.94	0.35 - 2.98	0.31

Data Plot and Equation



Land Use: 251

Senior Adult Housing—Single-Family

Description

Senior adult housing—single-family sites are independent living developments that are called various names including retirement communities, age-restricted housing, and active adult communities. The development has a specific age restriction for its residents, typically a minimum of 55 years of age for at least one resident of the household.

Residents in these communities are typically considered active and requiring little to no medical supervision. The percentage of retired residents varies by development. The development may include amenities such as a golf course, swimming pool, 24-hour security, transportation, and common recreational facilities. They generally lack centralized dining and on-site health facilities.

The dwelling units can be either detached or attached. The types of housing types represented by sites in the database include traditional single-family detached homes, patio homes, duplexes, and townhouses. Single-family attached housing includes any single-family housing unit that shares a wall with an adjoining dwelling unit, whether the walls are for living space, a vehicle garage, or storage space.

Senior adult housing—multifamily (Land Use 252), congregate care facility (Land Use 253), assisted living (Land Use 254), and continuing care retirement community (Land Use 255) are related land uses.

Additional Data

Caution should be used when applying trip rates for this land use as it may contain a wide variety of studies ranging from communities with very active, working residents to communities with older, retired residents. As more data become available, consideration will be given to future stratification of this land use.

Many factors affected the trip rates for detached senior adult housing. Factors such as the average age of residents, development location and size, affluence of residents, employment status, and vehicular access should be taken into consideration when conducting an analysis. Some developments were located within close proximity to medical facilities, restaurants, shopping centers, banks, and recreational activities.

The technical appendices provide supporting information on time-of-day distributions for this land use. The appendices can be accessed through either the ITETripGen web app or the trip generation resource page on the ITE website (<https://www.ite.org/technical-resources/topics/trip-and-parking-generation/>).

For the six sites for which data were provided for both occupied dwelling units and total dwelling units, an average of 98 percent of the units were occupied.

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in Alberta (CAN), California, Delaware, Florida, Maryland, New Jersey, New York, Pennsylvania, Virginia, and Washington.

Source Numbers

221, 289, 398, 421, 500, 550, 598, 601, 602, 629, 930, 1015, 1060, 1074

Senior Adult Housing - Single-Family (251)

Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban

Number of Studies: 15

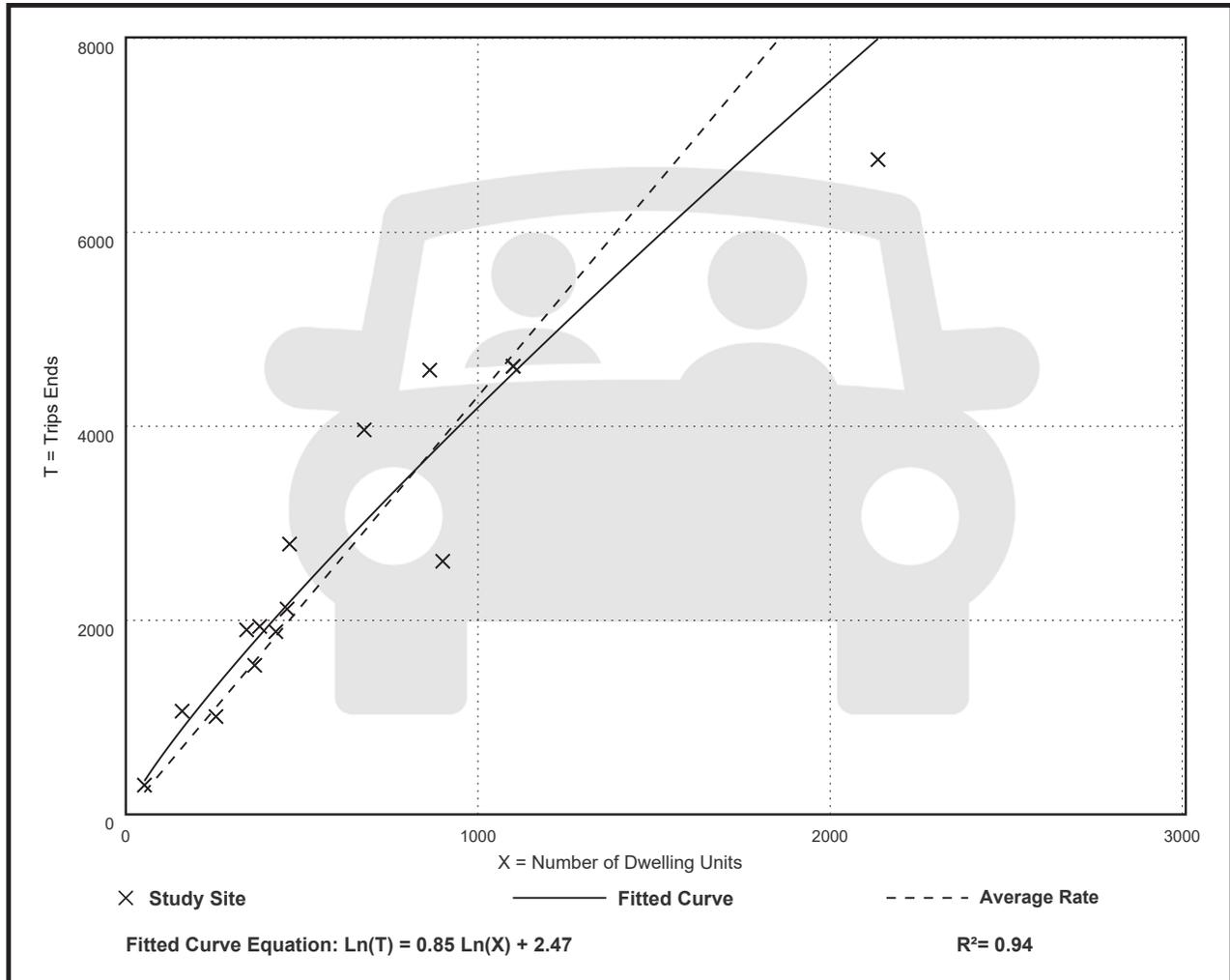
Avg. Num. of Dwelling Units: 646

Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
4.31	2.90 - 6.66	1.07

Data Plot and Equation



Senior Adult Housing - Single-Family (251)

Vehicle Trip Ends vs: Dwelling Units

On a: **Weekday,**

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 34

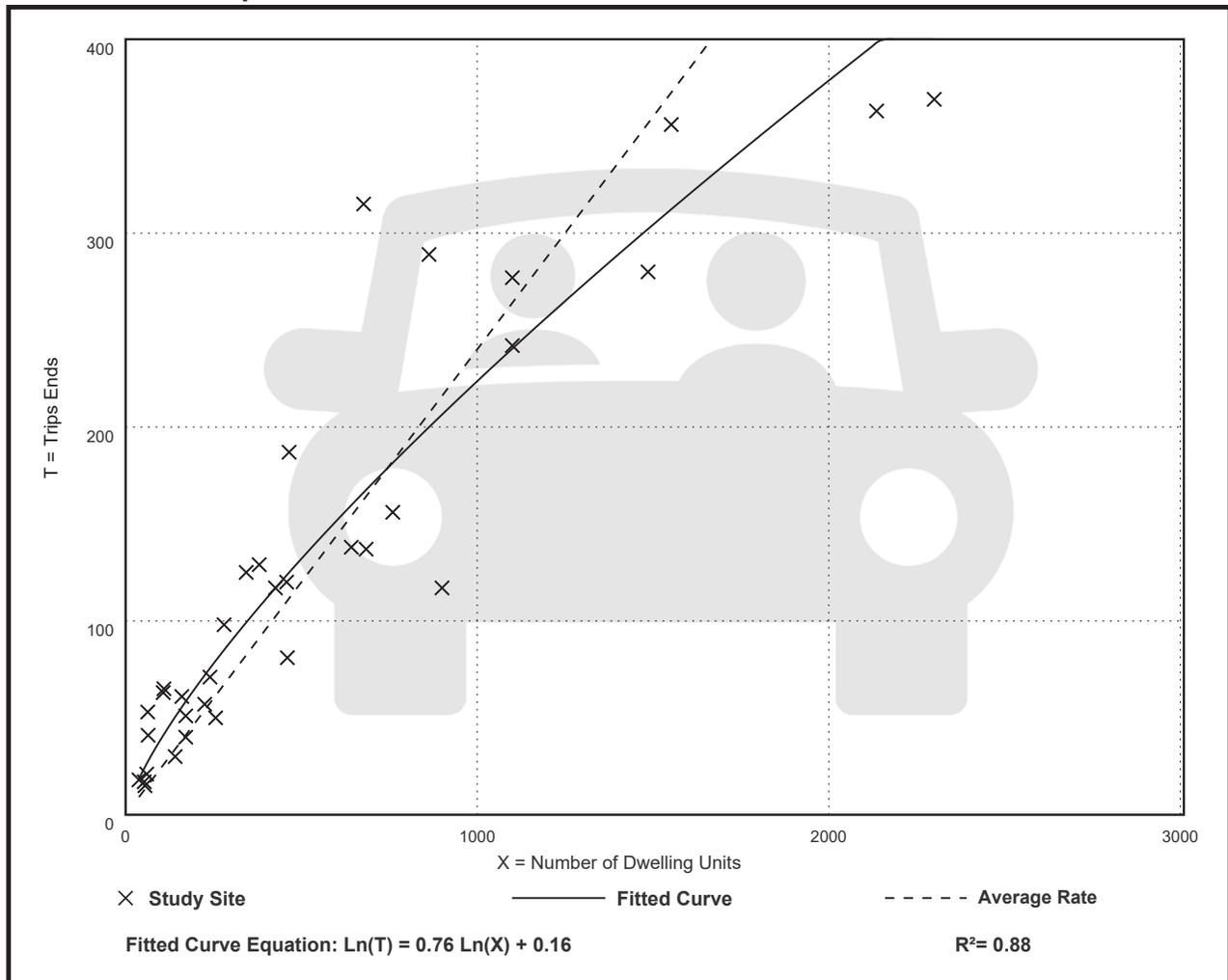
Avg. Num. of Dwelling Units: 557

Directional Distribution: 33% entering, 67% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.24	0.13 - 0.84	0.10

Data Plot and Equation



Senior Adult Housing - Single-Family (251)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 35

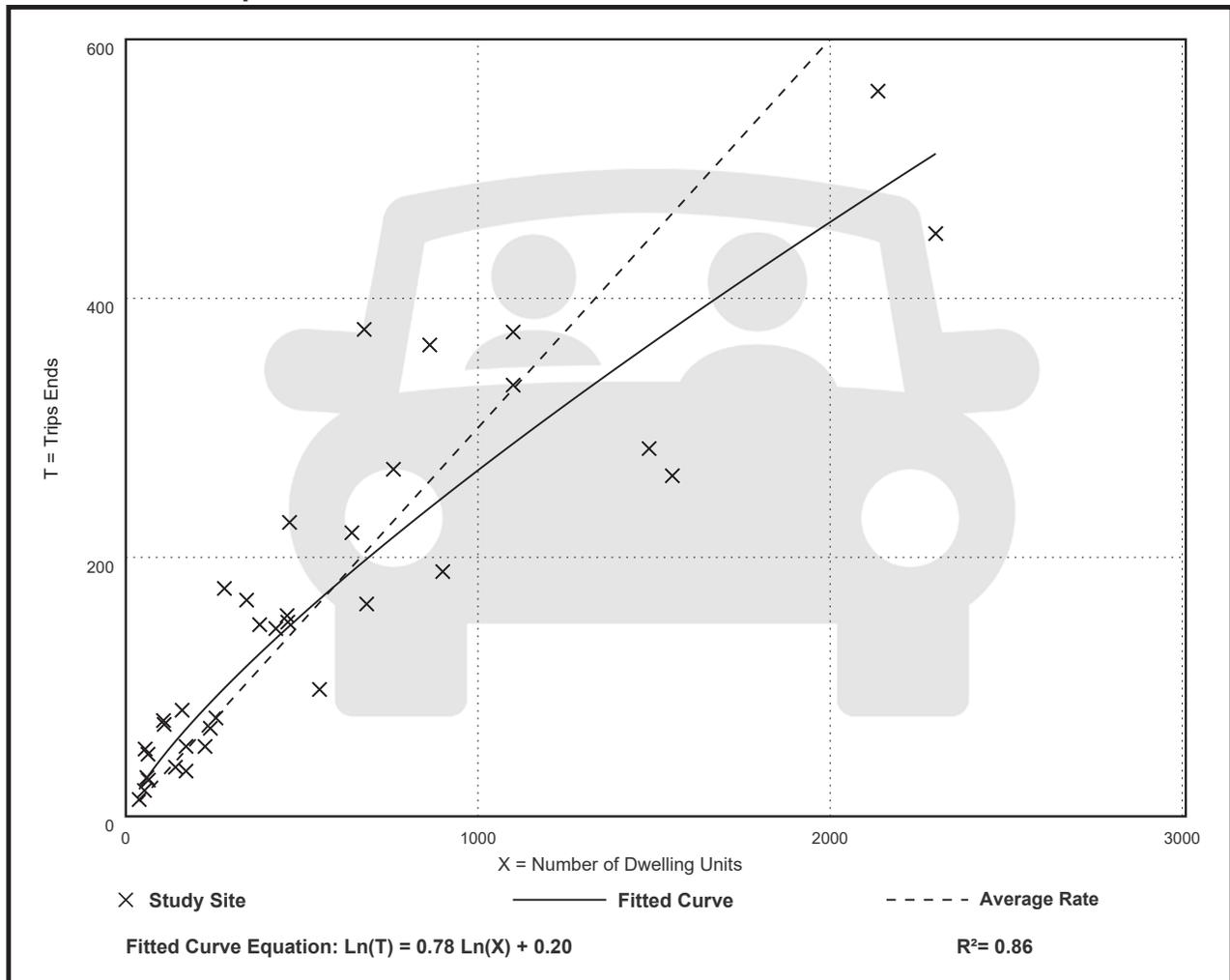
Avg. Num. of Dwelling Units: 556

Directional Distribution: 61% entering, 39% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.30	0.17 - 0.95	0.12

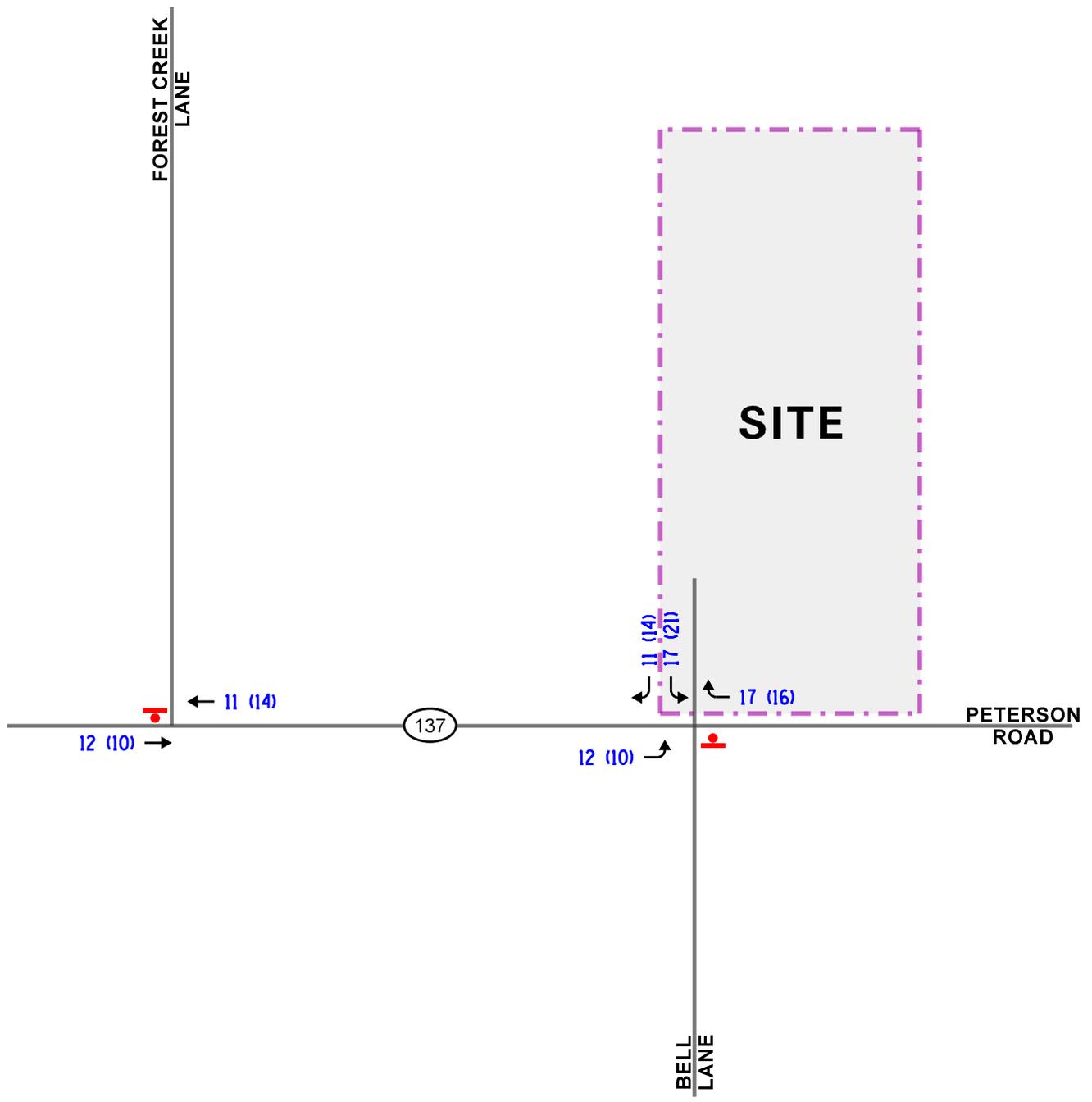
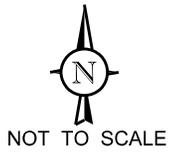
Data Plot and Equation



**Site Traffic Assignment Figures of Adjacent
Developments (From Original Traffic Study)**

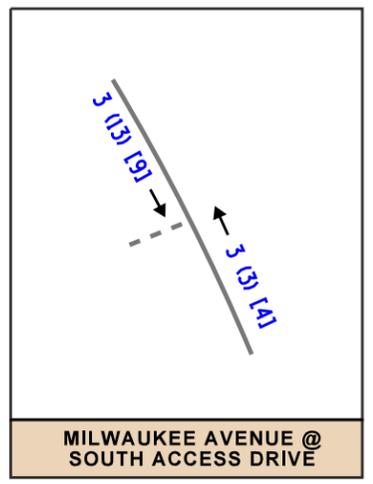
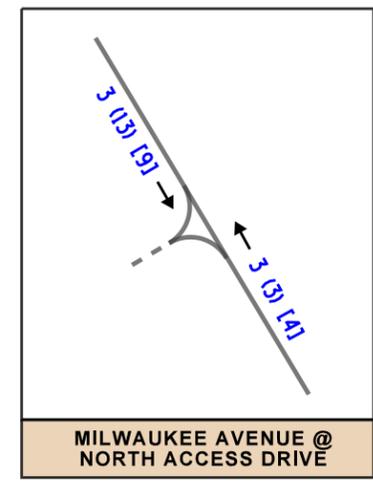
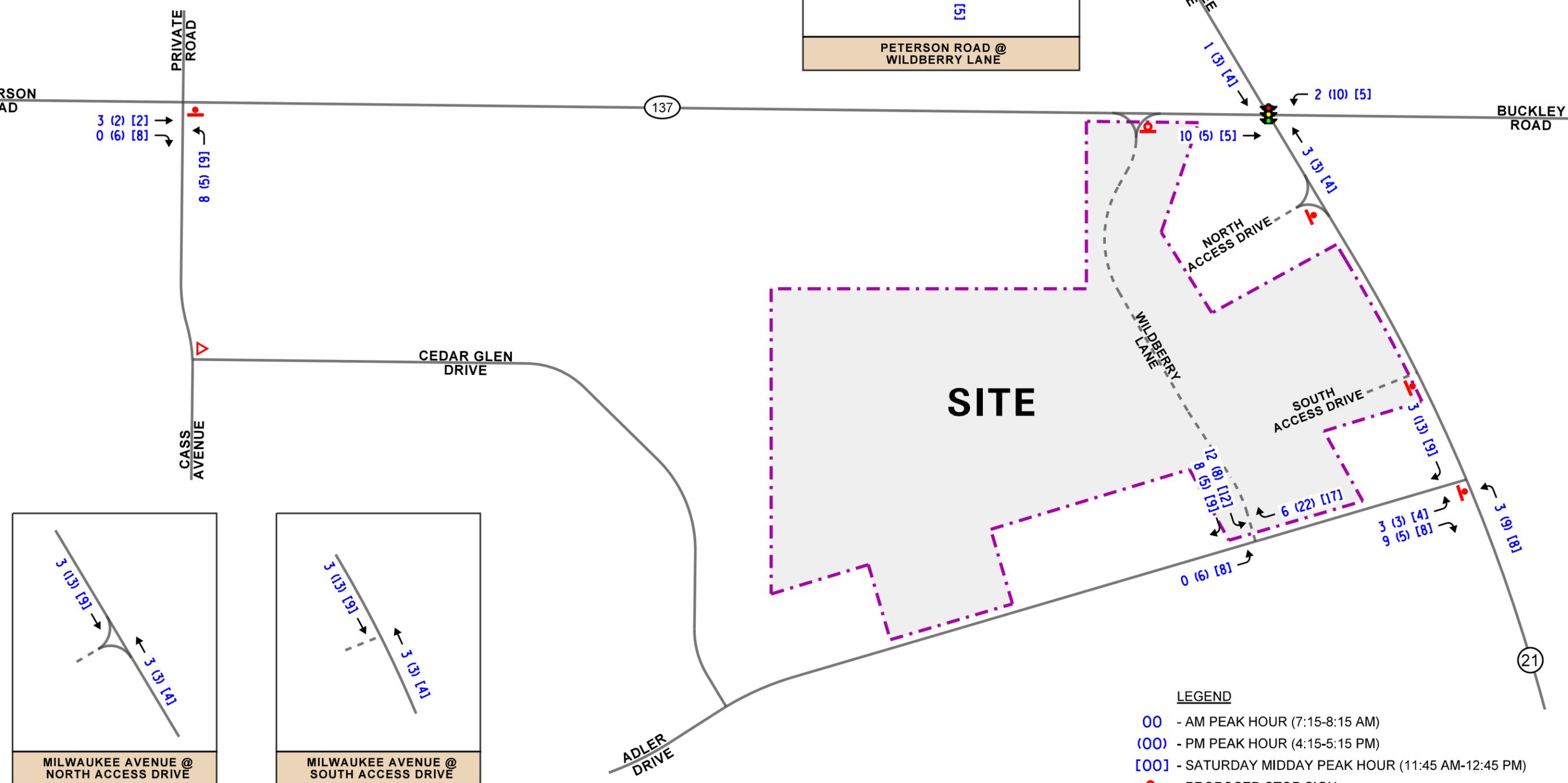
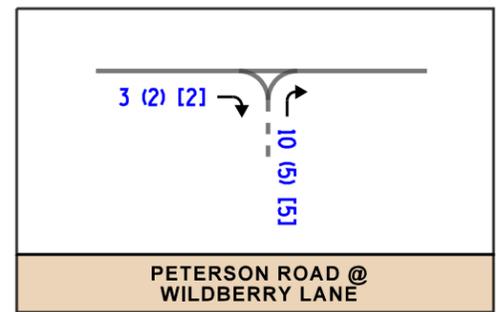
LEGEND

- 00 - AM PEAK HOUR (7:00-8:00 AM)
- (00) - PM PEAK HOUR (4:15-5:15 PM)





NOT TO SCALE



- LEGEND**
- 00 - AM PEAK HOUR (7:15-8:15 AM)
 - (00) - PM PEAK HOUR (4:15-5:15 PM)
 - [00] - SATURDAY MIDDAY PEAK HOUR (11:45 AM-12:45 PM)
 - PROPOSED STOP SIGN

PROPOSED RESIDENTIAL DEVELOPMENT
LIBERTYVILLE, ILLINOIS

ESTIMATED SITE-GENERATED TRAFFIC VOLUMES

Level of Service Criteria

LEVEL OF SERVICE CRITERIA

Signalized Intersections		
Level of Service	Interpretation	Average Control Delay (seconds per vehicle)
A	Favorable progression. Most vehicles arrive during the green indication and travel through the intersection without stopping.	≤ 10
B	Good progression, with more vehicles stopping than for Level of Service A.	$> 10 - 20$
C	Individual cycle failures (i.e., one or more queued vehicles are not able to depart as a result of insufficient capacity during the cycle) may begin to appear. Number of vehicles stopping is significant, although many vehicles still pass through the intersection without stopping.	$> 20 - 35$
D	The volume-to-capacity ratio is high and either progression is ineffective or the cycle length is too long. Many vehicles stop and individual cycle failures are noticeable.	$> 35 - 55$
E	Progression is unfavorable. The volume-to-capacity ratio is high and the cycle length is long. Individual cycle failures are frequent.	$> 55 - 80$
F	The volume-to-capacity ratio is very high, progression is very poor, and the cycle length is long. Most cycles fail to clear the queue.	> 80
Unsignalized Intersections		
Level of Service	Average Total Delay (sec/veh)	
A	0 - 10	
B	$> 10 - 15$	
C	$> 15 - 25$	
D	$> 25 - 35$	
E	$> 35 - 50$	
F	> 50	
Source: <i>Highway Capacity Manual</i> , 6 th Edition.		

Capacity Analysis Summary Sheets
Weekday Morning Peak Hour – Existing Conditions

Lanes, Volumes, Timings

11: Butterfield Square Access Drive & Buckley/Peterson Road

12/11/2024



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↓	↑↑	↓	↑
Traffic Volume (vph)	1488	2	48	1075	16	73
Future Volume (vph)	1488	2	48	1075	16	73
Ideal Flow (vphpl)	2000	1900	1900	2000	1900	1900
Lane Width (ft)	12	12	12	12	12	12
Grade (%)	0%			0%	0%	
Storage Length (ft)		200	270		0	0
Storage Lanes		1	1		1	1
Taper Length (ft)			240		25	
Lane Util. Factor	0.95	1.00	1.00	0.95	1.00	1.00
Ped Bike Factor						
Frt		0.850				0.850
Flt Protected			0.950		0.950	
Satd. Flow (prot)	3551	1077	1736	3551	1517	1568
Flt Permitted			0.950		0.950	
Satd. Flow (perm)	3551	1077	1736	3551	1517	1568
Right Turn on Red		Yes				Yes
Satd. Flow (RTOR)		2				78
Link Speed (mph)	30			30	30	
Link Distance (ft)	990			919	330	
Travel Time (s)	22.5			20.9	7.5	
Confl. Peds. (#/hr)						
Confl. Bikes (#/hr)						
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94
Growth Factor	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	7%	50%	4%	7%	19%	3%
Bus Blockages (#/hr)	0	0	0	0	0	0
Parking (#/hr)						
Mid-Block Traffic (%)	0%			0%	0%	
Shared Lane Traffic (%)						
Lane Group Flow (vph)	1583	2	51	1144	17	78
Turn Type	NA	pm+ov	Prot	NA	Prot	Prot
Protected Phases	2	8	1	6	8	8
Permitted Phases		2				
Detector Phase	2	8	1	6	8	8
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	24.0	24.0	9.5	24.0	24.0	24.0
Total Split (s)	101.0	24.0	15.0	116.0	24.0	24.0
Total Split (%)	72.1%	17.1%	10.7%	82.9%	17.1%	17.1%
Yellow Time (s)	4.0	4.0	3.5	4.0	4.0	4.0
All-Red Time (s)	2.0	2.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.0	6.0	4.5	6.0	6.0	6.0
Lead/Lag	Lag		Lead			
Lead-Lag Optimize?	Yes		Yes			
Recall Mode	C-Max	Max	None	C-Max	Max	Max
Act Effct Green (s)	98.7	123.9	8.9	110.0	18.0	18.0
Actuated g/C Ratio	0.71	0.89	0.06	0.79	0.13	0.13

Lanes, Volumes, Timings

11: Butterfield Square Access Drive & Buckley/Peterson Road

12/11/2024



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
v/c Ratio	0.63	0.00	0.47	0.41	0.09	0.29
Control Delay (s/veh)	13.1	1.0	76.6	5.2	55.1	14.1
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	13.1	1.0	76.6	5.2	55.1	14.1
LOS	B	A	E	A	E	B
Approach Delay (s/veh)	13.1			8.3	21.4	
Approach LOS	B			A	C	
Queue Length 50th (ft)	396	0	45	148	14	0
Queue Length 95th (ft)	477	1	91	176	38	49
Internal Link Dist (ft)	910			839	250	
Turn Bay Length (ft)		200	270			
Base Capacity (vph)	2503	953	130	2790	195	269
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.63	0.00	0.39	0.41	0.09	0.29

Intersection Summary

Area Type:	Other
Cycle Length:	140
Actuated Cycle Length:	140
Offset:	95 (68%), Referenced to phase 2:EBT and 6:WBT, Start of Green
Natural Cycle:	75
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.63
Intersection Signal Delay (s/veh):	11.4
Intersection LOS:	B
Intersection Capacity Utilization:	54.1%
ICU Level of Service:	A
Analysis Period (min):	15

Splits and Phases: 11: Butterfield Square Access Drive & Buckley/Peterson Road



Intersection												
Int Delay, s/veh	0.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↔		↕	↕↔			↕↔			↕↔	
Traffic Vol, veh/h	1	1600	4	1	1133	7	2	0	7	4	0	0
Future Vol, veh/h	1	1600	4	1	1133	7	2	0	7	4	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None									
Storage Length	-	-	-	105	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	96	96	96	96	96	96	96	96	96	96	96	96
Heavy Vehicles, %	0	6	75	0	7	0	0	0	14	0	0	0
Mvmt Flow	1	1667	4	1	1180	7	2	0	7	4	0	0

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1188	0	0	1671	0	0	2263	2860	835	2021	2859	594
Stage 1	-	-	-	-	-	-	1671	1671	-	1186	1186	-
Stage 2	-	-	-	-	-	-	592	1190	-	835	1673	-
Critical Hdwy	4.1	-	-	4.1	-	-	7.5	6.5	7.18	7.5	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.44	3.5	4	3.3
Pot Cap-1 Maneuver	595	-	-	389	-	-	23	17	287	35	17	453
Stage 1	-	-	-	-	-	-	102	154	-	204	265	-
Stage 2	-	-	-	-	-	-	464	264	-	332	154	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	595	-	-	389	-	-	23	17	287	34	17	453
Mov Cap-2 Maneuver	-	-	-	-	-	-	82	96	-	128	95	-
Stage 1	-	-	-	-	-	-	102	154	-	203	264	-
Stage 2	-	-	-	-	-	-	463	263	-	323	153	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s/v	0.04			0.01			25.59			34.03		
HCM LOS							D			D		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	184	2	-	-	389	-	-	128
HCM Lane V/C Ratio	0.051	0.002	-	-	0.003	-	-	0.033
HCM Control Delay (s/veh)	25.6	11.1	0	-	14.3	-	-	34
HCM Lane LOS	D	B	A	-	B	-	-	D
HCM 95th %tile Q(veh)	0.2	0	-	-	0	-	-	0.1

Intersection						
Int Delay, s/veh	0.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↔↔	↔↔		↔	
Traffic Vol, veh/h	2	1609	1135	4	9	6
Future Vol, veh/h	2	1609	1135	4	9	6
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	1	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	95	95	95	95	95	95
Heavy Vehicles, %	50	5	6	0	0	33
Mvmt Flow	2	1694	1195	4	9	6

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	1199	0	-	0	2048 599
Stage 1	-	-	-	-	1197 -
Stage 2	-	-	-	-	851 -
Critical Hdwy	5.1	-	-	-	6.8 7.56
Critical Hdwy Stg 1	-	-	-	-	5.8 -
Critical Hdwy Stg 2	-	-	-	-	5.8 -
Follow-up Hdwy	2.7	-	-	-	3.5 3.63
Pot Cap-1 Maneuver	370	-	-	-	50 375
Stage 1	-	-	-	-	253 -
Stage 2	-	-	-	-	384 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	370	-	-	-	49 375
Mov Cap-2 Maneuver	-	-	-	-	160 -
Stage 1	-	-	-	-	250 -
Stage 2	-	-	-	-	384 -

Approach	EB	WB	SB
HCM Control Delay, s/v	0.18	0	23.81
HCM LOS			C

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	4	-	-	-	207
HCM Lane V/C Ratio	0.006	-	-	-	0.076
HCM Control Delay (s/veh)	14.8	0.2	-	-	23.8
HCM Lane LOS	B	A	-	-	C
HCM 95th %tile Q(veh)	0	-	-	-	0.2

Intersection												
Int Delay, s/veh	0.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↙	↕		↙	↕			↕	↕		↕	
Traffic Vol, veh/h	7	1547	7	19	1110	6	5	0	54	4	0	8
Future Vol, veh/h	7	1547	7	19	1110	6	5	0	54	4	0	8
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None									
Storage Length	110	-	-	145	-	-	-	-	0	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	14	7	0	10	7	0	0	0	4	0	0	12
Mvmt Flow	7	1646	7	20	1181	6	5	0	57	4	0	9

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1187	0	0	1653	0	0	2295	2892	827	2062	2893	594
Stage 1	-	-	-	-	-	-	1664	1664	-	1224	1224	-
Stage 2	-	-	-	-	-	-	631	1228	-	838	1668	-
Critical Hdwy	4.38	-	-	4.3	-	-	7.5	6.5	6.98	7.5	6.5	7.14
Critical Hdwy Stg 1	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Follow-up Hdwy	2.34	-	-	2.3	-	-	3.5	4	3.34	3.5	4	3.42
Pot Cap-1 Maneuver	521	-	-	352	-	-	22	16	311	32	16	424
Stage 1	-	-	-	-	-	-	103	155	-	193	254	-
Stage 2	-	-	-	-	-	-	441	253	-	331	155	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	521	-	-	352	-	-	20	15	311	25	15	424
Mov Cap-2 Maneuver	-	-	-	-	-	-	79	91	-	106	84	-
Stage 1	-	-	-	-	-	-	101	153	-	182	239	-
Stage 2	-	-	-	-	-	-	407	238	-	266	152	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s/v	0.05			0.27			22.11			23.02		
HCM LOS							C			C		

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	79	311	521	-	-	352	-	-	213
HCM Lane V/C Ratio	0.067	0.185	0.014	-	-	0.057	-	-	0.06
HCM Control Delay (s/veh)	53.6	19.2	12	-	-	15.9	-	-	23
HCM Lane LOS	F	C	B	-	-	C	-	-	C
HCM 95th %tile Q(veh)	0.2	0.7	0	-	-	0.2	-	-	0.2

Capacity Analysis Summary Sheets
Weekday Evening Peak Hour – Existing Conditions

Lanes, Volumes, Timings

11: Butterfield Square Access Drive & Buckley/Peterson Road

12/11/2024



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↗	↘	↑↑	↘	↗
Traffic Volume (vph)	1196	2	104	1666	50	53
Future Volume (vph)	1196	2	104	1666	50	53
Ideal Flow (vphpl)	2000	1900	1900	2000	1900	1900
Lane Width (ft)	12	12	12	12	12	12
Grade (%)	0%			0%	0%	
Storage Length (ft)		200	270		0	0
Storage Lanes		1	1		1	1
Taper Length (ft)			240		25	
Lane Util. Factor	0.95	1.00	1.00	0.95	1.00	1.00
Ped Bike Factor						
Frt		0.850				0.850
Flt Protected			0.950		0.950	
Satd. Flow (prot)	3725	1615	1805	3689	1805	1615
Flt Permitted			0.950		0.950	
Satd. Flow (perm)	3725	1615	1805	3689	1805	1615
Right Turn on Red		Yes				Yes
Satd. Flow (RTOR)		2				57
Link Speed (mph)	30			30	30	
Link Distance (ft)	990			919	330	
Travel Time (s)	22.5			20.9	7.5	
Confl. Peds. (#/hr)						
Confl. Bikes (#/hr)						
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93
Growth Factor	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	2%	0%	0%	3%	0%	0%
Bus Blockages (#/hr)	0	0	0	0	0	0
Parking (#/hr)						
Mid-Block Traffic (%)	0%			0%	0%	
Shared Lane Traffic (%)						
Lane Group Flow (vph)	1286	2	112	1791	54	57
Turn Type	NA	pm+ov	Prot	NA	Prot	Perm
Protected Phases	2	8	1	6	8	
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	8
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	24.0	24.0	9.5	24.0	24.0	24.0
Total Split (s)	92.0	28.0	20.0	112.0	28.0	28.0
Total Split (%)	65.7%	20.0%	14.3%	80.0%	20.0%	20.0%
Yellow Time (s)	4.0	4.0	3.5	4.0	4.0	4.0
All-Red Time (s)	2.0	2.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.0	6.0	4.5	6.0	6.0	6.0
Lead/Lag	Lag		Lead			
Lead-Lag Optimize?	Yes		Yes			
Recall Mode	C-Min	None	None	C-Min	None	None
Act Effct Green (s)	102.4	117.9	13.9	122.0	9.6	9.6
Actuated g/C Ratio	0.73	0.84	0.10	0.87	0.07	0.07

Lanes, Volumes, Timings

11: Butterfield Square Access Drive & Buckley/Peterson Road

12/11/2024



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
v/c Ratio	0.47	0.00	0.63	0.56	0.44	0.35
Control Delay (s/veh)	9.6	2.0	75.0	3.9	72.8	20.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	9.6	2.0	75.0	3.9	72.8	20.2
LOS	A	A	E	A	E	C
Approach Delay (s/veh)	9.6			8.1	45.8	
Approach LOS	A			A	D	
Queue Length 50th (ft)	245	0	100	198	48	0
Queue Length 95th (ft)	362	2	160	283	93	44
Internal Link Dist (ft)	910			839	250	
Turn Bay Length (ft)		200	270			
Base Capacity (vph)	2724	1484	210	3215	283	301
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.47	0.00	0.53	0.56	0.19	0.19

Intersection Summary

Area Type:	Other
Cycle Length:	140
Actuated Cycle Length:	140
Offset:	0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
Natural Cycle:	65
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.63
Intersection Signal Delay (s/veh):	9.9
Intersection LOS:	A
Intersection Capacity Utilization:	57.9%
ICU Level of Service:	B
Analysis Period (min):	15

Splits and Phases: 11: Butterfield Square Access Drive & Buckley/Peterson Road



Intersection												
Int Delay, s/veh	0.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔		↔	↔			↔			↔	
Traffic Vol, veh/h	0	1247	6	2	1776	4	4	0	2	4	0	0
Future Vol, veh/h	0	1247	6	2	1776	4	4	0	2	4	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None									
Storage Length	-	-	-	105	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	93	93	93	93	93	93	93	93	93	93	93	93
Heavy Vehicles, %	0	2	0	0	2	0	0	0	0	0	0	0
Mvmt Flow	0	1341	6	2	1910	4	4	0	2	4	0	0

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1914	0	0	1347	0	0	2303	3262	674	2587	3263	957
Stage 1	-	-	-	-	-	-	1344	1344	-	1916	1916	-
Stage 2	-	-	-	-	-	-	959	1918	-	670	1347	-
Critical Hdwy	4.1	-	-	4.1	-	-	7.5	6.5	6.9	7.5	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.3	3.5	4	3.3
Pot Cap-1 Maneuver	314	-	-	518	-	-	21	9	402	13	9	262
Stage 1	-	-	-	-	-	-	163	222	-	71	116	-
Stage 2	-	-	-	-	-	-	280	116	-	417	222	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	314	-	-	518	-	-	21	9	402	13	9	262
Mov Cap-2 Maneuver	-	-	-	-	-	-	103	73	-	58	73	-
Stage 1	-	-	-	-	-	-	163	222	-	71	116	-
Stage 2	-	-	-	-	-	-	279	116	-	415	222	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s/v	0			0.01			32.65			71.91		
HCM LOS							D			F		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	137	314	-	-	518	-	-	58
HCM Lane V/C Ratio	0.047	-	-	-	0.004	-	-	0.074
HCM Control Delay (s/veh)	32.7	0	-	-	12	-	-	71.9
HCM Lane LOS	D	A	-	-	B	-	-	F
HCM 95th %tile Q(veh)	0.1	0	-	-	0	-	-	0.2

Intersection						
Int Delay, s/veh	0.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕↕	↕↕		↕	
Traffic Vol, veh/h	2	1251	1774	7	4	8
Future Vol, veh/h	2	1251	1774	7	4	8
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	1	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	93	93	93	93	93	93
Heavy Vehicles, %	0	2	3	0	0	0
Mvmt Flow	2	1345	1908	8	4	9

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	1915	0	-	0	2588 958
Stage 1	-	-	-	-	1911 -
Stage 2	-	-	-	-	677 -
Critical Hdwy	4.1	-	-	-	6.8 6.9
Critical Hdwy Stg 1	-	-	-	-	5.8 -
Critical Hdwy Stg 2	-	-	-	-	5.8 -
Follow-up Hdwy	2.2	-	-	-	3.5 3.3
Pot Cap-1 Maneuver	314	-	-	-	21 262
Stage 1	-	-	-	-	104 -
Stage 2	-	-	-	-	472 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	314	-	-	-	21 262
Mov Cap-2 Maneuver	-	-	-	-	82 -
Stage 1	-	-	-	-	103 -
Stage 2	-	-	-	-	472 -

Approach	EB	WB	SB
HCM Control Delay, s/v	0.21	0	30.92
HCM LOS			D

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	6	-	-	-	152
HCM Lane V/C Ratio	0.007	-	-	-	0.085
HCM Control Delay (s/veh)	16.6	0.2	-	-	30.9
HCM Lane LOS	C	A	-	-	D
HCM 95th %tile Q(veh)	0	-	-	-	0.3

Intersection												
Int Delay, s/veh	0.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↙	↕		↙	↕			↕	↗		↕	
Traffic Vol, veh/h	6	1222	21	27	1752	1	3	0	31	0	0	15
Future Vol, veh/h	6	1222	21	27	1752	1	3	0	31	0	0	15
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None									
Storage Length	110	-	-	145	-	-	-	-	0	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	93	93	93	93	93	93	93	93	93	93	93	93
Heavy Vehicles, %	0	2	0	0	3	0	0	0	3	0	0	0
Mvmt Flow	6	1314	23	29	1884	1	3	0	33	0	0	16

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1885	0	0	1337	0	0	2338	3281	668	2612	3292	942
Stage 1	-	-	-	-	-	-	1338	1338	-	1942	1942	-
Stage 2	-	-	-	-	-	-	1000	1943	-	670	1349	-
Critical Hdwy	4.1	-	-	4.1	-	-	7.5	6.5	6.96	7.5	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.33	3.5	4	3.3
Pot Cap-1 Maneuver	322	-	-	523	-	-	20	9	398	12	9	268
Stage 1	-	-	-	-	-	-	164	224	-	69	113	-
Stage 2	-	-	-	-	-	-	264	113	-	418	221	-
Platoon blocked, %		-	-	-	-	-						
Mov Cap-1 Maneuver	322	-	-	523	-	-	17	8	398	10	8	268
Mov Cap-2 Maneuver	-	-	-	-	-	-	94	66	-	52	66	-
Stage 1	-	-	-	-	-	-	161	219	-	65	107	-
Stage 2	-	-	-	-	-	-	235	107	-	375	217	-

Approach	EB	WB	NB	SB
HCM Control Delay, s/v	0.08	0.19	17.5	19.32
HCM LOS			C	C

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	94	398	322	-	-	523	-	-	268
HCM Lane V/C Ratio	0.034	0.084	0.02	-	-	0.056	-	-	0.06
HCM Control Delay (s/veh)	44.7	14.9	16.4	-	-	12.3	-	-	19.3
HCM Lane LOS	E	B	C	-	-	B	-	-	C
HCM 95th %tile Q(veh)	0.1	0.3	0.1	-	-	0.2	-	-	0.2

Capacity Analysis Summary Sheets
Weekday Morning Peak Hour – No-Build Conditions

Lanes, Volumes, Timings

11: Butterfield Square Access Drive & Buckley/Peterson Road

03/26/2025



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↓	↑↑	↓	↑
Traffic Volume (vph)	1547	2	48	1139	16	73
Future Volume (vph)	1547	2	48	1139	16	73
Ideal Flow (vphpl)	2000	1900	1900	2000	1900	1900
Lane Width (ft)	12	12	12	12	12	12
Grade (%)	0%			0%	0%	
Storage Length (ft)		200	270		0	0
Storage Lanes		1	1		1	1
Taper Length (ft)			240		25	
Lane Util. Factor	0.95	1.00	1.00	0.95	1.00	1.00
Ped Bike Factor						
Frt		0.850				0.850
Flt Protected			0.950		0.950	
Satd. Flow (prot)	3551	1077	1736	3551	1517	1568
Flt Permitted			0.950		0.950	
Satd. Flow (perm)	3551	1077	1736	3551	1517	1568
Right Turn on Red		Yes				Yes
Satd. Flow (RTOR)		2				78
Link Speed (mph)	30			30	30	
Link Distance (ft)	990			919	330	
Travel Time (s)	22.5			20.9	7.5	
Confl. Peds. (#/hr)						
Confl. Bikes (#/hr)						
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94
Growth Factor	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	7%	50%	4%	7%	19%	3%
Bus Blockages (#/hr)	0	0	0	0	0	0
Parking (#/hr)						
Mid-Block Traffic (%)	0%			0%	0%	
Shared Lane Traffic (%)						
Lane Group Flow (vph)	1646	2	51	1212	17	78
Turn Type	NA	pm+ov	Prot	NA	Prot	Prot
Protected Phases	2	8	1	6	8	8
Permitted Phases		2				
Detector Phase	2	8	1	6	8	8
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	24.0	24.0	9.5	24.0	24.0	24.0
Total Split (s)	101.0	24.0	15.0	116.0	24.0	24.0
Total Split (%)	72.1%	17.1%	10.7%	82.9%	17.1%	17.1%
Yellow Time (s)	4.0	4.0	3.5	4.0	4.0	4.0
All-Red Time (s)	2.0	2.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.0	6.0	4.5	6.0	6.0	6.0
Lead/Lag	Lag		Lead			
Lead-Lag Optimize?	Yes		Yes			
Recall Mode	C-Max	Max	None	C-Max	Max	Max
Act Effct Green (s)	98.7	123.9	8.9	110.0	18.0	18.0
Actuated g/C Ratio	0.71	0.89	0.06	0.79	0.13	0.13

Lanes, Volumes, Timings

11: Butterfield Square Access Drive & Buckley/Peterson Road

03/26/2025



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
v/c Ratio	0.66	0.00	0.47	0.43	0.09	0.29
Control Delay (s/veh)	13.6	1.0	76.6	5.4	55.1	14.1
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	13.6	1.0	76.6	5.4	55.1	14.1
LOS	B	A	E	A	E	B
Approach Delay (s/veh)	13.6			8.3	21.4	
Approach LOS	B			A	C	
Queue Length 50th (ft)	426	0	45	162	14	0
Queue Length 95th (ft)	512	1	91	191	38	49
Internal Link Dist (ft)	910			839	250	
Turn Bay Length (ft)		200	270			
Base Capacity (vph)	2503	953	130	2790	195	269
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.66	0.00	0.39	0.43	0.09	0.29

Intersection Summary

Area Type:	Other
Cycle Length:	140
Actuated Cycle Length:	140
Offset:	95 (68%), Referenced to phase 2:EBT and 6:WBT, Start of Green
Natural Cycle:	80
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.66
Intersection Signal Delay (s/veh):	11.6
Intersection LOS:	B
Intersection Capacity Utilization:	55.1%
ICU Level of Service:	B
Analysis Period (min):	15

Splits and Phases: 11: Butterfield Square Access Drive & Buckley/Peterson Road



Intersection												
Int Delay, s/veh	0.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔↔		↗	↕↔			↕↔			↕↔	
Traffic Vol, veh/h	1	1659	4	1	1197	7	2	0	7	4	0	0
Future Vol, veh/h	1	1659	4	1	1197	7	2	0	7	4	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None									
Storage Length	-	-	-	105	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	96	96	96	96	96	96	96	96	96	96	96	96
Heavy Vehicles, %	0	6	75	0	7	0	0	0	14	0	0	0
Mvmt Flow	1	1728	4	1	1247	7	2	0	7	4	0	0

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1254	0	0	1732	0	0	2358	2989	866	2119	2987	627
Stage 1	-	-	-	-	-	-	1732	1732	-	1253	1253	-
Stage 2	-	-	-	-	-	-	626	1256	-	866	1734	-
Critical Hdwy	4.1	-	-	4.1	-	-	7.5	6.5	7.18	7.5	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.44	3.5	4	3.3
Pot Cap-1 Maneuver	562	-	-	369	-	-	19	14	273	29	14	431
Stage 1	-	-	-	-	-	-	93	144	-	185	246	-
Stage 2	-	-	-	-	-	-	444	245	-	319	143	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	562	-	-	369	-	-	19	14	273	28	14	431
Mov Cap-2 Maneuver	-	-	-	-	-	-	75	88	-	117	88	-
Stage 1	-	-	-	-	-	-	93	143	-	185	245	-
Stage 2	-	-	-	-	-	-	442	244	-	309	143	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s/v	0.05			0.01			27.14			36.77		
HCM LOS							D			E		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	172	2	-	-	369	-	-	117
HCM Lane V/C Ratio	0.055	0.002	-	-	0.003	-	-	0.035
HCM Control Delay (s/veh)	27.1	11.4	0	-	14.8	-	-	36.8
HCM Lane LOS	D	B	A	-	B	-	-	E
HCM 95th %tile Q(veh)	0.2	0	-	-	0	-	-	0.1

Intersection						
Int Delay, s/veh	0.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↔↔	↔↔		↔	
Traffic Vol, veh/h	2	1668	1199	4	9	6
Future Vol, veh/h	2	1668	1199	4	9	6
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	1	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	95	95	95	95	95	95
Heavy Vehicles, %	50	5	6	0	0	33
Mvmt Flow	2	1756	1262	4	9	6

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	1266	0	-	0	2146 633
Stage 1	-	-	-	-	1264 -
Stage 2	-	-	-	-	882 -
Critical Hdwy	5.1	-	-	-	6.8 7.56
Critical Hdwy Stg 1	-	-	-	-	5.8 -
Critical Hdwy Stg 2	-	-	-	-	5.8 -
Follow-up Hdwy	2.7	-	-	-	3.5 3.63
Pot Cap-1 Maneuver	343	-	-	-	43 355
Stage 1	-	-	-	-	233 -
Stage 2	-	-	-	-	370 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	343	-	-	-	42 355
Mov Cap-2 Maneuver	-	-	-	-	148 -
Stage 1	-	-	-	-	230 -
Stage 2	-	-	-	-	370 -

Approach	EB	WB	SB
HCM Control Delay, s/v	0.2	0	25.36
HCM LOS			D

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	4	-	-	-	193
HCM Lane V/C Ratio	0.006	-	-	-	0.082
HCM Control Delay (s/veh)	15.5	0.2	-	-	25.4
HCM Lane LOS	C	A	-	-	D
HCM 95th %tile Q(veh)	0	-	-	-	0.3

Intersection												
Int Delay, s/veh	0.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↙	↕		↙	↕			↕	↗		↕	
Traffic Vol, veh/h	7	1606	7	19	1174	6	5	0	54	4	0	8
Future Vol, veh/h	7	1606	7	19	1174	6	5	0	54	4	0	8
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None									
Storage Length	110	-	-	145	-	-	-	-	0	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	14	7	0	10	7	0	0	0	4	0	0	12
Mvmt Flow	7	1709	7	20	1249	6	5	0	57	4	0	9

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1255	0	0	1716	0	0	2392	3023	858	2162	3023	628
Stage 1	-	-	-	-	-	-	1727	1727	-	1293	1293	-
Stage 2	-	-	-	-	-	-	665	1296	-	869	1731	-
Critical Hdwy	4.38	-	-	4.3	-	-	7.5	6.5	6.98	7.5	6.5	7.14
Critical Hdwy Stg 1	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Follow-up Hdwy	2.34	-	-	2.3	-	-	3.5	4	3.34	3.5	4	3.42
Pot Cap-1 Maneuver	489	-	-	332	-	-	18	13	296	27	13	402
Stage 1	-	-	-	-	-	-	94	145	-	175	235	-
Stage 2	-	-	-	-	-	-	420	235	-	317	144	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	489	-	-	332	-	-	16	12	296	20	12	402
Mov Cap-2 Maneuver	-	-	-	-	-	-	72	83	-	97	77	-
Stage 1	-	-	-	-	-	-	92	142	-	164	221	-
Stage 2	-	-	-	-	-	-	386	220	-	252	142	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s/v	0.05			0.26			23.33			24.64		
HCM LOS							C			C		

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	72	296	489	-	-	332	-	-	196
HCM Lane V/C Ratio	0.073	0.194	0.015	-	-	0.061	-	-	0.065
HCM Control Delay (s/veh)	58.6	20.1	12.5	-	-	16.6	-	-	24.6
HCM Lane LOS	F	C	B	-	-	C	-	-	C
HCM 95th %tile Q(veh)	0.2	0.7	0	-	-	0.2	-	-	0.2

Capacity Analysis Summary Sheets
Weekday Evening Peak Hour – No-Build Conditions

Lanes, Volumes, Timings

11: Butterfield Square Access Drive & Buckley/Peterson Road

03/26/2025



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↗	↘	↑↑	↘	↗
Traffic Volume (vph)	1263	2	104	1735	50	53
Future Volume (vph)	1263	2	104	1735	50	53
Ideal Flow (vphpl)	2000	1900	1900	2000	1900	1900
Lane Width (ft)	12	12	12	12	12	12
Grade (%)	0%			0%	0%	
Storage Length (ft)		200	270		0	0
Storage Lanes		1	1		1	1
Taper Length (ft)			240		25	
Lane Util. Factor	0.95	1.00	1.00	0.95	1.00	1.00
Ped Bike Factor						
Frt		0.850				0.850
Flt Protected			0.950		0.950	
Satd. Flow (prot)	3725	1615	1805	3689	1805	1615
Flt Permitted			0.950		0.950	
Satd. Flow (perm)	3725	1615	1805	3689	1805	1615
Right Turn on Red		Yes				Yes
Satd. Flow (RTOR)		2				57
Link Speed (mph)	30			30	30	
Link Distance (ft)	990			919	330	
Travel Time (s)	22.5			20.9	7.5	
Confl. Peds. (#/hr)						
Confl. Bikes (#/hr)						
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93
Growth Factor	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	2%	0%	0%	3%	0%	0%
Bus Blockages (#/hr)	0	0	0	0	0	0
Parking (#/hr)						
Mid-Block Traffic (%)	0%			0%	0%	
Shared Lane Traffic (%)						
Lane Group Flow (vph)	1358	2	112	1866	54	57
Turn Type	NA	pm+ov	Prot	NA	Prot	Perm
Protected Phases	2	8	1	6	8	
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	8
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	24.0	24.0	9.5	24.0	24.0	24.0
Total Split (s)	92.0	28.0	20.0	112.0	28.0	28.0
Total Split (%)	65.7%	20.0%	14.3%	80.0%	20.0%	20.0%
Yellow Time (s)	4.0	4.0	3.5	4.0	4.0	4.0
All-Red Time (s)	2.0	2.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.0	6.0	4.5	6.0	6.0	6.0
Lead/Lag	Lag		Lead			
Lead-Lag Optimize?	Yes		Yes			
Recall Mode	C-Min	None	None	C-Min	None	None
Act Effct Green (s)	102.4	117.9	13.9	122.0	9.6	9.6
Actuated g/C Ratio	0.73	0.84	0.10	0.87	0.07	0.07

Lanes, Volumes, Timings

11: Butterfield Square Access Drive & Buckley/Peterson Road

03/26/2025



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
v/c Ratio	0.50	0.00	0.63	0.58	0.44	0.35
Control Delay (s/veh)	10.0	2.0	75.0	4.1	72.8	20.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	10.0	2.0	75.0	4.1	72.8	20.2
LOS	A	A	E	A	E	C
Approach Delay (s/veh)	10.0			8.1	45.8	
Approach LOS	A			A	D	
Queue Length 50th (ft)	266	0	100	215	48	0
Queue Length 95th (ft)	392	2	160	308	93	44
Internal Link Dist (ft)	910			839	250	
Turn Bay Length (ft)		200	270			
Base Capacity (vph)	2724	1484	210	3215	283	301
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.50	0.00	0.53	0.58	0.19	0.19

Intersection Summary

Area Type:	Other
Cycle Length:	140
Actuated Cycle Length:	140
Offset:	0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.63
Intersection Signal Delay (s/veh):	10.1
Intersection LOS:	B
Intersection Capacity Utilization:	59.7%
ICU Level of Service:	B
Analysis Period (min):	15

Splits and Phases: 11: Butterfield Square Access Drive & Buckley/Peterson Road



Intersection												
Int Delay, s/veh	0.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↕		↕	↕↕			↕↕			↕↕	
Traffic Vol, veh/h	0	1314	6	2	1845	4	4	0	2	4	0	0
Future Vol, veh/h	0	1314	6	2	1845	4	4	0	2	4	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None									
Storage Length	-	-	-	105	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	93	93	93	93	93	93	93	93	93	93	93	93
Heavy Vehicles, %	0	2	0	0	2	0	0	0	0	0	0	0
Mvmt Flow	0	1413	6	2	1984	4	4	0	2	4	0	0

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1988	0	0	1419	0	0	2412	3409	710	2697	3410	994
Stage 1	-	-	-	-	-	-	1416	1416	-	1990	1990	-
Stage 2	-	-	-	-	-	-	996	1992	-	706	1419	-
Critical Hdwy	4.1	-	-	4.1	-	-	7.5	6.5	6.9	7.5	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.3	3.5	4	3.3
Pot Cap-1 Maneuver	294	-	-	486	-	-	18	7	381	11	7	247
Stage 1	-	-	-	-	-	-	147	205	-	64	107	-
Stage 2	-	-	-	-	-	-	266	107	-	397	205	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	294	-	-	486	-	-	17	7	381	10	7	247
Mov Cap-2 Maneuver	-	-	-	-	-	-	93	67	-	52	67	-
Stage 1	-	-	-	-	-	-	147	205	-	64	106	-
Stage 2	-	-	-	-	-	-	265	106	-	395	205	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s/v	0			0.01			35.39			80.08		
HCM LOS							E			F		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	125	294	-	-	486	-	-	52
HCM Lane V/C Ratio	0.052	-	-	-	0.004	-	-	0.082
HCM Control Delay (s/veh)	35.4	0	-	-	12.4	-	-	80.1
HCM Lane LOS	E	A	-	-	B	-	-	F
HCM 95th %tile Q(veh)	0.2	0	-	-	0	-	-	0.3

Intersection						
Int Delay, s/veh	0.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↔↔	↔↔		↔	
Traffic Vol, veh/h	2	1318	1843	7	4	8
Future Vol, veh/h	2	1318	1843	7	4	8
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	1	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	93	93	93	93	93	93
Heavy Vehicles, %	0	2	3	0	0	0
Mvmt Flow	2	1417	1982	8	4	9

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	1989	0	0 2698 995
Stage 1	-	-	- 1985 -
Stage 2	-	-	- 713 -
Critical Hdwy	4.1	-	- 6.8 6.9
Critical Hdwy Stg 1	-	-	- 5.8 -
Critical Hdwy Stg 2	-	-	- 5.8 -
Follow-up Hdwy	2.2	-	- 3.5 3.3
Pot Cap-1 Maneuver	293	-	- 18 247
Stage 1	-	-	- 95 -
Stage 2	-	-	- 452 -
Platoon blocked, %		-	- -
Mov Cap-1 Maneuver	293	-	- 18 247
Mov Cap-2 Maneuver	-	-	- 75 -
Stage 1	-	-	- 94 -
Stage 2	-	-	- 452 -

Approach	EB	WB	SB
HCM Control Delay, s/v	0.24	0	33.27
HCM LOS			D

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	5	-	-	-	140
HCM Lane V/C Ratio	0.007	-	-	-	0.092
HCM Control Delay (s/veh)	17.4	0.2	-	-	33.3
HCM Lane LOS	C	A	-	-	D
HCM 95th %tile Q(veh)	0	-	-	-	0.3

Intersection												
Int Delay, s/veh	0.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↙	↕		↙	↕			↕	↗		↕	
Traffic Vol, veh/h	6	1289	21	27	1821	1	3	0	31	0	0	15
Future Vol, veh/h	6	1289	21	27	1821	1	3	0	31	0	0	15
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None									
Storage Length	110	-	-	145	-	-	-	-	0	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	93	93	93	93	93	93	93	93	93	93	93	93
Heavy Vehicles, %	0	2	0	0	3	0	0	0	3	0	0	0
Mvmt Flow	6	1386	23	29	1958	1	3	0	33	0	0	16

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1959	0	0	1409	0	0	2447	3427	704	2723	3438	980
Stage 1	-	-	-	-	-	-	1410	1410	-	2017	2017	-
Stage 2	-	-	-	-	-	-	1037	2017	-	706	1422	-
Critical Hdwy	4.1	-	-	4.1	-	-	7.5	6.5	6.96	7.5	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.33	3.5	4	3.3
Pot Cap-1 Maneuver	301	-	-	491	-	-	16	7	377	10	7	253
Stage 1	-	-	-	-	-	-	148	207	-	62	104	-
Stage 2	-	-	-	-	-	-	251	104	-	397	204	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	301	-	-	491	-	-	14	7	377	8	7	253
Mov Cap-2 Maneuver	-	-	-	-	-	-	85	60	-	47	59	-
Stage 1	-	-	-	-	-	-	145	202	-	58	97	-
Stage 2	-	-	-	-	-	-	221	97	-	355	200	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s/v	0.08			0.19			18.42			20.2		
HCM LOS							C			C		

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	85	377	301	-	-	491	-	-	253
HCM Lane V/C Ratio	0.038	0.088	0.021	-	-	0.059	-	-	0.064
HCM Control Delay (s/veh)	48.9	15.5	17.2	-	-	12.8	-	-	20.2
HCM Lane LOS	E	C	C	-	-	B	-	-	C
HCM 95th %tile Q(veh)	0.1	0.3	0.1	-	-	0.2	-	-	0.2

Capacity Analysis Summary Sheets
Weekday Morning Peak Hour – Projected Conditions

Lanes, Volumes, Timings

11: Butterfield Square Access Drive & Buckley/Peterson Road

03/26/2025



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↓	↑↑	↓	↑
Traffic Volume (vph)	1548	2	48	1154	16	73
Future Volume (vph)	1548	2	48	1154	16	73
Ideal Flow (vphpl)	2000	1900	1900	2000	1900	1900
Lane Width (ft)	12	12	12	12	12	12
Grade (%)	0%			0%	0%	
Storage Length (ft)		200	270		0	0
Storage Lanes		1	1		1	1
Taper Length (ft)			240		25	
Lane Util. Factor	0.95	1.00	1.00	0.95	1.00	1.00
Ped Bike Factor						
Frt		0.850				0.850
Flt Protected			0.950		0.950	
Satd. Flow (prot)	3551	1077	1736	3551	1517	1568
Flt Permitted			0.950		0.950	
Satd. Flow (perm)	3551	1077	1736	3551	1517	1568
Right Turn on Red		Yes				Yes
Satd. Flow (RTOR)		2				78
Link Speed (mph)	30			30	30	
Link Distance (ft)	990			919	330	
Travel Time (s)	22.5			20.9	7.5	
Confl. Peds. (#/hr)						
Confl. Bikes (#/hr)						
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94
Growth Factor	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	7%	50%	4%	7%	19%	3%
Bus Blockages (#/hr)	0	0	0	0	0	0
Parking (#/hr)						
Mid-Block Traffic (%)	0%			0%	0%	
Shared Lane Traffic (%)						
Lane Group Flow (vph)	1647	2	51	1228	17	78
Turn Type	NA	pm+ov	Prot	NA	Prot	Prot
Protected Phases	2	8	1	6	8	8
Permitted Phases		2				
Detector Phase	2	8	1	6	8	8
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	24.0	24.0	9.5	24.0	24.0	24.0
Total Split (s)	101.0	24.0	15.0	116.0	24.0	24.0
Total Split (%)	72.1%	17.1%	10.7%	82.9%	17.1%	17.1%
Yellow Time (s)	4.0	4.0	3.5	4.0	4.0	4.0
All-Red Time (s)	2.0	2.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.0	6.0	4.5	6.0	6.0	6.0
Lead/Lag	Lag		Lead			
Lead-Lag Optimize?	Yes		Yes			
Recall Mode	C-Max	Max	None	C-Max	Max	Max
Act Effct Green (s)	98.7	123.9	8.9	110.0	18.0	18.0
Actuated g/C Ratio	0.71	0.89	0.06	0.79	0.13	0.13

Lanes, Volumes, Timings

11: Butterfield Square Access Drive & Buckley/Peterson Road

03/26/2025



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
v/c Ratio	0.66	0.00	0.47	0.44	0.09	0.29
Control Delay (s/veh)	13.6	1.0	76.6	5.5	55.1	14.1
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	13.6	1.0	76.6	5.5	55.1	14.1
LOS	B	A	E	A	E	B
Approach Delay (s/veh)	13.6			8.3	21.4	
Approach LOS	B			A	C	
Queue Length 50th (ft)	427	0	45	164	14	0
Queue Length 95th (ft)	513	1	91	195	38	49
Internal Link Dist (ft)	910			839	250	
Turn Bay Length (ft)		200	270			
Base Capacity (vph)	2503	953	130	2790	195	269
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.66	0.00	0.39	0.44	0.09	0.29

Intersection Summary

Area Type:	Other
Cycle Length:	140
Actuated Cycle Length:	140
Offset:	95 (68%), Referenced to phase 2:EBT and 6:WBT, Start of Green
Natural Cycle:	80
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.66
Intersection Signal Delay (s/veh):	11.6
Intersection LOS:	B
Intersection Capacity Utilization:	55.2%
ICU Level of Service:	B
Analysis Period (min):	15

Splits and Phases: 11: Butterfield Square Access Drive & Buckley/Peterson Road



Intersection												
Int Delay, s/veh	0.3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↖↗		↖	↖↗	↖		↔			↔	
Traffic Vol, veh/h	4	1678	4	1	1198	6	2	0	7	12	0	8
Future Vol, veh/h	4	1678	4	1	1198	6	2	0	7	12	0	8
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None									
Storage Length	215	-	-	150	-	150	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	96	96	96	96	96	96	96	96	96	96	96	96
Heavy Vehicles, %	0	6	75	0	7	0	0	0	14	0	0	0
Mvmt Flow	4	1748	4	1	1248	6	2	0	7	13	0	8

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1254	0	0	1752	0	0	2384	3015	876	2132	3010	624
Stage 1	-	-	-	-	-	-	1758	1758	-	1250	1250	-
Stage 2	-	-	-	-	-	-	626	1256	-	882	1760	-
Critical Hdwy	4.1	-	-	4.1	-	-	7.5	6.5	7.18	7.5	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.44	3.5	4	3.3
Pot Cap-1 Maneuver	562	-	-	362	-	-	18	14	269	29	14	433
Stage 1	-	-	-	-	-	-	90	140	-	186	247	-
Stage 2	-	-	-	-	-	-	443	245	-	311	139	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	562	-	-	362	-	-	18	13	269	28	13	433
Mov Cap-2 Maneuver	-	-	-	-	-	-	72	86	-	116	86	-
Stage 1	-	-	-	-	-	-	89	139	-	185	246	-
Stage 2	-	-	-	-	-	-	434	244	-	301	138	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s/v	0.03			0.01			27.83			30.03		
HCM LOS							D			D		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	167	562	-	-	362	-	-	165
HCM Lane V/C Ratio	0.056	0.007	-	-	0.003	-	-	0.127
HCM Control Delay (s/veh)	27.8	11.5	-	-	15	-	-	30
HCM Lane LOS	D	B	-	-	B	-	-	D
HCM 95th %tile Q(veh)	0.2	0	-	-	0	-	-	0.4

HCM 7th TWSC
 7: Buckley/Peterson Road & Forest Creek Lane

03/26/2025

Intersection						
Int Delay, s/veh	0.3					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↔↔	↔↔		↔	
Traffic Vol, veh/h	2	1695	1199	4	9	6
Future Vol, veh/h	2	1695	1199	4	9	6
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	1	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	95	95	95	95	95	95
Heavy Vehicles, %	50	5	6	0	0	33
Mvmt Flow	2	1784	1262	4	9	6

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	1266	0	-	0	2161 633
Stage 1	-	-	-	-	1264 -
Stage 2	-	-	-	-	896 -
Critical Hdwy	5.1	-	-	-	6.8 7.56
Critical Hdwy Stg 1	-	-	-	-	5.8 -
Critical Hdwy Stg 2	-	-	-	-	5.8 -
Follow-up Hdwy	2.7	-	-	-	3.5 3.63
Pot Cap-1 Maneuver	343	-	-	-	42 355
Stage 1	-	-	-	-	233 -
Stage 2	-	-	-	-	364 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	343	-	-	-	41 355
Mov Cap-2 Maneuver	-	-	-	-	146 -
Stage 1	-	-	-	-	230 -
Stage 2	-	-	-	-	364 -

Approach	EB	WB	SB
HCM Control Delay, s/v	0.21	0	25.5
HCM LOS			D

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	4	-	-	-	191
HCM Lane V/C Ratio	0.006	-	-	-	0.083
HCM Control Delay (s/veh)	15.5	0.2	-	-	25.5
HCM Lane LOS	C	A	-	-	D
HCM 95th %tile Q(veh)	0	-	-	-	0.3

Intersection												
Int Delay, s/veh	1.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘	↕↗		↘	↕↗	↗		↖	↗		↖	↗
Traffic Vol, veh/h	5	1609	7	19	1182	7	5	0	54	23	0	15
Future Vol, veh/h	5	1609	7	19	1182	7	5	0	54	23	0	15
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None									
Storage Length	215	-	-	215	-	215	-	-	0	-	-	0
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	14	7	0	10	7	0	0	0	4	0	0	12
Mvmt Flow	5	1712	7	20	1257	7	5	0	57	24	0	16

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1265	0	0	1719	0	0	2395	3031	860	2164	3028	629
Stage 1	-	-	-	-	-	-	1726	1726	-	1298	1298	-
Stage 2	-	-	-	-	-	-	669	1305	-	866	1730	-
Critical Hdwy	4.38	-	-	4.3	-	-	7.5	6.5	6.98	7.5	6.5	7.14
Critical Hdwy Stg 1	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Follow-up Hdwy	2.34	-	-	2.3	-	-	3.5	4	3.34	3.5	4	3.42
Pot Cap-1 Maneuver	484	-	-	331	-	-	18	13	295	27	13	402
Stage 1	-	-	-	-	-	-	94	145	-	174	234	-
Stage 2	-	-	-	-	-	-	418	232	-	318	144	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	484	-	-	331	-	-	16	12	295	~ 20	12	402
Mov Cap-2 Maneuver	-	-	-	-	-	-	72	83	-	97	77	-
Stage 1	-	-	-	-	-	-	93	143	-	163	220	-
Stage 2	-	-	-	-	-	-	377	218	-	254	143	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s/v	0.04			0.26			23.37			38.62		
HCM LOS							C			E		

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1	SBLn2
Capacity (veh/h)	72	295	484	-	-	331	-	-	97	402
HCM Lane V/C Ratio	0.073	0.194	0.011	-	-	0.061	-	-	0.253	0.04
HCM Control Delay (s/veh)	58.6	20.1	12.5	-	-	16.6	-	-	54.5	14.3
HCM Lane LOS	F	C	B	-	-	C	-	-	F	B
HCM 95th %tile Q(veh)	0.2	0.7	0	-	-	0.2	-	-	0.9	0.1

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Capacity Analysis Summary Sheets
Weekday Evening Peak Hour – Projected Conditions

Lanes, Volumes, Timings

11: Butterfield Square Access Drive & Buckley/Peterson Road

03/26/2025



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↗	↘	↑↑	↘	↗
Traffic Volume (vph)	1281	2	104	1736	50	53
Future Volume (vph)	1281	2	104	1736	50	53
Ideal Flow (vphpl)	2000	1900	1900	2000	1900	1900
Lane Width (ft)	12	12	12	12	12	12
Grade (%)	0%			0%	0%	
Storage Length (ft)		200	270		0	0
Storage Lanes		1	1		1	1
Taper Length (ft)			240		25	
Lane Util. Factor	0.95	1.00	1.00	0.95	1.00	1.00
Ped Bike Factor						
Frt		0.850				0.850
Flt Protected			0.950		0.950	
Satd. Flow (prot)	3725	1615	1805	3689	1805	1615
Flt Permitted			0.950		0.950	
Satd. Flow (perm)	3725	1615	1805	3689	1805	1615
Right Turn on Red		Yes				Yes
Satd. Flow (RTOR)		2				57
Link Speed (mph)	30			30	30	
Link Distance (ft)	990			919	330	
Travel Time (s)	22.5			20.9	7.5	
Confl. Peds. (#/hr)						
Confl. Bikes (#/hr)						
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93
Growth Factor	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	2%	0%	0%	3%	0%	0%
Bus Blockages (#/hr)	0	0	0	0	0	0
Parking (#/hr)						
Mid-Block Traffic (%)	0%			0%	0%	
Shared Lane Traffic (%)						
Lane Group Flow (vph)	1377	2	112	1867	54	57
Turn Type	NA	pm+ov	Prot	NA	Prot	Perm
Protected Phases	2	8	1	6	8	
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	8
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	24.0	24.0	9.5	24.0	24.0	24.0
Total Split (s)	92.0	28.0	20.0	112.0	28.0	28.0
Total Split (%)	65.7%	20.0%	14.3%	80.0%	20.0%	20.0%
Yellow Time (s)	4.0	4.0	3.5	4.0	4.0	4.0
All-Red Time (s)	2.0	2.0	1.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.0	6.0	4.5	6.0	6.0	6.0
Lead/Lag	Lag		Lead			
Lead-Lag Optimize?	Yes		Yes			
Recall Mode	C-Min	None	None	C-Min	None	None
Act Effct Green (s)	102.4	117.9	13.9	122.0	9.6	9.6
Actuated g/C Ratio	0.73	0.84	0.10	0.87	0.07	0.07

Lanes, Volumes, Timings

11: Butterfield Square Access Drive & Buckley/Peterson Road

03/26/2025



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
v/c Ratio	0.51	0.00	0.63	0.58	0.44	0.35
Control Delay (s/veh)	10.1	2.0	75.0	4.1	72.8	20.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	10.1	2.0	75.0	4.1	72.8	20.2
LOS	B	A	E	A	E	C
Approach Delay (s/veh)	10.1			8.1	45.8	
Approach LOS	B			A	D	
Queue Length 50th (ft)	272	0	100	215	48	0
Queue Length 95th (ft)	401	2	160	308	93	44
Internal Link Dist (ft)	910			839	250	
Turn Bay Length (ft)		200	270			
Base Capacity (vph)	2724	1484	210	3215	283	301
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.51	0.00	0.53	0.58	0.19	0.19

Intersection Summary

Area Type:	Other
Cycle Length:	140
Actuated Cycle Length:	140
Offset:	0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.63
Intersection Signal Delay (s/veh):	10.1
Intersection LOS:	B
Intersection Capacity Utilization:	59.8%
ICU Level of Service:	B
Analysis Period (min):	15

Splits and Phases: 11: Butterfield Square Access Drive & Buckley/Peterson Road



Intersection												
Int Delay, s/veh	0.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↕↗		↖	↕↗	↖		↕↗			↕↗	
Traffic Vol, veh/h	8	1328	6	2	1864	17	4	0	2	8	0	6
Future Vol, veh/h	8	1328	6	2	1864	17	4	0	2	8	0	6
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None									
Storage Length	215	-	-	105	-	125	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	93	93	93	93	93	93	93	93	93	93	93	93
Heavy Vehicles, %	0	2	0	0	2	0	0	0	0	0	0	0
Mvmt Flow	9	1428	6	2	2004	18	4	0	2	9	0	6

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	2023	0	0	1434	0	0	2455	3475	717	2740	3460	1002
Stage 1	-	-	-	-	-	-	1448	1448	-	2009	2009	-
Stage 2	-	-	-	-	-	-	1006	2027	-	731	1452	-
Critical Hdwy	4.1	-	-	4.1	-	-	7.5	6.5	6.9	7.5	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.3	3.5	4	3.3
Pot Cap-1 Maneuver	285	-	-	480	-	-	16	7	376	10	7	244
Stage 1	-	-	-	-	-	-	140	198	-	62	105	-
Stage 2	-	-	-	-	-	-	262	102	-	384	197	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	285	-	-	480	-	-	15	6	376	9	7	244
Mov Cap-2 Maneuver	-	-	-	-	-	-	86	60	-	50	64	-
Stage 1	-	-	-	-	-	-	136	192	-	62	104	-
Stage 2	-	-	-	-	-	-	254	102	-	370	191	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s/v	0.11			0.01			37.84			63.3		
HCM LOS							E			F		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	116	285	-	-	480	-	-	76
HCM Lane V/C Ratio	0.056	0.03	-	-	0.004	-	-	0.197
HCM Control Delay (s/veh)	37.8	18	-	-	12.5	-	-	63.3
HCM Lane LOS	E	C	-	-	B	-	-	F
HCM 95th %tile Q(veh)	0.2	0.1	-	-	0	-	-	0.7

Intersection						
Int Delay, s/veh	0.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↔↔	↔↔		↔	
Traffic Vol, veh/h	2	1336	1875	7	4	8
Future Vol, veh/h	2	1336	1875	7	4	8
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	1	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	93	93	93	93	93	93
Heavy Vehicles, %	0	2	3	0	0	0
Mvmt Flow	2	1437	2016	8	4	9

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	2024	0	0 2742 1012
Stage 1	-	-	- 2020 -
Stage 2	-	-	- 723 -
Critical Hdwy	4.1	-	- 6.8 6.9
Critical Hdwy Stg 1	-	-	- 5.8 -
Critical Hdwy Stg 2	-	-	- 5.8 -
Follow-up Hdwy	2.2	-	- 3.5 3.3
Pot Cap-1 Maneuver	285	-	- 17 241
Stage 1	-	-	- 91 -
Stage 2	-	-	- 447 -
Platoon blocked, %		-	- -
Mov Cap-1 Maneuver	285	-	- 16 241
Mov Cap-2 Maneuver	-	-	- 72 -
Stage 1	-	-	- 90 -
Stage 2	-	-	- 447 -

Approach	EB	WB	SB
HCM Control Delay, s/v	0.25	0	34.41
HCM LOS			D

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	5	-	-	-	135
HCM Lane V/C Ratio	0.008	-	-	-	0.095
HCM Control Delay (s/veh)	17.7	0.2	-	-	34.4
HCM Lane LOS	C	A	-	-	D
HCM 95th %tile Q(veh)	0	-	-	-	0.3

Intersection												
Int Delay, s/veh	1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘	↗		↘	↗	↗		↗	↗		↗	↗
Traffic Vol, veh/h	16	1297	21	27	1827	20	3	0	31	14	0	10
Future Vol, veh/h	16	1297	21	27	1827	20	3	0	31	14	0	10
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None									
Storage Length	215	-	-	145	-	215	-	-	0	-	-	0
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	93	93	93	93	93	93	93	93	93	93	93	93
Heavy Vehicles, %	0	2	0	0	3	0	0	0	3	0	0	0
Mvmt Flow	17	1395	23	29	1965	22	3	0	33	15	0	11

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1986	0	0	1417	0	0	2481	3484	709	2754	3474	982
Stage 1	-	-	-	-	-	-	1440	1440	-	2023	2023	-
Stage 2	-	-	-	-	-	-	1040	2044	-	732	1452	-
Critical Hdwy	4.1	-	-	4.1	-	-	7.5	6.5	6.96	7.5	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.5	5.5	-	6.5	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.33	3.5	4	3.3
Pot Cap-1 Maneuver	294	-	-	487	-	-	16	7	375	~10	7	252
Stage 1	-	-	-	-	-	-	142	200	-	61	103	-
Stage 2	-	-	-	-	-	-	250	100	-	384	197	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	294	-	-	487	-	-	13	6	375	~8	6	252
Mov Cap-2 Maneuver	-	-	-	-	-	-	80	52	-	46	57	-
Stage 1	-	-	-	-	-	-	134	188	-	57	97	-
Stage 2	-	-	-	-	-	-	225	94	-	329	186	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s/v	0.22			0.19			18.76			77.26		
HCM LOS							C			F		

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1	SBLn2
Capacity (veh/h)	80	375	294	-	-	487	-	-	46	252
HCM Lane V/C Ratio	0.04	0.089	0.058	-	-	0.06	-	-	0.329	0.043
HCM Control Delay (s/veh)	51.9	15.5	18	-	-	12.9	-	-	118.2	19.9
HCM Lane LOS	F	C	C	-	-	B	-	-	F	C
HCM 95th %tile Q(veh)	0.1	0.3	0.2	-	-	0.2	-	-	1.1	0.1

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon



DRAINAGE MEMORANDUM

To: Village of Libertyville

From: Ryan Martin, P.E.
Ian Spence, P.E.
Kimley-Horn and Associates, Inc.

Date: May 19, 2025

Re: ***Proposed 42-Acre Townhome Community
610 Peterson Road – North of Elderberry Drive & Peterson Road
Libertyville, Illinois***

Introduction

Kimley-Horn and Associates, Inc., serves as the engineering consultant for Pulte Home Company, LLC, who is proposing to develop approximately 42 acres into a residential community. The sitework includes demolition of existing buildings/pavement, grading, stormwater management facilities, water, sanitary sewer, paving, landscaping, and construction of single-family homes and duplexes.

This memo assesses the post-development runoff in accordance with the stormwater regulations of Lake County and the Village of Libertyville. This memo summarizes the preliminary stormwater calculations for the site and is not intended to be a comprehensive design of all final engineering requirements. The objective is to illustrate that the proposed site layout can adequately handle the necessary stormwater detention volume and adhere to the prescribed release rates mandated by regulations. Analysis of the proposed stormwater detention basins were conducted using HydroCAD Version 10.20-5c.

1. Existing Conditions

The existing site is currently about 10.5 acres of developed land that is used for extended care facilities. The remaining site is primarily undeveloped wooded area and has remained largely undisturbed since before 1990. It is bound by Peterson Road to the south, Victoria Park housing complex and COMED right-of-way to the West, unincorporated Lake County land to the north, and Forest Creek housing complex and a business park to the east.

The site generally drains from south to north. Runoff captured from the developed area is captured via existing storm infrastructure and is conveyed to an existing constructed basin. Runoff from the IDOT right-of-way (Peterson Road) is directed east via a drainage ditch. A drainage ditch will be maintained in the proposed conditions.

A wetland map was initially pulled from U.S. Fish and Wildlife Service National Wetlands Inventory and was then supplemented with a delineated wetland map prepared by DK Environmental Services, Inc. (see **Exhibit 1**). There are multiple wetlands identified on site. This development will cause impacts to the existing wetlands and will likely require permitting from Lake County Stormwater

Commission. Additional information regarding the jurisdiction and allowable wetland impacts will be provided by the client’s wetland consultant in a subsequent submittal.

A majority of the site is classified by FEMA as Zone X, an area of minimal flood hazard. However, the northeast and northwest corners are classified by FEMA as Zone AE, special flood hazard areas (see **Exhibit 1**).

A soils survey was obtained from the Natural Resources Conservation Service (NRCS), which shows that the majority of the site is underlain with Varna silt loam (group C) and Pella silty clay loam (group B/D). See **Exhibit 1** for a detailed breakdown of the soil groups. The existing conditions is modelled with consideration to the existing soil groups.

1.1 Existing Depressional Storage (Constructed Basin)

As noted, review of the existing site topography determined that there is an existing, constructed depressional storage area that is drained to the northeast corner of the project site via a 15-inch diameter storm sewer. While we feel that this would not fall under the classification of a depressional storage area, we feel that this is a constructed storage basin that will be disturbed during the future site development. As part of our existing site analysis, it was determined that the storage basin provides 1.88 ac-ft of stormwater storage. This lost stormwater storage will be maintained and provided within a proposed stormwater management basin. See **Exhibit 2** for details.

1.2 Pre-1992 Impervious Surface

A portion of the site is currently developed with a building and associated parking lots. The previously disturbed area equates to approximately 5.66 acres of impervious surface. Per section 300.06 of the Lake County Watershed Development Ordinance, this area is exempt from current stormwater detention requirements.

Table 1 summarizes the Pre-1992 impervious surface associated with each proposed condition sub-watershed delineation. The existing impervious area is calculated as a combination of existing building, sheds, gravel, concrete, brick pavement, and asphalt pavement. See **Exhibit 2** for details.

Table 1: Pre-1992 Impervious Surface Summary			
	Sub catchment A	Sub catchment B	Sub catchment C
Existing Impervious Area	0.44 Ac	3.29 Ac	1.93 Ac
Actual Area	3.30 Ac	20.79 Ac	11.24 Ac
Theoretical Area*	2.86 Ac	17.50 Ac	9.31 Ac

*Refers to the area required for detention volume sizing less the existing impervious area on-site. See Page 5 for details.

1.3 Federal Emergency Management Agency Regulatory Floodplain

The Federal Emergency Management Agency (FEMA) Regulatory Flood Insurance Rate Map (FIRM) for Bull Creek was reviewed with respect to the updated site-specific topography. The flood profile was obtained from the FEMA Flood Insurance Study (FIS); specifically, FIS Profile 036P of Bull Creek Tributary. The portion of Bull Creek, immediately north of the development site has FEMA Zone AE regulatory floodplain that extends from Buckley Road crossing east towards North Milwaukee Avenue. The effective floodplain elevations relative to this site are included in Table 2 below. See **Exhibit 2** for details.

Table 2: FEMA FIS Regulatory Floodplain Elevations		
	10-Yr	100-Yr
A	686.50	687.80
B	697.90	698.80
C	706.00	707.00
Basin 1 Outfall	689.00	690.00
Basin 2 Outfall	683.80	684.80

The floodplain elevations listed on Table 2 were delineated using a combination of the site-specific topography and 2017 Lake County aerial topography. When comparing the floodplain limits and elevation to both topographic sources, it was determined that the FEMA floodplain shown on the effective FIRM By Map is understated when compared to the delineation using the site-specific topography. The floodplain limits have been updated and are shown throughout the engineering plans and within the existing and proposed conditions exhibits. See **Exhibit 2** and **Exhibit 3** for details.

Our proposed site plan shows no disturbance to the existing regulatory floodplain associated with the construction of the stormwater management basin. As a preventive measure, an extends of grading delineation has been proposed 5' off of the existing floodplain delineation. Compensatory storage will not be required within the stormwater management basin.

2. Proposed Conditions

The proposed development will consist of three drainage areas and three stormwater detention basins that will be required to capture runoff from the site. All drainage will be routed to the basins via storm sewer or overland flow. After being collected in the detention basins, drainage will be released at or below the maximum allowable release rate to the existing wetlands and floodplains and ultimately continue flow northeast of the property.

The proposed stormwater management system will be designed to meet the requirements of the WDO. Stormwater management, compensatory storage, runoff volume reduction and water quality standards will all be met as part of the final design. As part of the preliminary engineering design, the stormwater management basins were designed per the following:

- Maximum percent impervious for duplex & single-family lots to be 55% & 50% respectively
- Time of Concentration: 15 minutes
- 2-year and 100-year rainfall events per Illinois State Water Survey Bulletin 75
- Allowable release rate of 0.04 cfs/acre for the 2-year, 24-hour, rainfall event
- Allowable release rate of 0.15 cfs/acre for the 100-year, 24-hour, rainfall event
- Natively planted grass bottoms to provide water quality benefit and runoff volume reduction

The runoff curve numbers for the site have been calculated utilizing TR-55 methodology as well as the Village of Libertyville standards for lot coverage. Refer to Table 3 below for summary of the areas used to calculate composite curve numbers.

Coverage Type	Pervious ROW (SF)	Impervious ROW (SF)	Total (for 10 LF)	
Area	230	370	600	
CN Value	80	98	91	per LF
Roadway ROW Area:	6.17	Ac		
Coverage Type	Single Family (64)	Duplex (70)		
Area	7.75	9.94	17.69	Ac
Calculated % Impervious	50.00%	55.00%		
CN Value	89	89.9	89.5	per Lot
Lot Coverages Area:	17.69	Ac		
Duplex Amentity	Pervious (SF)	Impervious (SF)		
Area	57683	3715		1.41 Ac
CN Value	80	98		81.09 CN
Park Amentity	Pervious (SF)	Impervious (SF)	Gravel (SF)	Playground (SF)
Area	28959	7322	2160	11735
CN Value	80	98	91	77
				1.15 Ac
				82.40 CN
Stormwater Management	3.72	Ac	80	CN
Outlots and Landscape Buffers	5.19	Ac	80	CN
Amentity Areas	2.56	Ac	81.68	CN
Composite Area	35.33	Ac		
Composite CN Value	87	CN		

As noted in the calculation above, a curve number of 80 has been calculated for the stormwater management areas, as well as for out lots and landscape buffers on site. A curve number has been calculated for the parks and amenity areas separately. The composite CN value for the site is 87.

As noted, the development parcel had approximately 5.66 acres of existing impervious area. This area will be excluded from any required stormwater detention calculations per the WDO. Refer to Table 4 for summary of the tributary areas used in determining the required stormwater storage volume. See **Exhibit 3** for details.

Table 4: Stormwater Management Basins Hydrologic Parameters

Sub Basin ID	Total Tributary Area	Pre 1992 Impervious Area	Area used to determine stormwater detention	Runoff Curve Number	Time of Concentration
	Acres	Acres	Acres	(CN)	(Hrs)
A	3.30	0.44	2.86	87	.25
B	20.79	3.29	17.50	87	.25
C	11.24	1.93	9.31	87	.25

Stormwater detention calculations were performed using HydroCAD Version 10.20-5c. The required volume was determined using both the 2-year and 100-year allowable release rates and Lake County Bulletin 75 effective rainfall data. The current configuration routes stormwater management Basin 3 to Basin 1 at the northern limits of the development. Basin 1 is sized to meet the allowable release rate of the developable area of both Sub catchments A and B. Stormwater management Basin 2 is sized for the tributary area associated with the subbasin C.

The required volume is based off the *Proposed Conditions – Theoretical Model*. The theoretical model is utilized to calculate the required storage volume less the existing impervious area. Once the required storage volume was determined, a *Proposed Conditions – Actual Model* was performed to ensure the actual area (additional existing impervious area) tributary towards each basin is released at the appropriate net allowable release. The orifice sizes and high-water levels associated with the *Proposed Conditions – Actual Model* shall be considered the design / construction parameters. Refer to table 5 and table 6 below for a breakdown of Theoretical & Actual volumes and release rates. See **Exhibit 3** for details.

Table 5: Stormwater Management Analysis Summary – Volume

	Actual Area	Theoretical Area	Theoretical 2 Yr	Actual 2 Year	Theoretical 100 Yr	Plus (+) Volume SF	Actual 100 YR
<i>Allowable Release (0.15 cfs/ac)</i>	Acres	Acres	AC-FT	AC-FT	AC-FT	AC-FT	AC-FT
A	3.30	3.30	N/A	N/A	N/A	N/A	N/A
B	20.79	20.79	2.76	3.25	8.97	8.97	11.00
C	11.24	11.24	1.26	1.51	4.16	4.16	4.99
Total Site Release	35.33	35.33	4.02	4.76	13.13	13.13	15.99
		A + B Combined:					

Table 6: Stormwater Management Analysis Summary – Release Rates

	Actual Area	Theoretical Area	Theoretical 100 Yr	Theoretical 2 Yr	Actual 100 Yr	Actual 2 Yr
<i>Allowable Release (0.15 cfs/ac)</i>	<i>Acres</i>	<i>Acres</i>	<i>CFS</i>	<i>CFS</i>	<i>CFS</i>	<i>CFS</i>
A	3.30	3.30	0.50	0.13	0.50	0.13
B	20.79	20.79	3.12	0.83	3.12	0.83
C	11.24	11.24	1.69	0.45	1.69	0.45
Total Site Release	35.33	35.33	5.30	1.41	5.30	1.41
		A + B Combined:	3.61	0.96	3.61	0.96

2.1 Lake County WDO Detention Volume Safety Factor

Per Lake County WDO Section 508.01, additional detention volume has been provided to accommodate the detention volume safety factor. In this stormwater management, the ratio of existing impervious to proposed on-site tributary drainage area is utilized to calculate the required safety factor of 1.028. The detention volume safety factor has been applied to the *Proposed Conditions – Theoretical Model* and is represented in the aforementioned Table 5. The proposed basins. See below Table 7 for the breakdown.

Table 7: Detention Volume Safety Factor Calculations

	Onsite	Offsite	Ratio of off-site / on-site	0.05 of Ratio	Safety Factor
A	3.30	0.44	0.155	0.008	1.008
B	20.79	3.29	0.188	0.009	1.009
C	11.24	5.66	0.207	0.010	1.010
			0.551	0.028	1.028

2.2. Proposed Compensatory Storage

As previously mentioned, an existing detention facility or depressional storage area will be impacted and filled as part of the proposed development. Based on our analysis, it was determined that there are approximately 1.88 ac-ft of existing storage displaced as part of the development. Since this is a depression, compensatory storage will be compensated at a 1:1 ratio.

Basin 1 proposes to provide an additional 1.88 ac-ft of storage that will compensate for the lost existing storage. To access the compensatory storage, the 100-year restrictor in Basin 1 will be reduced from 5.7-inch diameter to 5.2-inch diameter. This will allow the basin to fill to a High-Water Level (HWL) of 700.66 ft and access both the required stormwater detention volume and existing compensatory storage.

Conclusion

Stormwater management for the proposed improvements has been preliminarily designed in accordance with the stormwater regulations of Lake County and the Village of Libertyville. There are no anticipated adverse impacts to the existing downstream drainage system as a result of the proposed improvements. During final engineering, the proposed storm sewer network will be sized appropriately to convey the 10-year storm event in accordance with the Village of Libertyville Code of Ordinances. Emergency overflow routes will be sized for the 100-year event and detailed in Final Engineering.



Exhibit 1 – Maps

DKES WETLANDS MAP

FEMA FLOODPLAIN MAP

FEMA FIS PROFILE

NRCS SOILS MAP



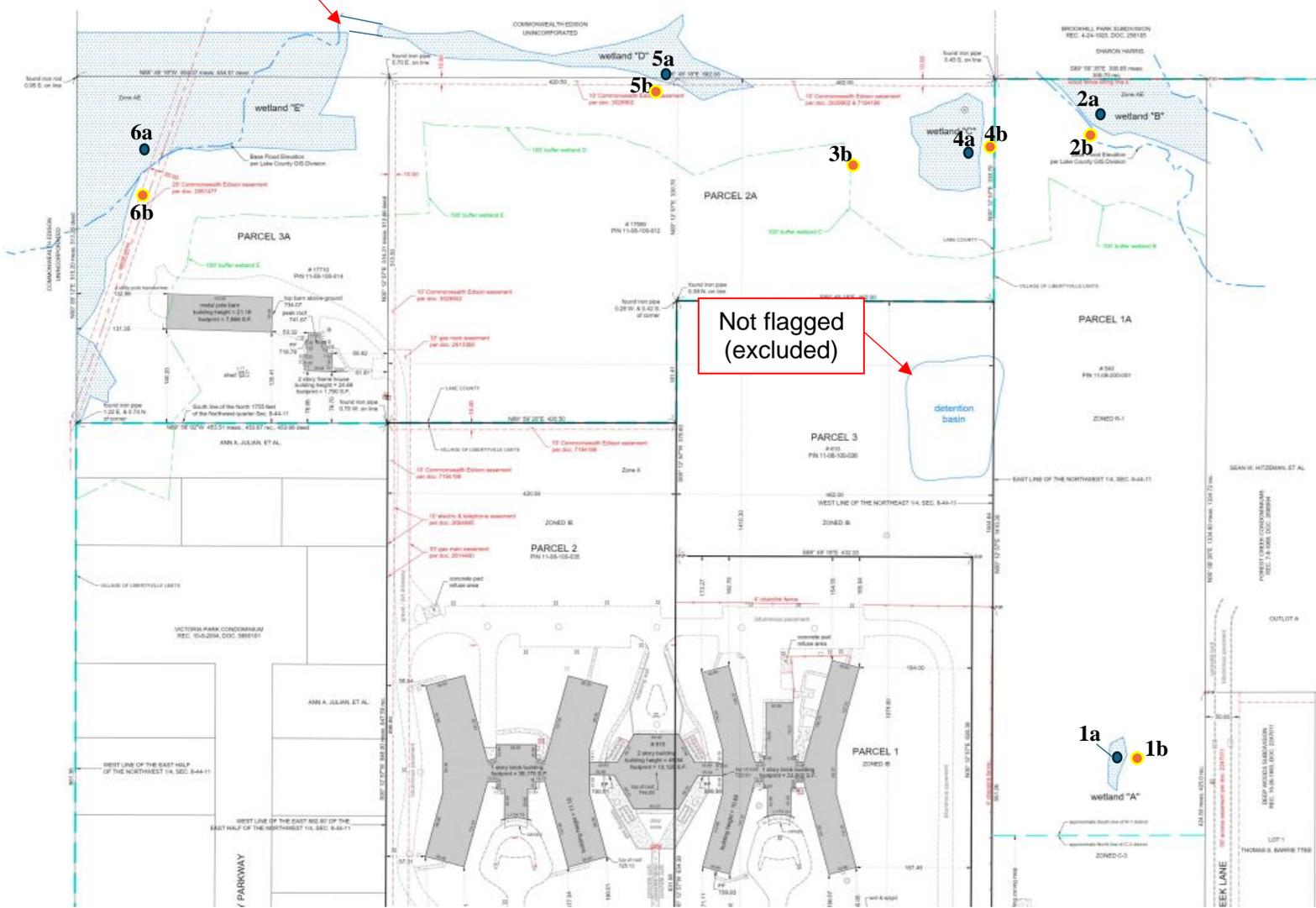


Wetland Boundaries & Data Point Locations – Field Reconnaissance 10.8.2024 (UPD)

WL D & WL E are connected here (off-site)



THIS SURVEY IS BASED UPON CURVED (PROJ.) PLAN (1142) - PULASKI, CHICAGO TITLE INSURANCE COMPANY, COMMITMENT NO. 2116071481535K, EFFECTIVE DATE: FEBRUARY 16, 2022



SUMMARY/NOTES:

- Wetland A through E re-flagged on 10.8.2024 (5 wetland areas total)
- Wetlands A and C are isolated wetlands wholly on-site under Lake County SMC jurisdiction
- Detention Basin not flagged (excluded from wetland regulation)
- USACE jurisdictional Wetlands B, D, and E extend off-site to the north and are part of the Bull Creek ADID wetland complex #94 (Liberty Prairie)

National Flood Hazard Layer FIRMMette



87°59'3"W 42°18'51"N



1:6,000

87°58'25"W 42°18'25"N

Basemap Imagery Source: USGS National Map 2023

Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

SPECIAL FLOOD HAZARD AREAS	Without Base Flood Elevation (BFE) Zone A, V, A99	With BFE or Depth Zone AE, AO, AH, VE, AR	Regulatory Floodway

OTHER AREAS OF FLOOD HAZARD	0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X	Future Conditions 1% Annual Chance Flood Hazard Zone X	Area with Reduced Flood Risk due to Levee. See Notes. Zone X	Area with Flood Risk due to Levee Zone D

OTHER AREAS	NO SCREEN Area of Minimal Flood Hazard Zone X	Effective LOMRs

OTHER AREAS	Area of Undetermined Flood Hazard Zone D

GENERAL STRUCTURES	Channel, Culvert, or Storm Sewer	Levee, Dike, or Floodwall

OTHER FEATURES	20.2 Cross Sections with 1% Annual Chance Water Surface Elevation	17.5 Coastal Transect	Base Flood Elevation Line (BFE)	Limit of Study	Jurisdiction Boundary	Coastal Transect Baseline	Profile Baseline	Hydrographic Feature

MAP PANELS	Digital Data Available	No Digital Data Available	Unmapped



The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

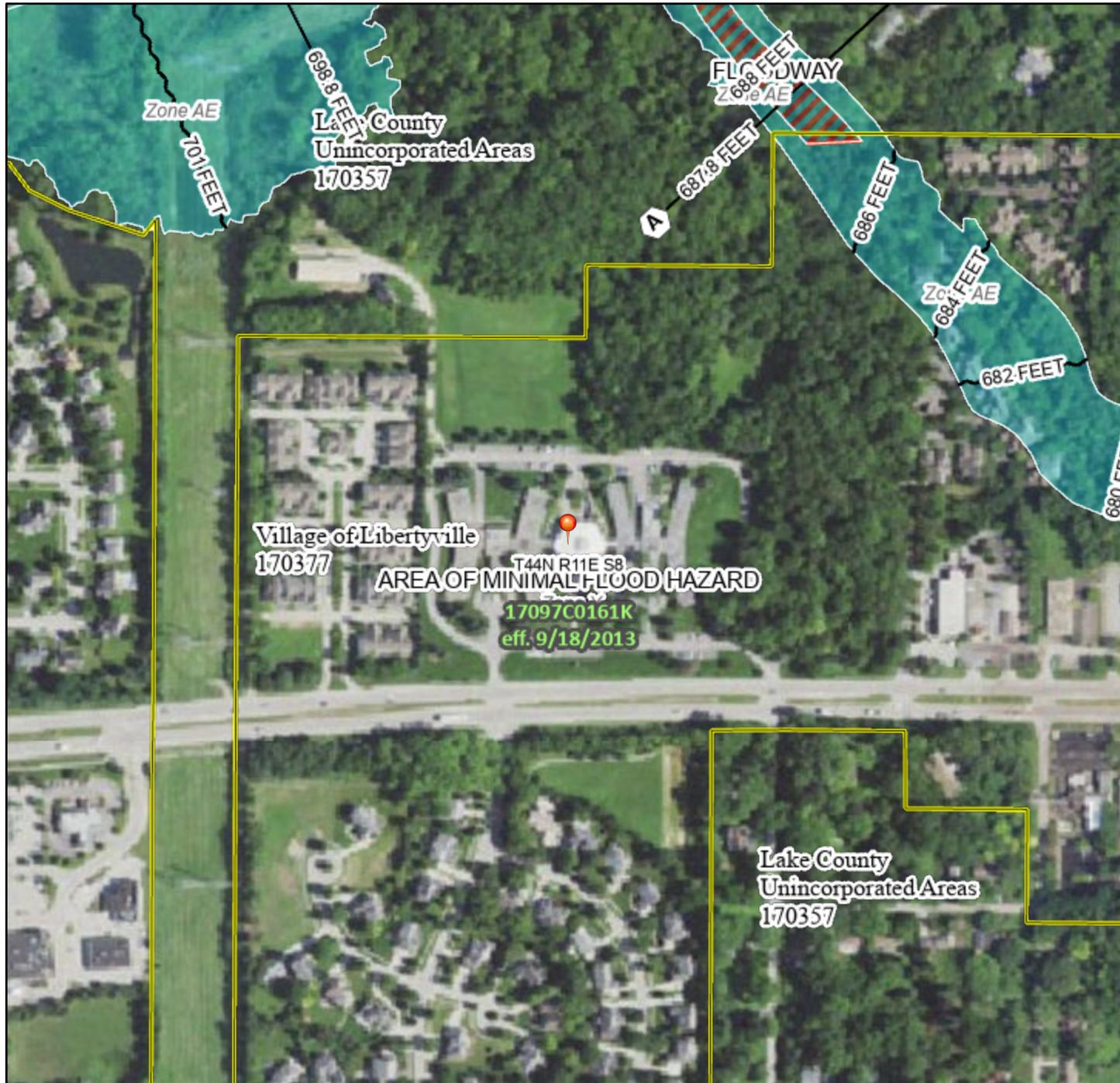
The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on **11/25/2024 at 7:27 PM** and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.

National Flood Hazard Layer FIRMMette



87°58'51"W 42°18'39"N



Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

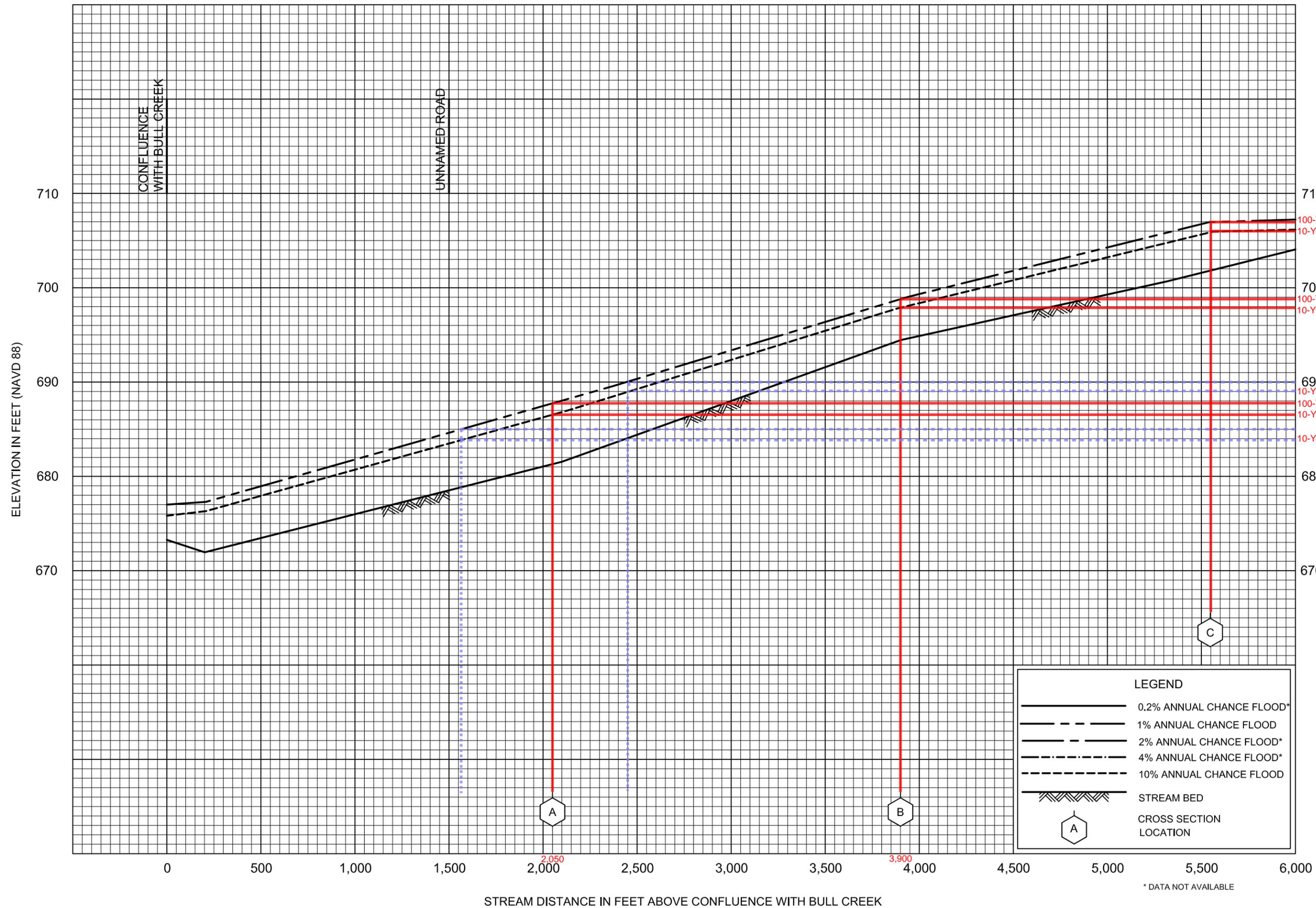
SPECIAL FLOOD HAZARD AREAS		Without Base Flood Elevation (BFE) Zone A, V, A99
		With BFE or Depth Zone AE, AO, AH, VE, AR
		Regulatory Floodway
OTHER AREAS OF FLOOD HAZARD		0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X
		Future Conditions 1% Annual Chance Flood Hazard Zone X
		Area with Reduced Flood Risk due to Levee. See Notes. Zone X
		Area with Flood Risk due to Levee Zone D
OTHER AREAS		NO SCREEN Area of Minimal Flood Hazard Zone X
		Effective LOMRs
		Area of Undetermined Flood Hazard Zone D
GENERAL STRUCTURES		Channel, Culvert, or Storm Sewer
		Levee, Dike, or Floodwall
OTHER FEATURES		20.2 Cross Sections with 1% Annual Chance
		17.5 Water Surface Elevation
		Coastal Transect
		Base Flood Elevation Line (BFE)
		Limit of Study
		Jurisdiction Boundary
MAP PANELS		Digital Data Available
		No Digital Data Available
		Unmapped

The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

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100-YR: 707.0
10-YR: 706.0

100-YR: 698.8
10-YR: 697.9

10-YR @ BASIN 1 OUTFALL: 689.0
100-YR: 687.8
10-YR: 686.5

10-YR @ BASIN 2 OUTFALL: 683.8

FLOOD PROFILES

BULL CREEK TRIBUTARY

FEDERAL EMERGENCY MANAGEMENT AGENCY

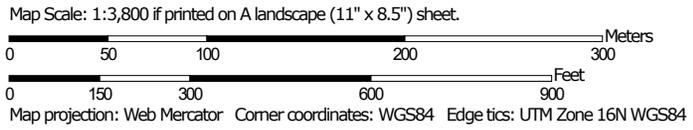
LAKE COUNTY, IL

AND INCORPORATED AREAS

Hydrologic Soil Group—Lake County, Illinois



Soil Map may not be valid at this scale.



MAP LEGEND

Area of Interest (AOI)

 Area of Interest (AOI)

Soils

Soil Rating Polygons

 A
 A/D
 B
 B/D
 C
 C/D
 D
 Not rated or not available

Soil Rating Lines

 A
 A/D
 B
 B/D
 C
 C/D
 D
 Not rated or not available

Soil Rating Points

 A
 A/D
 B
 B/D

 C
 C/D
 D
 Not rated or not available

Water Features

 Streams and Canals

Transportation

 Rails
 Interstate Highways
 US Routes
 Major Roads
 Local Roads

Background

 Aerial Photography

MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:12,000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service
 Web Soil Survey URL:
 Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Lake County, Illinois
 Survey Area Data: Version 19, Aug 21, 2024

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Aug 19, 2022—Sep 30, 2022

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

Hydrologic Soil Group

Map unit symbol	Map unit name	Rating	Acres in AOI	Percent of AOI
146A	Elliott silt loam, 0 to 2 percent slopes	C/D	3.3	6.9%
153A	Pella silty clay loam, 0 to 2 percent slopes	B/D	11.7	24.8%
223B	Varna silt loam, 2 to 4 percent slopes	C	12.9	27.1%
223C2	Varna silt loam, 4 to 6 percent slopes, eroded	C	12.8	27.0%
442A	Mundelein silt loam, 0 to 2 percent slopes	B/D	0.2	0.5%
443B	Barrington silt loam, 2 to 4 percent slopes	C	2.6	5.5%
626A	Kish loam, 0 to 2 percent slopes	B/D	0.4	0.8%
1107A	Sawmill silty clay loam, undrained, cool, 0 to 2 percent slopes, frequently flooded	B/D	3.5	7.4%
Totals for Area of Interest			47.4	100.0%

Description

Hydrologic soil groups are based on estimates of runoff potential. Soils are assigned to one of four groups according to the rate of water infiltration when the soils are not protected by vegetation, are thoroughly wet, and receive precipitation from long-duration storms.

The soils in the United States are assigned to four groups (A, B, C, and D) and three dual classes (A/D, B/D, and C/D). The groups are defined as follows:

Group A. Soils having a high infiltration rate (low runoff potential) when thoroughly wet. These consist mainly of deep, well drained to excessively drained sands or gravelly sands. These soils have a high rate of water transmission.

Group B. Soils having a moderate infiltration rate when thoroughly wet. These consist chiefly of moderately deep or deep, moderately well drained or well drained soils that have moderately fine texture to moderately coarse texture. These soils have a moderate rate of water transmission.

Group C. Soils having a slow infiltration rate when thoroughly wet. These consist chiefly of soils having a layer that impedes the downward movement of water or soils of moderately fine texture or fine texture. These soils have a slow rate of water transmission.

Group D. Soils having a very slow infiltration rate (high runoff potential) when thoroughly wet. These consist chiefly of clays that have a high shrink-swell potential, soils that have a high water table, soils that have a claypan or clay layer at or near the surface, and soils that are shallow over nearly impervious material. These soils have a very slow rate of water transmission.

If a soil is assigned to a dual hydrologic group (A/D, B/D, or C/D), the first letter is for drained areas and the second is for undrained areas. Only the soils that in their natural condition are in group D are assigned to dual classes.

Rating Options

Aggregation Method: Dominant Condition

Component Percent Cutoff: None Specified

Tie-break Rule: Higher



Exhibit 2 – Existing Conditions Exhibits & Models

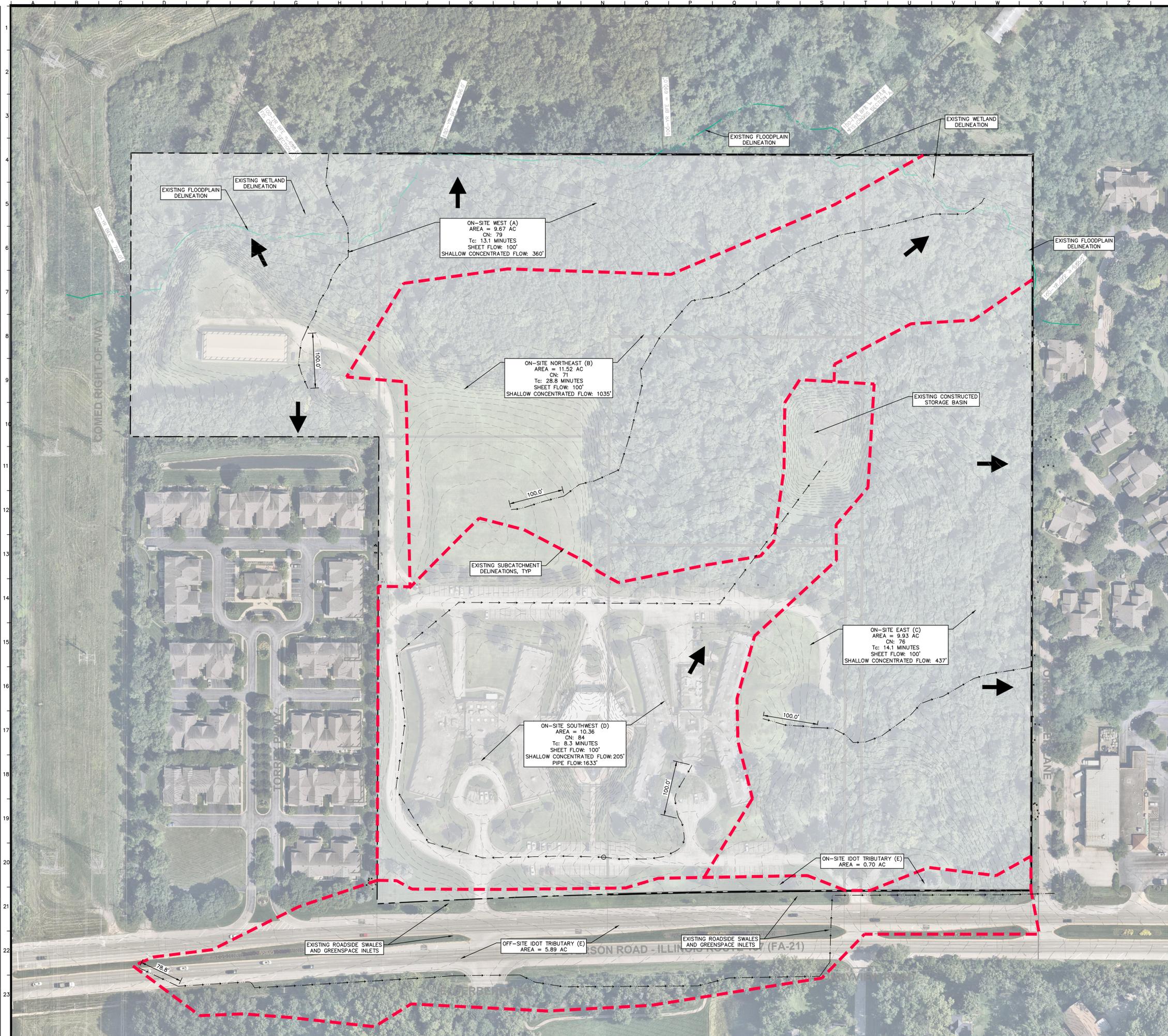
EXISTING CONDITIONS EXHIBIT

EXISTING CONDITIONS HYDROCAD MODEL

EXISTING (PRE 1992) IMPERVIOUS AREA CONDITIONS MODEL



Drawing name: K:\GIS\DEV\168247001_Pulte_Libertyville_IL_V2_Design\CAD\Exhibits\168247001-Existing Drainage Conditions.dwg EKH Dec 12, 2024 10:35am by: IonSpence
 This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.




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 WWW.KIMLEY-HORN.COM

SCALE: AS NOTED
 DESIGNED BY: INS
 DRAWN BY: KTRM
 CHECKED BY: RNM


PULTE GROUP

EXISTING DRAINAGE CONDITIONS EXHIBIT

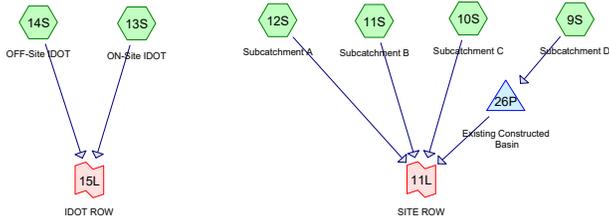
GREENWAY CHASE
 610 PETERSON ROAD
 LIBERTYVILLE, IL 60048

ORIGINAL ISSUE:
 12/11/2024
 KHA PROJECT NO.
 168247001
 SHEET NUMBER
EXH.

Rainfall Events Listing (selected events)

Event#	Event Name	Storm Type	Curve	Mode	Duration (hours)	B/B	Depth (inches)	AMC
1	002YR-024.00HR	Huff 0-10sm	3Q	Scale	24.00	1	3.34	2
2	100YR-024.00HR	Huff 0-10sm	3Q	Scale	24.00	1	8.57	2

Existing Conditions



Routing Diagram for 168247001 HydroCAD
 Prepared by Kimley-Horn & Associates, Printed 12/12/2024
 HydroCAD® 10.20-5c s/n 02344 © 2023 HydroCAD Software Solutions LLC

Summary for Subcatchment 9S: Subcatchment D

[47] Hint: Peak is 103% of capacity of segment #3

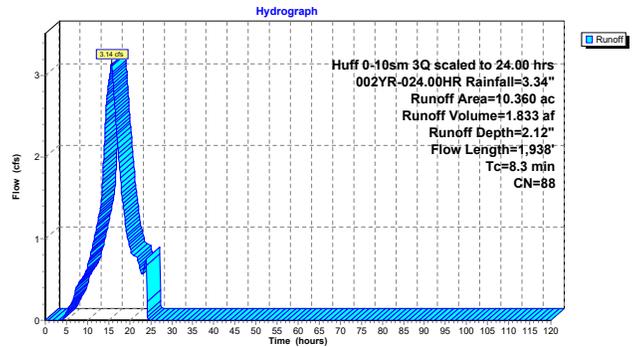
Runoff = 3.14 cfs @ 15.70 hrs, Volume= 1.833 af, Depth= 2.12"
 Routed to Pond 26P : Existing Constructed Basin

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 002YR-024.00HR Rainfall=3.34"

Area (ac)	CN	Description
4.340	98	
6.020	80	>75% Grass cover, Good, HSG D
10.360	88	Weighted Average
6.020		58.11% Pervious Area
4.340		41.89% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
1.4	100	0.0180	1.18		Sheet Flow, n= 0.013 P2= 3.34"
1.3	205	0.0170	2.65		Shallow Concentrated Flow, Shallow Conc Paved Kv= 20.3 fps
3.0	685	0.0052	3.87	3.04	Pipe Channel, RCP, Round 12" 12.0" Round Area= 0.8 sf Perim= 3.1' r= 0.25' n= 0.011 Concrete pipe, straight & clean
2.6	948	0.0097	6.13	7.52	Pipe Channel, RCP, Round 15" 15.0" Round Area= 1.2 sf Perim= 3.9' r= 0.31' n= 0.011 Concrete pipe, straight & clean
8.3	1,938	Total			

Subcatchment 9S: Subcatchment D



Summary for Subcatchment 10S: Subcatchment C

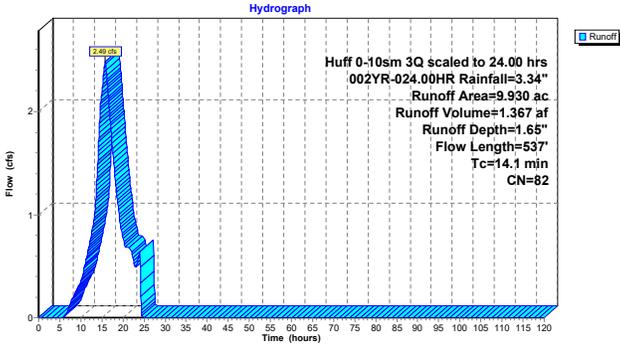
Runoff = 2.49 cfs @ 15.79 hrs, Volume= 1.367 af, Depth= 1.65"
 Routed to Link 11L : SITE ROW

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 002YR-024.00HR Rainfall=3.34"

Area (ac)	CN	Description
9.930	82	Woods/grass comb., Fair, HSG D
9.930		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.9	100	0.0670	0.28		Sheet Flow, Sheet Flow Grass: Short n= 0.150 P2= 3.34"
8.2	437	0.0313	0.88		Shallow Concentrated Flow, Woodland Kv= 5.0 fps
14.1	537	Total			

Subcatchment 10S: Subcatchment C



Summary for Subcatchment 11S: Subcatchment B

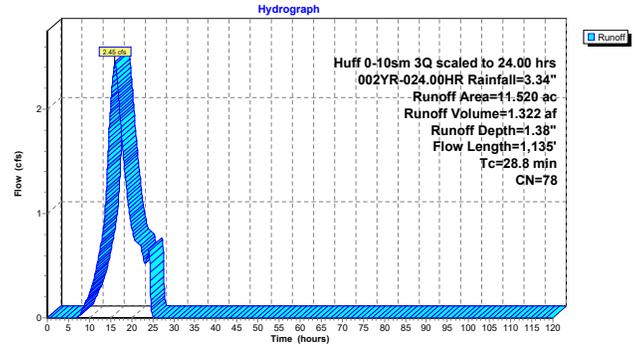
Runoff = 2.45 cfs @ 16.03 hrs, Volume= 1.322 af, Depth= 1.38"
 Routed to Link 11L : SITE ROW

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 002YR-024.00HR Rainfall=3.34"

Area (ac)	CN	Description
2.450	80	>75% Grass cover, Good, HSG D
9.070	77	Woods, Good, HSG D
11.520	78	Weighted Average
11.520		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
8.5	100	0.0273	0.20		Sheet Flow, Sheet Flow Grass: Short n= 0.150 P2= 3.34"
20.3	1,035	0.0290	0.85		Shallow Concentrated Flow, Woodland Kv= 5.0 fps
28.8	1,135	Total			

Subcatchment 11S: Subcatchment B



Summary for Subcatchment 12S: Subcatchment A

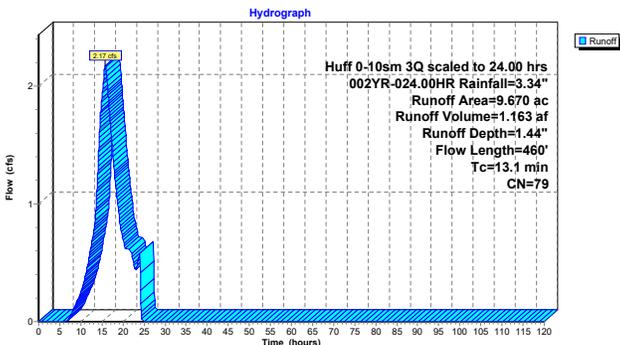
Runoff = 2.17 cfs @ 15.79 hrs, Volume= 1.163 af, Depth= 1.44"
 Routed to Link 11L : SITE ROW

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 002YR-024.00HR Rainfall=3.34"

Area (ac)	CN	Description
0.340	96	Gravel surface, HSG D
0.330	98	Roofs, HSG D
0.820	80	>75% Grass cover, Good, HSG D
8.180	77	Woods, Good, HSG D
9.670	79	Weighted Average
9.340		96.59% Pervious Area
0.330		3.41% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
7.6	100	0.0356	0.22		Sheet Flow, Sheet Flow Grass: Short n= 0.150 P2= 3.34"
5.5	360	0.0480	1.10		Shallow Concentrated Flow, Shallow Concentrated Flow Woodland Kv= 5.0 fps
13.1	460	Total			

Subcatchment 12S: Subcatchment A



Summary for Subcatchment 13S: ON-Site IDOT

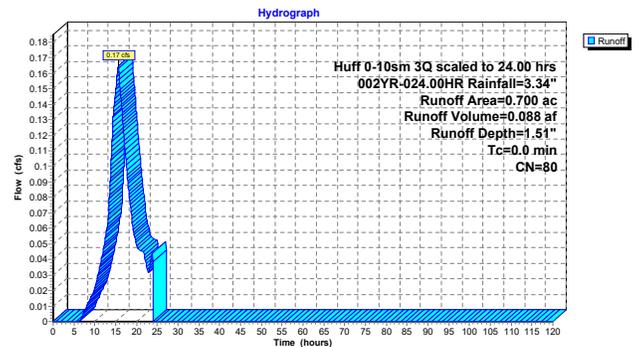
[46] Hint: Tc=0 (Instant runoff peak depends on dt)

Runoff = 0.17 cfs @ 15.60 hrs, Volume= 0.088 af, Depth= 1.51"
 Routed to Link 15L : IDOT ROW

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 002YR-024.00HR Rainfall=3.34"

Area (ac)	CN	Description
0.700	80	>75% Grass cover, Good, HSG D
0.700		100.00% Pervious Area

Subcatchment 13S: ON-Site IDOT



Summary for Subcatchment 14S: OFF-Site IDOT

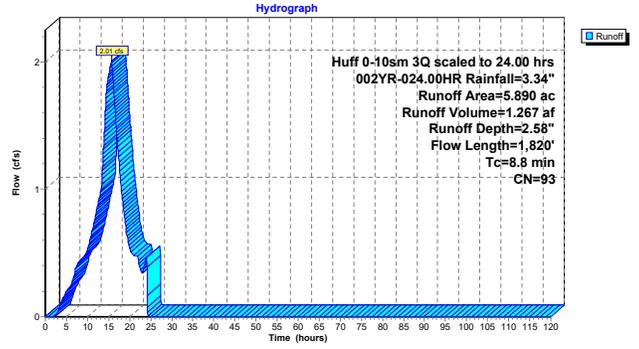
Runoff = 2.01 cfs @ 15.69 hrs, Volume= 1.267 af, Depth= 2.58"
 Routed to Link 15L : IDOT ROW

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 1.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 002YR-024.00HR Rainfall=3.34"

Area (ac)	CN	Description
5.890	93	Paved roads w/open ditches, 50% imp, HSG D
2.945	50.00%	Pervious Area
2.945	50.00%	Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
1.0	79	0.0218	1.29		Sheet Flow, n= 0.012 P2= 3.34"
1.2	136	0.0090	1.93		Shallow Concentrated Flow, Paved Kv= 20.3 fps
1.3	370	0.0830	4.64		Shallow Concentrated Flow, Unpaved Kv= 16.1 fps
0.4	124	0.0060	5.44	9.62	Pipe Channel, RCP_Round 18" 18.0" Round Area= 1.8 sf Perim= 4.7' r= 0.38" n= 0.011 Concrete pipe, straight & clean
4.2	584	0.0211	2.34		Shallow Concentrated Flow, Unpaved Kv= 16.1 fps
0.7	527	0.0224	12.74	40.01	Pipe Channel, RCP_Round 24" 24.0" Round Area= 3.1 sf Perim= 6.3' r= 0.50' n= 0.011 Concrete pipe, straight & clean
8.8	1,820	Total			

Subcatchment 14S: OFF-Site IDOT



Summary for Pond 26P: Existing Constructed Basin

[44] Hint: Outlet device #1 is below defined storage

Inflow Area = 10.360 ac, 41.89% Impervious, Inflow Depth = 2.12" for 002YR-024.00HR event
 Inflow = 3.14 cfs @ 15.70 hrs, Volume= 1.833 af
 Outflow = 3.08 cfs @ 15.85 hrs, Volume= 1.833 af, Atten= 2%, Lag= 9.1 min
 Primary = 3.08 cfs @ 15.85 hrs, Volume= 1.833 af
 Routed to Link 11L : SITE ROW

Routing by Stor-Ind method, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Peak Elev= 688.56' @ 15.85 hrs Surf.Area= 0.115 ac Storage= 0.070 af

Plug-Flow detention time= 17.9 min calculated for 1.833 af (100% of inflow)
 Center-of-Mass det. time= 17.8 min (953.8 - 936.0)

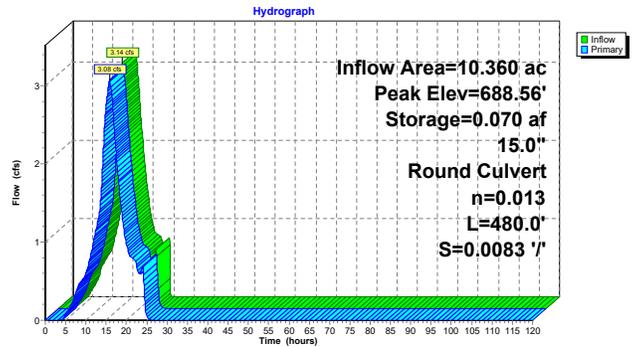
Volume	Invert	Avail. Storage	Storage Description
#1	687.70'	2.233 af	Custom Stage Data (Prismatic), Listed below (Recalc)

Elevation (feet)	Surf. Area (acres)	Inc. Store (acre-feet)	Cum. Store (acre-feet)
687.70	0.008	0.000	0.000
688.00	0.085	0.014	0.014
689.00	0.139	0.112	0.126
690.00	0.175	0.157	0.283
691.00	0.214	0.194	0.477
692.00	0.267	0.240	0.718
693.00	0.325	0.296	1.014
694.00	0.377	0.351	1.365
695.00	0.433	0.405	1.770
696.00	0.494	0.463	2.233

Device	Routing	Invert	Outlet Devices
#1	Primary	687.66'	15.0" Round RCP_Round 15" L= 480.0' RCP, end-section conforming to fill, Ke= 0.500 Inlet / Outlet Invert= 687.66' / 683.66' S= 0.0083 /' Cc= 0.900 n= 0.013, Flow Area= 1.23 sf

Primary OutFlow Max=3.08 cfs @ 15.85 hrs HW=688.56' (Free Discharge)
 1=RCP_Round 15" (Inlet Controls 3.08 cfs @ 3.24 fps)

Pond 26P: Existing Constructed Basin



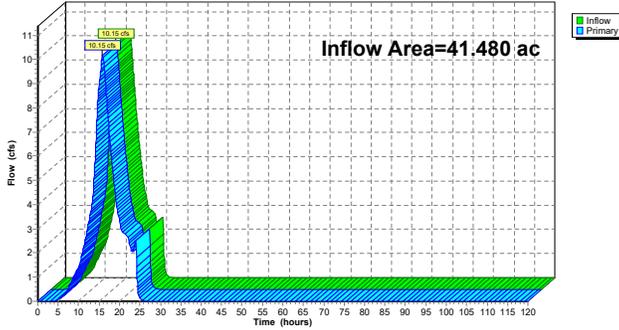
Summary for Link 11L: SITE ROW

Inflow Area = 41.480 ac, 11.26% Impervious, Inflow Depth = 1.64" for 002YR-024.00HR event
 Inflow = 10.15 cfs @ 15.85 hrs, Volume= 5.684 af
 Primary = 10.15 cfs @ 15.85 hrs, Volume= 5.684 af, Atten= 0%, Lag= 0.0 min

Primary outflow = Inflow, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs

Link 11L: SITE ROW

Hydrograph



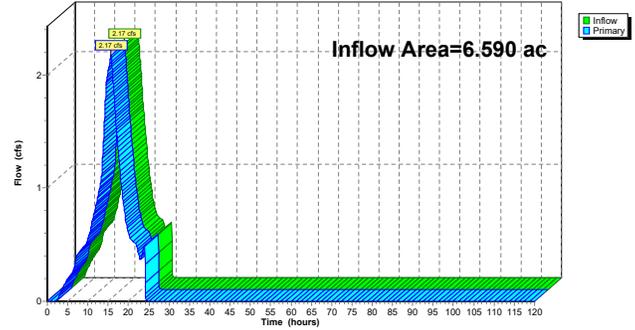
Summary for Link 15L: IDOT ROW

Inflow Area = 6.590 ac, 44.69% Impervious, Inflow Depth = 2.47" for 002YR-024.00HR event
 Inflow = 2.17 cfs @ 15.68 hrs, Volume= 1.355 af
 Primary = 2.17 cfs @ 15.68 hrs, Volume= 1.355 af, Atten= 0%, Lag= 0.0 min

Primary outflow = Inflow, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs

Link 15L: IDOT ROW

Hydrograph



Summary for Subcatchment 9S: Subcatchment D

[47] Hint: Peak is 309% of capacity of segment #3
 [47] Hint: Peak is 125% of capacity of segment #4

Runoff = 9.38 cfs @ 15.68 hrs, Volume= 6.152 af, Depth= 7.13"
 Routed to Pond 26P : Existing Constructed Basin

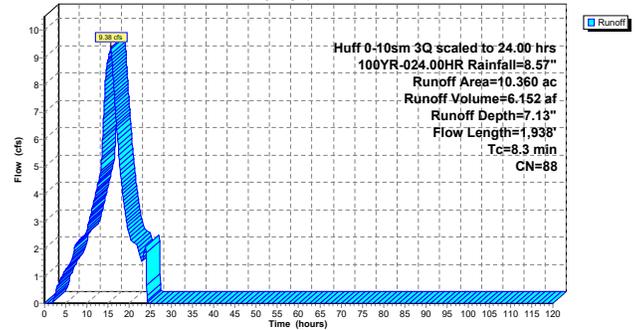
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 100YR-024.00HR Rainfall=8.57"

Area (ac)	CN	Description
4.340	98	
6.020	80	>75% Grass cover, Good, HSG D
10.360	88	Weighted Average
6.020		58.11% Pervious Area
4.340		41.89% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
1.4	100	0.0180	1.18		Sheet Flow, n= 0.013 P2= 3.34"
1.3	205	0.0170	2.65		Shallow Concentrated Flow, Shallow Conc Paved Kv= 20.3 fps
3.0	685	0.0052	3.87	3.04	Pipe Channel, RCP_Round 12" 12.0" Round Area= 0.8 sf Perim= 3.1' r= 0.25' n= 0.011 Concrete pipe, straight & clean
2.6	948	0.0097	6.13	7.52	Pipe Channel, RCP_Round 15" 15.0" Round Area= 1.2 sf Perim= 3.9' r= 0.31' n= 0.011 Concrete pipe, straight & clean
8.3	1,938	Total			

Subcatchment 9S: Subcatchment D

Hydrograph



Summary for Subcatchment 10S: Subcatchment C

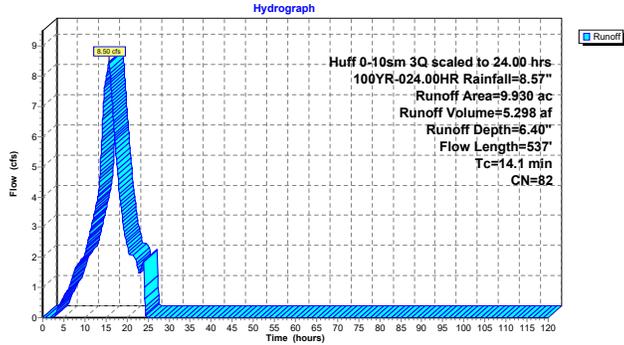
Runoff = 8.50 cfs @ 15.75 hrs, Volume= 5.298 af, Depth= 6.40"
 Routed to Link 11L : SITE ROW

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 100YR-024.00HR Rainfall=8.57"

Area (ac)	CN	Description
9.930	82	Woods/grass comb., Fair, HSG D
9.930		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.9	100	0.0670	0.28		Sheet Flow, Sheet Flow Grass: Short n= 0.150 P2= 3.34"
8.2	437	0.0313	0.88		Shallow Concentrated Flow, Woodland Kv= 5.0 fps
14.1	537	Total			

Subcatchment 10S: Subcatchment C



Summary for Subcatchment 11S: Subcatchment B

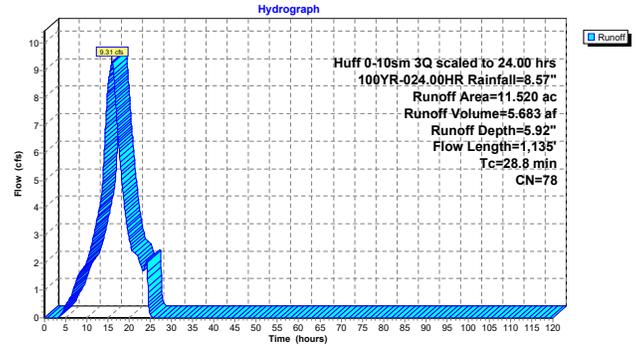
Runoff = 9.31 cfs @ 15.92 hrs, Volume= 5.683 af, Depth= 5.92"
 Routed to Link 11L : SITE ROW

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 100YR-024.00HR Rainfall=8.57"

Area (ac)	CN	Description
2.450	80	>75% Grass cover, Good, HSG D
9.070	77	Woods, Good, HSG D
11.520	78	Weighted Average
11.520		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
8.5	100	0.0273	0.20		Sheet Flow, Sheet Flow Grass: Short n= 0.150 P2= 3.34"
20.3	1,035	0.0290	0.85		Shallow Concentrated Flow, Woodland Kv= 5.0 fps
28.8	1,135	Total			

Subcatchment 11S: Subcatchment B



Summary for Subcatchment 12S: Subcatchment A

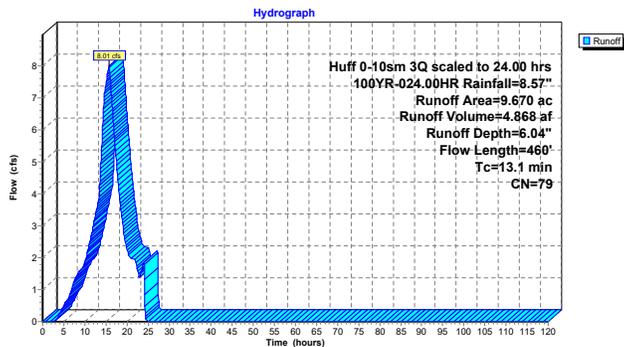
Runoff = 8.01 cfs @ 15.75 hrs, Volume= 4.868 af, Depth= 6.04"
 Routed to Link 11L : SITE ROW

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 100YR-024.00HR Rainfall=8.57"

Area (ac)	CN	Description
0.340	96	Gravel surface, HSG D
0.330	98	Roofs, HSG D
0.820	80	>75% Grass cover, Good, HSG D
8.180	77	Woods, Good, HSG D
9.670	79	Weighted Average
9.340		96.59% Pervious Area
0.330		3.41% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
7.6	100	0.0356	0.22		Sheet Flow, Sheet Flow Grass: Short n= 0.150 P2= 3.34"
5.5	360	0.0480	1.10		Shallow Concentrated Flow, Shallow Concentrated Flow Woodland Kv= 5.0 fps
13.1	460	Total			

Subcatchment 12S: Subcatchment A



Summary for Subcatchment 13S: ON-Site IDOT

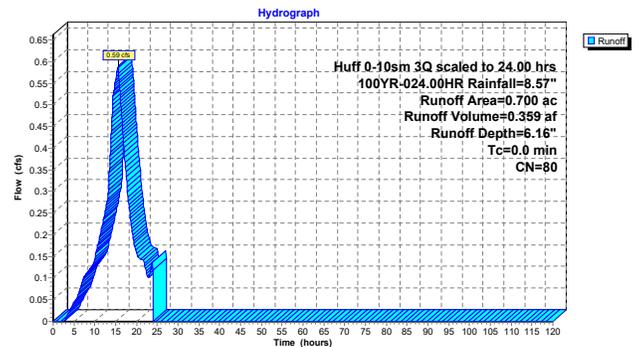
[46] Hint: Tc=0 (Instant runoff peak depends on dt)

Runoff = 0.59 cfs @ 15.59 hrs, Volume= 0.359 af, Depth= 6.16"
 Routed to Link 15L : IDOT ROW

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 100YR-024.00HR Rainfall=8.57"

Area (ac)	CN	Description
0.700	80	>75% Grass cover, Good, HSG D
0.700		100.00% Pervious Area

Subcatchment 13S: ON-Site IDOT



Summary for Subcatchment 14S: OFF-Site IDOT

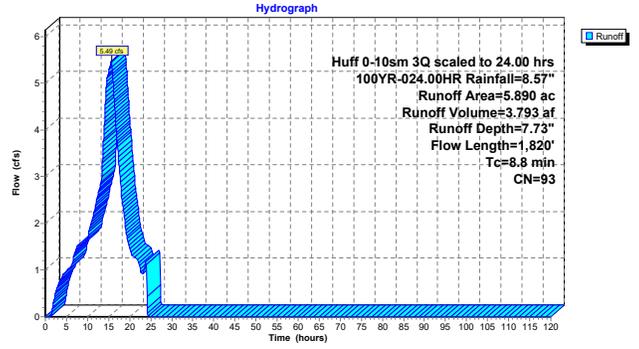
Runoff = 5.49 cfs @ 15.68 hrs, Volume= 3.793 af, Depth= 7.73"
 Routed to Link 15L : IDOT ROW

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 100YR-024.00HR Rainfall=8.57"

Area (ac)	CN	Description
5.890	93	Paved roads w/open ditches, 50% imp, HSG D
2.945		50.00% Pervious Area
2.945		50.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
1.0	79	0.0218	1.29		Sheet Flow, n= 0.012 P2= 3.34"
1.2	136	0.0090	1.93		Shallow Concentrated Flow, Paved Kv= 20.3 fps
1.3	370	0.0830	4.64		Shallow Concentrated Flow, Unpaved Kv= 16.1 fps
0.4	124	0.0060	5.44	9.62	Pipe Channel, RCP_Round 18" 18.0" Round Area= 1.8 sf Perim= 4.7' r= 0.38" n= 0.011 Concrete pipe, straight & clean
4.2	584	0.0211	2.34		Shallow Concentrated Flow, Unpaved Kv= 16.1 fps
0.7	527	0.0224	12.74	40.01	Pipe Channel, RCP_Round 24" 24.0" Round Area= 3.1 sf Perim= 6.3' r= 0.50' n= 0.011 Concrete pipe, straight & clean
8.8	1,820	Total			

Subcatchment 14S: OFF-Site IDOT



Summary for Pond 26P: Existing Constructed Basin

[44] Hint: Outlet device #1 is below defined storage

Inflow Area = 10.360 ac, 41.89% Impervious, Inflow Depth = 7.13" for 100YR-024.00HR event
 Inflow = 9.38 cfs @ 15.68 hrs, Volume= 6.152 af
 Outflow = 7.09 cfs @ 16.72 hrs, Volume= 6.152 af, Atten= 24%, Lag= 62.6 min
 Primary = 7.09 cfs @ 16.72 hrs, Volume= 6.152 af
 Routed to Link 11L : SITE ROW

Routing by Stor-Ind method, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Peak Elev= 691.48' @ 16.72 hrs Surf.Area= 0.240 ac Storage= 0.587 af

Plug-Flow detention time= 30.8 min calculated for 6.152 af (100% of inflow)
 Center-of-Mass det. time= 30.7 min (905.6 - 874.9)

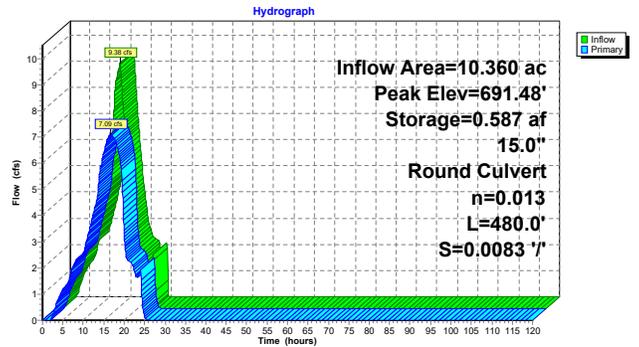
Volume	Invert	Avail. Storage	Storage Description
#1	687.70'	2.233 af	Custom Stage Data (Prismatic), Listed below (Recalc)

Elevation (feet)	Surf. Area (acres)	Inc. Store (acre-feet)	Cum. Store (acre-feet)
687.70	0.008	0.000	0.000
688.00	0.085	0.014	0.014
689.00	0.139	0.112	0.126
690.00	0.175	0.157	0.283
691.00	0.214	0.194	0.477
692.00	0.267	0.240	0.718
693.00	0.325	0.296	1.014
694.00	0.377	0.351	1.365
695.00	0.433	0.405	1.770
696.00	0.494	0.463	2.233

Device	Routing	Invert	Outlet Devices
#1	Primary	687.66'	15.0" Round RCP_Round 15" L= 480.0' RCP, end-section conforming to fill, Ke= 0.500 Inlet / Outlet Invert= 687.66' / 683.66' S= 0.0083 1/ S= 0.0083 1/ Cc= 0.900 n= 0.013, Flow Area= 1.23 sf

Primary OutFlow Max=7.09 cfs @ 16.72 hrs HW=691.48' (Free Discharge)
 1=RCP_Round 15" (Barrel Controls 7.09 cfs @ 5.77 fps)

Pond 26P: Existing Constructed Basin

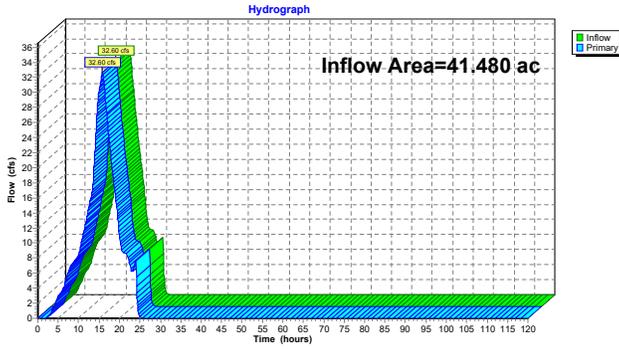


Summary for Link 11L: SITE ROW

Inflow Area = 41.480 ac, 11.26% Impervious, Inflow Depth = 6.36" for 100YR-024.00HR event
 Inflow = 32.60 cfs @ 15.80 hrs, Volume= 22.001 af
 Primary = 32.60 cfs @ 15.80 hrs, Volume= 22.001 af, Atten= 0%, Lag= 0.0 min

Primary outflow = Inflow, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs

Link 11L: SITE ROW

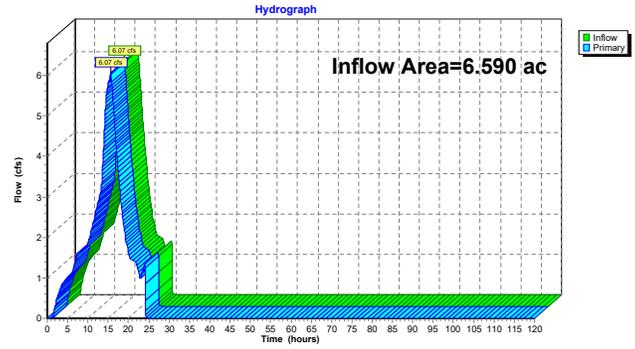


Summary for Link 15L: IDOT ROW

Inflow Area = 6.590 ac, 44.69% Impervious, Inflow Depth = 7.56" for 100YR-024.00HR event
 Inflow = 6.07 cfs @ 15.67 hrs, Volume= 4.153 af
 Primary = 6.07 cfs @ 15.67 hrs, Volume= 4.153 af, Atten= 0%, Lag= 0.0 min

Primary outflow = Inflow, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs

Link 15L: IDOT ROW



Events for Pond 26P: Existing Constructed Basin

Event	Inflow (cfs)	Primary (cfs)	Elevation (feet)	Storage (acre-feet)
002YR-024.00HR	3.14	3.08	688.56	0.070
100YR-001.00HR	59.57	8.73	694.88	1.717
100YR-002.00HR	47.77	8.88	695.24	1.875
100YR-003.00HR	40.14	8.85	695.17	1.844
100YR-006.00HR	27.17	8.61	694.62	1.608
100YR-012.00HR	14.17	8.08	693.46	1.169
100YR-018.00HR	11.68	7.59	692.45	0.845
100YR-024.00HR	9.38	7.09	691.48	0.587
100YR-048.00HR	5.27	5.15	689.05	0.132
100YR-072.00HR	3.74	3.71	688.69	0.085
100YR-120.00HR	2.44	2.43	688.44	0.057

PRE 1992 IMPERVIOUS AREA CONDITIONS

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Page 1

Rainfall Events Listing (selected events)

Event#	Event Name	Storm Type	Curve	Mode	Duration (hours)	B/B	Depth (inches)	AMC
1	100YR-024.00HR	Huff 0-10sm	3Q	Scale	24.00	1	8.57	2

168247001 HydroCAD

Huff 0-10sm 3Q scaled to 24.00 hrs 100YR-024.00HR Rainfall=8.57"
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Page 2

Summary for Subcatchment 15S: EX. Impervious - Subcatchment A

Runoff = 0.41 cfs @ 15.74 hrs, Volume= 0.305 af, Depth= 8.33"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
Huff 0-10sm 3Q scaled to 24.00 hrs 100YR-024.00HR Rainfall=8.57"

Area (ac)	CN	Description
* 0.440	98	Existing Impervious Area
0.440		100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.0					Direct Entry,

Summary for Subcatchment 16S: EX. Impervious - Subcatchment B

Runoff = 3.10 cfs @ 15.74 hrs, Volume= 2.284 af, Depth= 8.33"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
Huff 0-10sm 3Q scaled to 24.00 hrs 100YR-024.00HR Rainfall=8.57"

Area (ac)	CN	Description
* 3.290	98	Existing Impervious Area
3.290		100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.0					Direct Entry,

Summary for Subcatchment 17S: EX. Impervious - Subcatchment C

Runoff = 1.82 cfs @ 15.74 hrs, Volume= 1.340 af, Depth= 8.33"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
Huff 0-10sm 3Q scaled to 24.00 hrs 100YR-024.00HR Rainfall=8.57"

Area (ac)	CN	Description
* 1.930	98	Existing Impervious Area
1.930		100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.0					Direct Entry,

PRE 1992 IMPERVIOUS AREA CONDITIONS

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Multi-Event Tables

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Events for Subcatchment 15S: EX. Impervious - Subcatchment A

Event	Rainfall (inches)	Runoff (cfs)	Volume (acre-feet)	Depth (inches)
002YR-024.00HR	3.34	0.16	0.114	3.11
100YR-001.00HR	4.03	3.26	0.139	3.80
100YR-002.00HR	4.97	2.67	0.174	4.73
100YR-003.00HR	5.49	2.25	0.193	5.25
100YR-006.00HR	6.43	1.52	0.227	6.19
100YR-012.00HR	7.46	0.65	0.265	7.22
100YR-018.00HR	8.06	0.52	0.287	7.82
100YR-024.00HR	8.57	0.41	0.305	8.33
100YR-048.00HR	9.28	0.23	0.331	9.04
100YR-072.00HR	9.85	0.16	0.352	9.61
100YR-120.00HR	10.66	0.11	0.381	10.39

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Multi-Event Tables

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Events for Subcatchment 16S: EX. Impervious - Subcatchment B

Event	Rainfall (inches)	Runoff (cfs)	Volume (acre-feet)	Depth (inches)
002YR-024.00HR	3.34	1.20	0.852	3.11
100YR-001.00HR	4.03	24.37	1.040	3.80
100YR-002.00HR	4.97	19.96	1.298	4.73
100YR-003.00HR	5.49	16.86	1.440	5.25
100YR-006.00HR	6.43	11.36	1.697	6.19
100YR-012.00HR	7.46	4.90	1.980	7.22
100YR-018.00HR	8.06	3.87	2.144	7.82
100YR-024.00HR	8.57	3.10	2.284	8.33
100YR-048.00HR	9.28	1.72	2.478	9.04
100YR-072.00HR	9.85	1.22	2.635	9.61
100YR-120.00HR	10.66	0.80	2.848	10.39

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Multi-Event Tables

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Events for Subcatchment 17S: EX. Impervious - Subcatchment C

Event	Rainfall (inches)	Runoff (cfs)	Volume (acre-feet)	Depth (inches)
002YR-024.00HR	3.34	0.70	0.500	3.11
100YR-001.00HR	4.03	14.30	0.610	3.80
100YR-002.00HR	4.97	11.71	0.761	4.73
100YR-003.00HR	5.49	9.89	0.845	5.25
100YR-006.00HR	6.43	6.66	0.996	6.19
100YR-012.00HR	7.46	2.87	1.161	7.22
100YR-018.00HR	8.06	2.27	1.258	7.82
100YR-024.00HR	8.57	1.82	1.340	8.33
100YR-048.00HR	9.28	1.01	1.454	9.04
100YR-072.00HR	9.85	0.72	1.545	9.61
100YR-120.00HR	10.66	0.47	1.671	10.39



Exhibit 3 – Proposed Conditions Exhibits & Models

PROPOSED CONDITIONS EXHIBIT

PROPOSED LOT COVERAGE EXHIBIT

PROPOSED CONDITIONS – THEORETICAL HYDROCAD MODEL

PROPOSED CONDITIONS – ACTUAL HYDROCAD MODEL

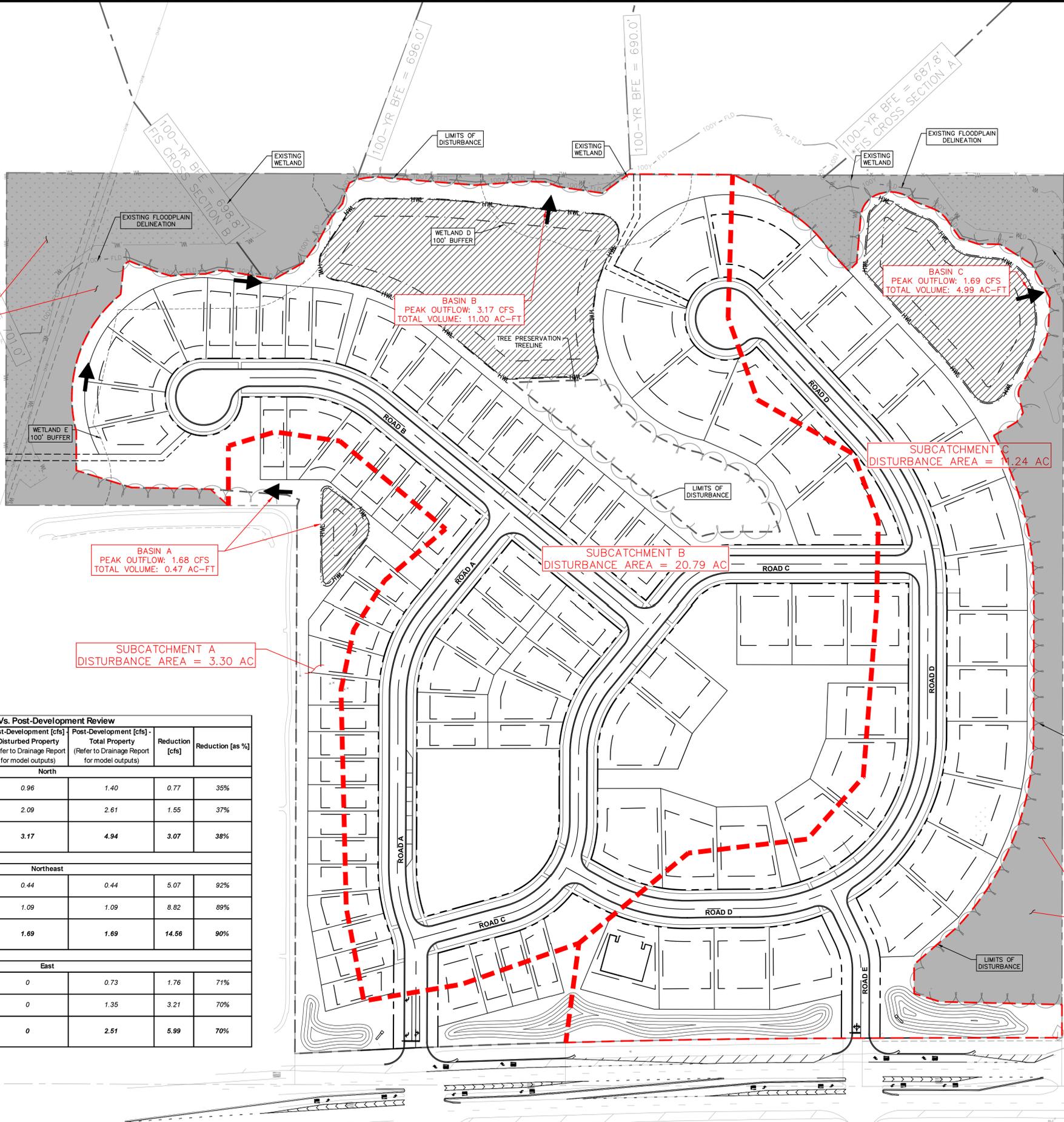
BASIN OVERFLOW WEIR SIZING – HYDROCAD MODEL OUTPUT (PEAK EVENT)



Drawing name: K:\GIS\DEV\168247001_Pulte_Libertyville_IL_V2_Design\CAD\Exhibits\168247001-Proposed Drainage Conditions_SMP.dwg EKH, May 14, 2025 3:14pm by: Ian.Spence
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NORTH
 GRAPHIC SCALE IN FEET
 0 40 80 160



UNDISTURBED NORTHWEST
 PEAK OUTFLOW: 2.55 CFS
 TOTAL VOLUME: 1.51 AC-FT

BASIN B
 PEAK OUTFLOW: 3.17 CFS
 TOTAL VOLUME: 11.00 AC-FT

BASIN C
 PEAK OUTFLOW: 1.69 CFS
 TOTAL VOLUME: 4.99 AC-FT

BASIN A
 PEAK OUTFLOW: 1.68 CFS
 TOTAL VOLUME: 0.47 AC-FT

SUBCATCHMENT B
 DISTURBANCE AREA = 20.79 AC

SUBCATCHMENT C
 DISTURBANCE AREA = 11.24 AC

SUBCATCHMENT A
 DISTURBANCE AREA = 3.30 AC

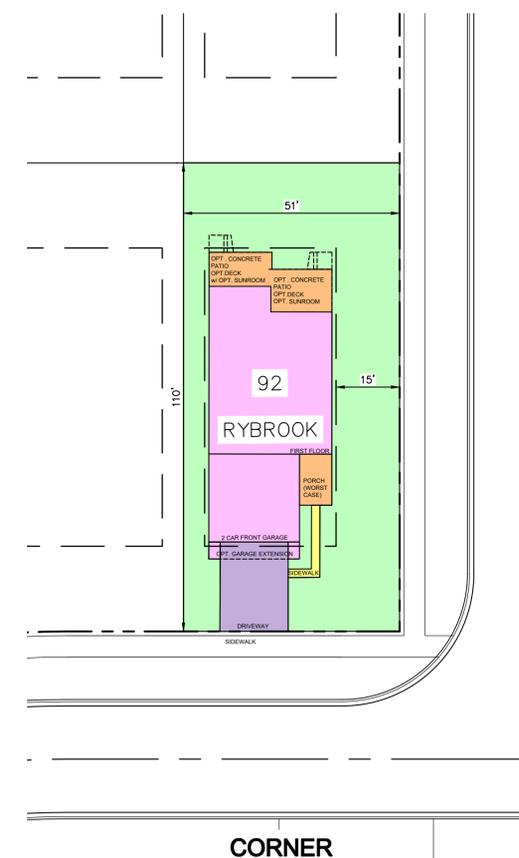
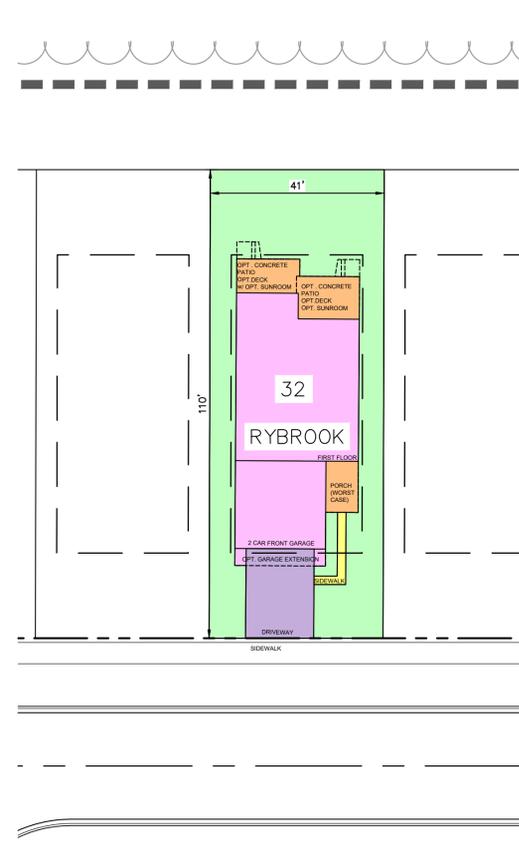
UNDISTURBED EAST
 PEAK OUTFLOW: 2.51 CFS
 TOTAL VOLUME: 1.56 AC-FT

North Basin: 4:1				
Elevation (ft)	Area (ft ²)	Area (acre)	Average Area (acre)	Cummulative Storage (acre-ft)
694.00	55,882	1.313		0.00
701.00	88,518	2.032	1.673	11.71
East Basin: 4:1				
Elevation (ft)	Area (ft ²)	Area (acre)	Average Area (acre)	Cummulative Storage (acre-ft)
685.50	25,962	0.610		0.00
692.00	47,182	1.083	0.847	5.50
West Basin: 4:1				
Elevation (ft)	Area (ft ²)	Area (acre)	Average Area (acre)	Cummulative Storage (acre-ft)
701.00	1,639	0.039		0.00
706.00	7,705	0.177	0.108	0.54

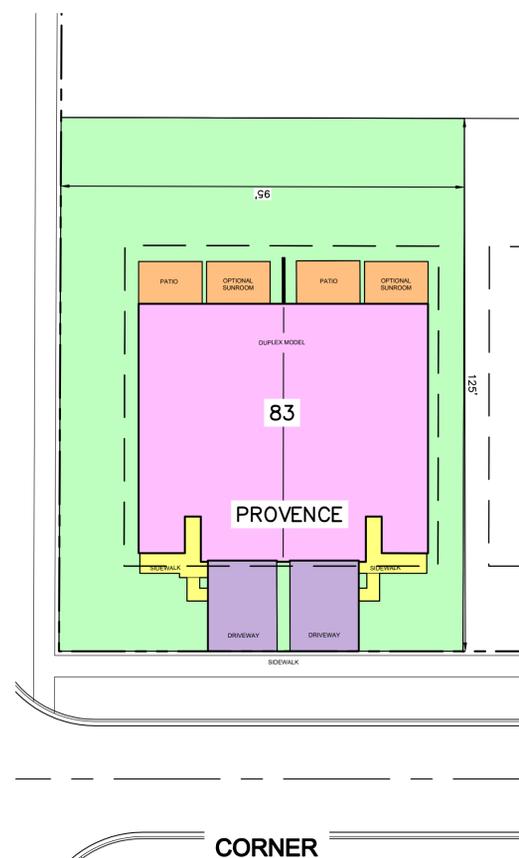
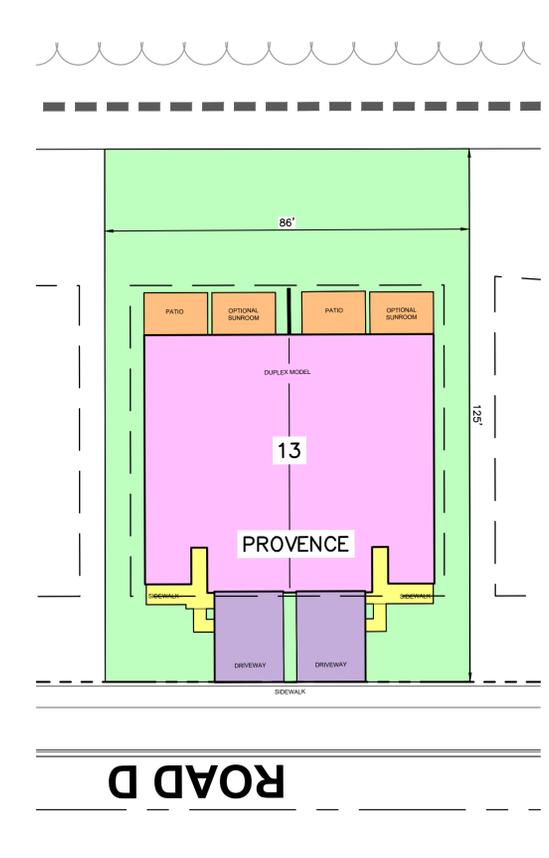
Pre- Vs. Post-Development Review					
	Pre-Development [cfs] (Refer to Drainage Report for model outputs)	Post-Development [cfs] Disturbed Property (Refer to Drainage Report for model outputs)	Post-Development [cfs] Total Property (Refer to Drainage Report for model outputs)	Reduction [cfs]	Reduction [as %]
North					
Runoff Rate (2-Year, 24-Hour)	2.17	0.96	1.40	0.77	35%
Runoff Rate (10-Year, 24-Hour)	4.16	2.09	2.61	1.55	37%
Runoff Rate (100-Year, 24-Hour)	8.01	3.17	4.94	3.07	38%
Northeast					
Runoff Rate (2-Year, 24-Hour)	5.51	0.44	0.44	5.07	92%
Runoff Rate (10-Year, 24-Hour)	9.91	1.09	1.09	8.82	89%
Runoff Rate (100-Year, 24-Hour)	16.25	1.69	1.69	14.56	90%
East					
Runoff Rate (2-Year, 24-Hour)	2.49	0	0.73	1.76	71%
Runoff Rate (10-Year, 24-Hour)	4.56	0	1.35	3.21	70%
Runoff Rate (100-Year, 24-Hour)	8.5	0	2.51	5.99	70%

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SCALE: AS NOTED	DESIGNED BY: INS	DRAWN BY: KTRM	CHECKED BY: RNM		
PROPOSED DRAINAGE CONDITIONS EXHIBIT			PULTE HOME COMPANY, LLC		
GREENWAY CHASE			610 PETERSON ROAD LIBERTYVILLE, IL 60048		
ORIGINAL ISSUE: 01/03/2025 KHA PROJECT NO. 168247001 SHEET NUMBER					
EXH.					

Drawing name: K:\CHS_DEV\168247001_Pulte_Libertyville_A_V2_Design\CAD\Exhibits\168247001-Lot Coverage Exhibits.dwg EXH May 13, 2025 6:11pm By: Kiera R. Miller
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SINGLE FAMILY (RYBROOK) LOTS
1" = 20'



DUPLEX (PROVENCE) LOTS
1" = 20'

SINGLE-FAMILY (RYBROOK) LOT 32 (4,510 S.F.)	
BUILDING STRUCTURE	1,582 S.F.
DRIVEWAY	273 S.F.
PORCH/PATIO	353 S.F.
WALK	45 S.F.
TOTAL IMPERVIOUS	2,253 S.F.
TOTAL OPEN SPACE	2,257 S.F. (50.04%)
BUILDING COVERAGE	35.10%
LOT COVERAGE	49.90%

SINGLE-FAMILY (RYBROOK) CORNER LOT 92 (5,610 S.F.)	
BUILDING STRUCTURE	1,582 S.F.
DRIVEWAY	273 S.F.
PORCH/PATIO	353 S.F.
SIDEWALK	45 S.F.
TOTAL IMPERVIOUS	2,253 S.F.
TOTAL OPEN SPACE	3,356 S.F. (59.8%)
BUILDING COVERAGE	28.20%
LOT COVERAGE	40.20%

DUPLEX FAMILY (PROVENCE) LOT 13 (10,750 S.F.)	
BUILDING STRUCTURE	4,009 S.F.
DRIVEWAY	692 S.F.
PORCH/PATIO	598 S.F.
SIDEWALK	264 S.F.
TOTAL IMPERVIOUS	5,563 S.F.
TOTAL OPEN SPACE	5,208 S.F. (48.5%)
BUILDING COVERAGE	40.00%
LOT COVERAGE	52.30%

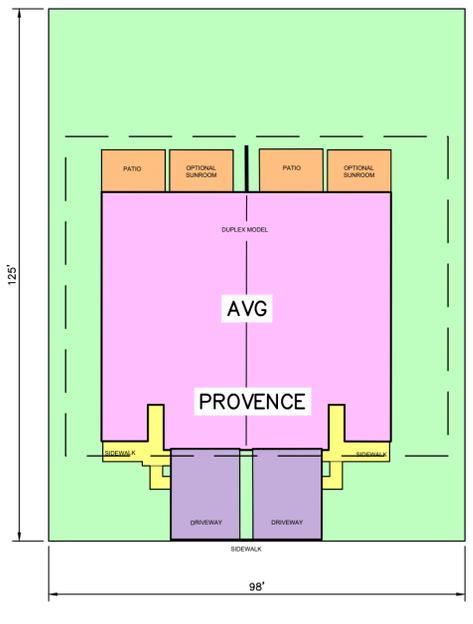
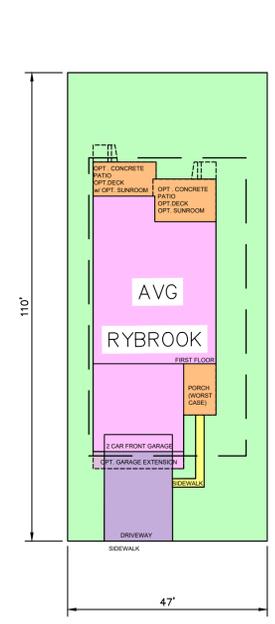
DUPLEX FAMILY (PROVENCE) CORNER LOT 83 (11,923 S.F.)	
BUILDING STRUCTURE	4,009 S.F.
DRIVEWAY	692 S.F.
PORCH/PATIO	598 S.F.
SIDEWALK	264 S.F.
TOTAL IMPERVIOUS	5,563 S.F.
TOTAL OPEN SPACE	6,367 S.F. (53.4%)
BUILDING COVERAGE	33.60%
LOT COVERAGE	46.70%

SINGLE-FAMILY AVERAGE LOT (5,179 S.F.)	
BUILDING STRUCTURE	1,582 S.F.
DRIVEWAY	273 S.F.
PORCH/PATIO	353 S.F.
SIDEWALK	45 S.F.
TOTAL IMPERVIOUS	2,253 S.F.
TOTAL OPEN SPACE	2,936 S.F. (56.7%)
BUILDING COVERAGE	30.50%
LOT COVERAGE	43.50%

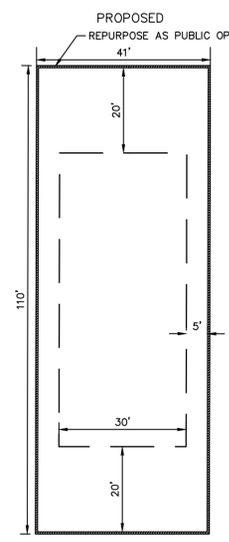
DUPLEX FAMILY AVERAGE LOT (12,275 S.F.)	
BUILDING STRUCTURE	4,009 S.F.
DRIVEWAY	692 S.F.
PORCH/PATIO	598 S.F.
SIDEWALK	264 S.F.
TOTAL IMPERVIOUS	5,563 S.F.
TOTAL OPEN SPACE	6,707 S.F. (54.6%)
BUILDING COVERAGE	32.70%
LOT COVERAGE	45.30%

LEGEND

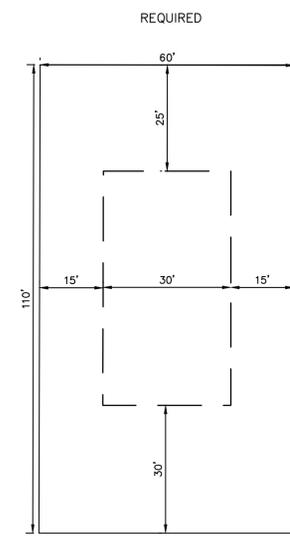
- DRIVEWAY
- PORCH/PATIO
- SIDEWALK
- BUILDING STRUCTURE
- GREEN SPACE



SINGLE FAMILY & DUPLEX AVERAGE LOTS
1" = 20'



A = 4,510 SF
LOT COVERAGE <
BUILDING COVERAGE <



A = 7,200 SF
LOT COVERAGE <
BUILDING COVERAGE <

SF REQUIRED VS. SF PROPOSED
1" = 20'

R-7 SINGLE-FAMILY CODE REQUIREMENT

MAX LOT COVERAGE FOR SINGLE-FAMILY ATTACHED OR TWO-FAMILY DWELLING ON AN INTERIOR LOT RESIDENTIAL DISTRICT: 50% (VARIANCE REQUIRED)

MAX LOT COVERAGE FOR SINGLE-FAMILY DETACHED ON AN INTERIOR LOT RESIDENTIAL DISTRICT: 45% (VARIANCE REQUIRED)

MAX LOT COVERAGE FOR SINGLE-FAMILY DETACHED DWELLING ON A CORNER LOT RESIDENTIAL DISTRICT: 40%

MAX LOT COVERAGE FOR SINGLE-FAMILY ATTACHED OR TWO FAMILY DWELLING ON A CORNER LOT RESIDENTIAL DISTRICT: 45% (VARIANCE REQUIRED)

MAX BUILDING INTERIOR COVERAGE ATTACHED RESIDENTIAL DISTRICT: 35% (VARIANCE REQUIRED)

MAX BUILDING CORNER COVERAGE ATTACHED RESIDENTIAL DISTRICT: 40%

MAX BUILDING COVERAGE DETACHED RESIDENTIAL DISTRICT: 40%

NO.	DATE	BY	REVISIONS	DATE	BY	REVISIONS	DATE
1	03/28/25	RNM	VILLAGE RESUBMITTAL	03/28/25	RNM	VILLAGE RESUBMITTAL	03/28/25
2	03/07/25	DC	ARC RE-SUBMITTAL	03/07/25	DC	ARC RE-SUBMITTAL	03/07/25
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SCALE:	DESIGNED BY: INS	DRAWN BY: KTRM	CHECKED BY: RNM				
PULTE HOME COMPANY, LLC							
LOT COVERAGE							
GREENWAY CHASE 610 PETERSON ROAD LIBERTYVILLE, IL 60048							
ORIGINAL ISSUE: 01/03/2025 KHA PROJECT NO. 168247001 SHEET NUMBER							
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PROPOSED CONDITIONS - THEORETICAL MODEL (DETENTION VOLUME SIZING)

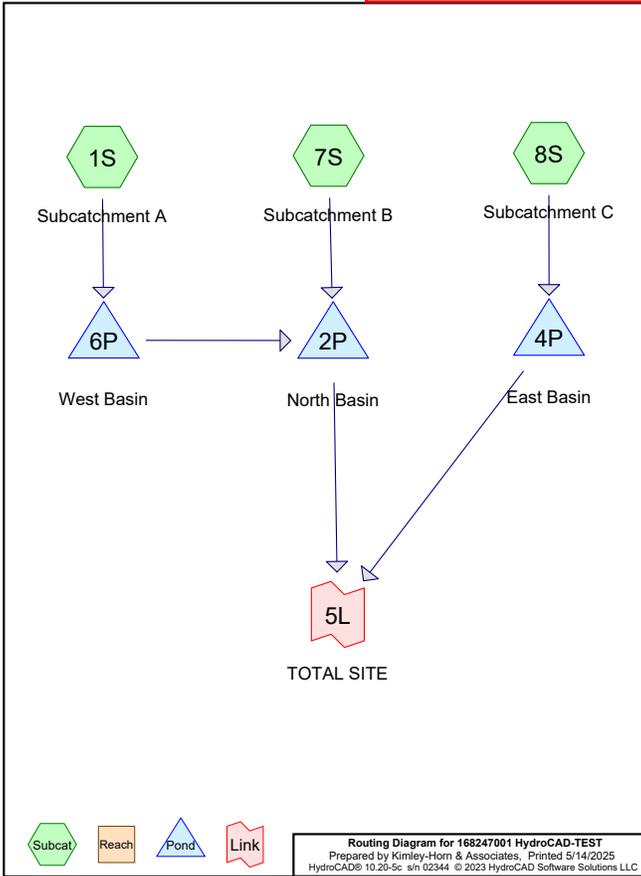
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Rainfall Events Listing (selected events)

Event#	Event Name	Storm Type	Curve	Mode	Duration (hours)	B/B	Depth (inches)	AMC
1	002YR-024.00HR	Huff 0-10sm	3Q	Scale	24.00	1	3.34	2
2	100YR-024.00HR	Huff 0-10sm	3Q	Scale	24.00	1	8.57	2



168247001 HydroCAD-TE Huff 0-10sm 3Q scaled to 24.00 hrs 002YR-024.00HR Rainfall=3.34"
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Summary for Subcatchment 1S: Subcatchment A

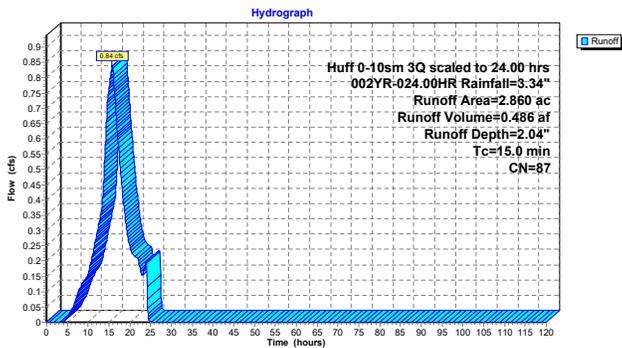
Runoff = 0.84 cfs @ 15.78 hrs, Volume= 0.486 af, Depth= 2.04"
Routed to Pond 6P : West Basin

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
Huff 0-10sm 3Q scaled to 24.00 hrs 002YR-024.00HR Rainfall=3.34"

Area (ac)	CN	Description
* 2.860	87	
2.860		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.0					Direct Entry,

Subcatchment 1S: Subcatchment A



168247001 HydroCAD-TE Huff 0-10sm 3Q scaled to 24.00 hrs 002YR-024.00HR Rainfall=3.34"
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Summary for Subcatchment 7S: Subcatchment B

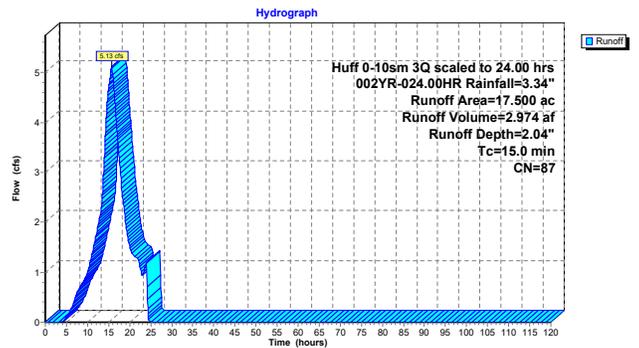
Runoff = 5.13 cfs @ 15.78 hrs, Volume= 2.974 af, Depth= 2.04"
Routed to Pond 2P : North Basin

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
Huff 0-10sm 3Q scaled to 24.00 hrs 002YR-024.00HR Rainfall=3.34"

Area (ac)	CN	Description
* 17.500	87	
17.500		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.0					Direct Entry,

Subcatchment 7S: Subcatchment B



Summary for Subcatchment 8S: Subcatchment C

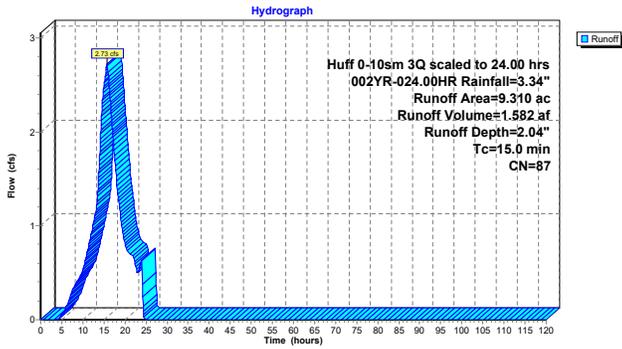
Runoff = 2.73 cfs @ 15.78 hrs, Volume= 1.582 af, Depth= 2.04"
 Routed to Pond 4P : East Basin

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 002YR-024.00HR Rainfall=3.34"

Area (ac)	CN	Description
9.310	87	
9.310		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.0					Direct Entry,

Subcatchment 8S: Subcatchment C



Summary for Pond 2P: North Basin

Inflow Area = 20.360 ac, 0.00% Impervious, Inflow Depth = 2.04" for 002YR-024.00HR event
 Inflow = 5.75 cfs @ 15.80 hrs, Volume= 3.460 af
 Outflow = 0.81 cfs @ 24.22 hrs, Volume= 3.324 af, Atten= 86%, Lag= 505.2 min
 Primary = 0.81 cfs @ 24.22 hrs, Volume= 3.324 af
 Routed to Link 5L : TOTAL SITE

Routing by Stor-Ind method, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Peak Elev= 695.95' @ 24.22 hrs Surf.Area= 1.511 ac Storage= 2.755 af

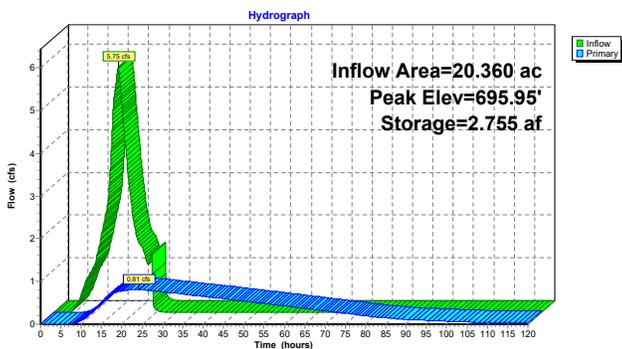
Plug-Flow detention time= 1,735.3 min calculated for 3.324 af (96% of inflow)
 Center-of-Mass det. time= 1,715.8 min (2,673.3 - 957.5)

Volume	Invert	Avail.Storage	Storage Description
#1	694.00'	11.690 af	Custom Stage Data (Prismatic) Listed below (Recalc)
Elevation (feet)	Surf.Area (acres)	Inc.Store (acre-feet)	Cum.Store (acre-feet)
694.00	1.310	0.000	0.000
701.00	2.030	11.690	11.690

Device	Routing	Invert	Outlet Devices
#1	Primary	694.00'	4.8" Vert. Orifice/Grate C= 0.610 Limited to weir flow at low heads
#2	Primary	695.95'	5.7" Vert. Orifice/Grate C= 0.610 Limited to weir flow at low heads

Primary OutFlow Max=0.81 cfs @ 24.22 hrs HW=695.95' (Free Discharge)
 1=Orifice/Grate (Orifice Controls 0.81 cfs @ 6.48 fps)
 2=Orifice/Grate (Orifice Controls 0.00 cfs @ 0.19 fps)

Pond 2P: North Basin



Summary for Pond 4P: East Basin

Inflow Area = 9.310 ac, 0.00% Impervious, Inflow Depth = 2.04" for 002YR-024.00HR event
 Inflow = 2.73 cfs @ 15.78 hrs, Volume= 1.582 af
 Outflow = 0.36 cfs @ 24.21 hrs, Volume= 1.539 af, Atten= 87%, Lag= 505.4 min
 Primary = 0.36 cfs @ 24.21 hrs, Volume= 1.539 af
 Routed to Link 5L : TOTAL SITE

Routing by Stor-Ind method, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Peak Elev= 687.36' @ 24.21 hrs Surf.Area= 0.745 ac Storage= 1.263 af

Plug-Flow detention time= 1,770.1 min calculated for 1.539 af (97% of inflow)
 Center-of-Mass det. time= 1,758.4 min (2,707.7 - 949.3)

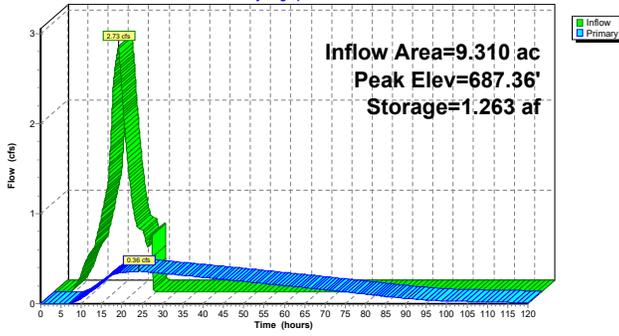
Volume	Invert	Avail.Storage	Storage Description
#1	685.50'	5.493 af	Custom Stage Data (Prismatic) Listed below (Recalc)
Elevation (feet)	Surf.Area (acres)	Inc.Store (acre-feet)	Cum.Store (acre-feet)
685.50	0.610	0.000	0.000
692.00	1.080	5.493	5.493

Device	Routing	Invert	Outlet Devices
#1	Primary	685.50'	3.2" Vert. Orifice/Grate C= 0.610 Limited to weir flow at low heads
#2	Primary	687.36'	4.0" Vert. Orifice/Grate C= 0.610 Limited to weir flow at low heads

Primary OutFlow Max=0.36 cfs @ 24.21 hrs HW=687.36' (Free Discharge)
 1=Orifice/Grate (Orifice Controls 0.36 cfs @ 6.44 fps)
 2=Orifice/Grate (Orifice Controls 0.00 cfs @ 0.23 fps)

Pond 4P: East Basin

Hydrograph



Summary for Pond 6P: West Basin

Inflow Area = 2.860 ac, 0.00% Impervious, Inflow Depth = 2.04" for 002YR-024.00HR event
 Inflow = 0.84 cfs @ 15.78 hrs, Volume= 0.486 af
 Outflow = 0.66 cfs @ 16.79 hrs, Volume= 0.486 af, Atten= 21%, Lag= 60.6 min
 Primary = 0.66 cfs @ 16.79 hrs, Volume= 0.486 af
 Routed to Pond 2P : North Basin

Routing by Stor-Ind method, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Peak Elev= 702.20' @ 16.79 hrs Surf.Area= 0.072 ac Storage= 0.067 af

Plug-Flow detention time= 59.4 min calculated for 0.486 af (100% of inflow)
 Center-of-Mass det. time= 58.6 min (1,007.9 - 949.3)

Volume	Invert	Avail.Storage	Storage Description
#1	701.00'	1.628 af	Custom Stage Data (Prismatic) Listed below (Recalc)

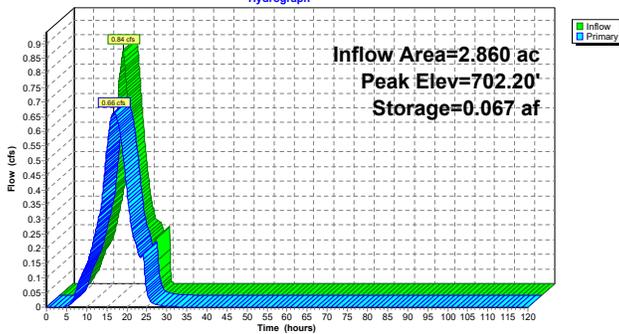
Elevation (feet)	Surf.Area (acres)	Inc.Store (acre-feet)	Cum.Store (acre-feet)
701.00	0.039	0.000	0.000
706.00	0.177	0.540	0.540
707.00	2.000	1.088	1.628

Device	Routing	Invert	Outlet Devices
#1	Primary	701.00'	5.0" Vert. Orifice/Gate C= 0.610 Limited to weir flow at low heads

Primary OutFlow Max=0.66 cfs @ 16.79 hrs HW=702.20' (Free Discharge)
 1=Orifice/Gate (Orifice Controls 0.66 cfs @ 4.87 fps)

Pond 6P: West Basin

Hydrograph



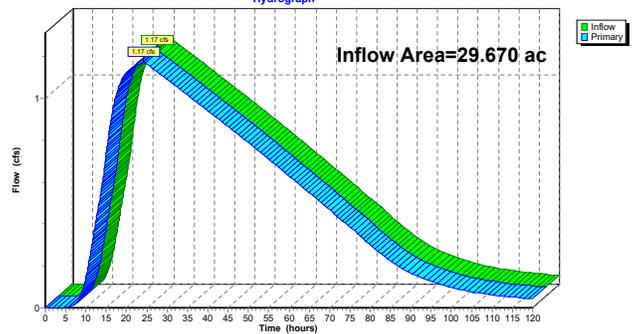
Summary for Link 5L: TOTAL SITE

Inflow Area = 29.670 ac, 0.00% Impervious, Inflow Depth > 1.97" for 002YR-024.00HR event
 Inflow = 1.17 cfs @ 24.21 hrs, Volume= 4.863 af
 Primary = 1.17 cfs @ 24.21 hrs, Volume= 4.863 af, Atten= 0%, Lag= 0.0 min

Primary outflow = Inflow, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs

Link 5L: TOTAL SITE

Hydrograph



Summary for Subcatchment 1S: Subcatchment A

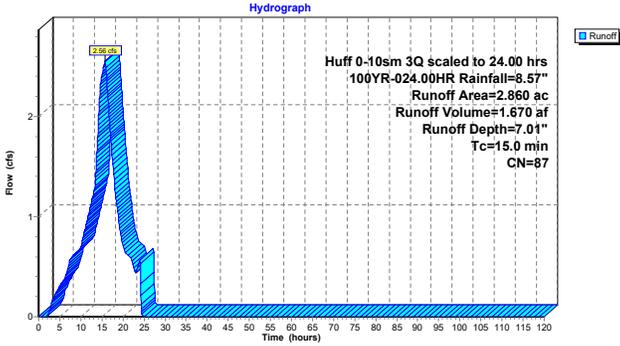
Runoff = 2.56 cfs @ 15.75 hrs, Volume= 1.670 af, Depth= 7.01"
 Routed to Pond 6P : West Basin

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 100YR-024.00HR Rainfall=8.57"

Area (ac)	CN	Description
* 2.860	87	
2.860		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.0					Direct Entry,

Subcatchment 1S: Subcatchment A



Summary for Subcatchment 7S: Subcatchment B

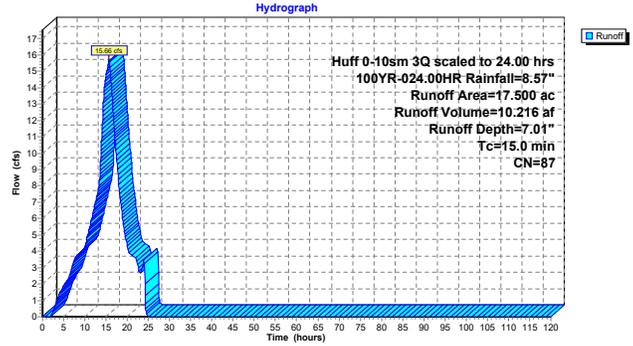
Runoff = 15.66 cfs @ 15.75 hrs, Volume= 10.216 af, Depth= 7.01"
 Routed to Pond 2P : North Basin

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 100YR-024.00HR Rainfall=8.57"

Area (ac)	CN	Description
* 17.500	87	
17.500		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.0					Direct Entry,

Subcatchment 7S: Subcatchment B



Summary for Subcatchment 8S: Subcatchment C

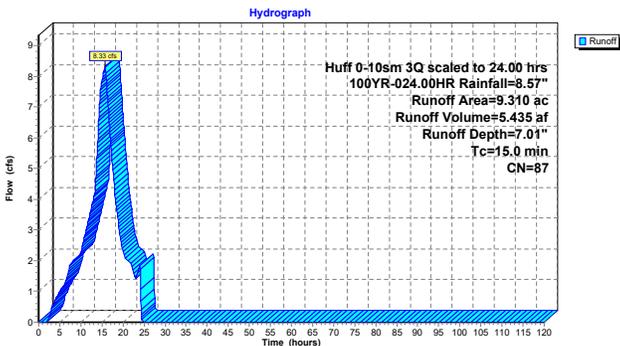
Runoff = 8.33 cfs @ 15.75 hrs, Volume= 5.435 af, Depth= 7.01"
 Routed to Pond 4P : East Basin

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 100YR-024.00HR Rainfall=8.57"

Area (ac)	CN	Description
* 9.310	87	
9.310		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.0					Direct Entry,

Subcatchment 8S: Subcatchment C



Summary for Pond 2P: North Basin

Inflow Area = 20.360 ac, 0.00% Impervious, Inflow Depth = 7.01" for 100YR-024.00HR event
 Inflow = 16.91 cfs @ 15.76 hrs, Volume= 11.886 af
 Outflow = 3.04 cfs @ 24.20 hrs, Volume= 11.502 af, Atten= 82%, Lag= 506.3 min
 Primary = 3.04 cfs @ 24.20 hrs, Volume= 11.502 af
 Routed to Link 5L : TOTAL SITE

Routing by Stor-Ind method, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Peak Elev= 699.61' @ 24.20 hrs Surf.Area= 1.887 ac Storage= 8.973 af

Plug-Flow detention time= 1,760.2 min calculated for 11.497 af (97% of inflow)
 Center-of-Mass det. time= 1,742.6 min (2,649.7 - 907.2)

Volume	Invert	Avail.Storage	Storage	Description
#1	694.00'	11.690 af	Custom Stage Data (Prismatic),	Listed below (Recalc)

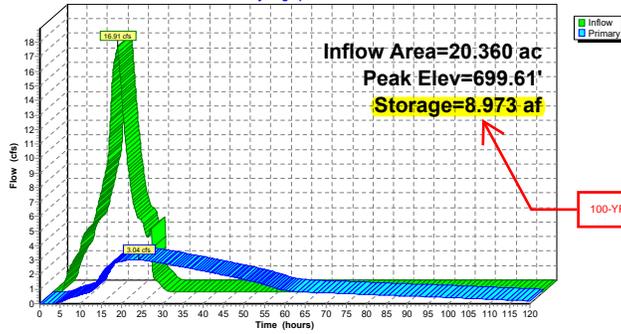
Elevation (feet)	Surf.Area (acres)	Inc.Store (acre-feet)	Cum.Store (acre-feet)
694.00	1.310	0.000	0.000
701.00	2.030	11.690	11.690

Device	Routing	Invert	Outlet Devices
#1	Primary	694.00'	4.8" Vert. Orifice/Grate C= 0.610 Limited to weir flow at low heads
#2	Primary	695.95'	5.7" Vert. Orifice/Grate C= 0.610 Limited to weir flow at low heads

Primary OutFlow Max=3.04 cfs @ 24.20 hrs HW=699.61' (Free Discharge)
 1=Orifice/Grate (Orifice Controls 1.43 cfs @ 11.39 fps)
 2=Orifice/Grate (Orifice Controls 1.61 cfs @ 9.06 fps)

Pond 2P: North Basin

Hydrograph



100-YR 24-HR REQUIRED VOLUME

Summary for Pond 4P: East Basin

Inflow Area = 9.310 ac, 0.00% Impervious, Inflow Depth = 7.01" for 100YR-024.00HR event
 Inflow = 8.33 cfs @ 15.75 hrs, Volume= 5.435 af
 Outflow = 1.38 cfs @ 24.16 hrs, Volume= 5.271 af, Atten= 83%, Lag= 504.6 min
 Primary = 1.38 cfs @ 24.16 hrs, Volume= 5.271 af
 Routed to Link 5L : TOTAL SITE

Routing by Stor-Ind method, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Peak Elev= 690.71' @ 24.16 hrs Surf.Area= 0.987 ac Storage= 4.160 af

Plug-Flow detention time= 1,784.6 min calculated for 5.269 af (97% of inflow)
 Center-of-Mass det. time= 1,769.6 min (2,655.7 - 886.1)

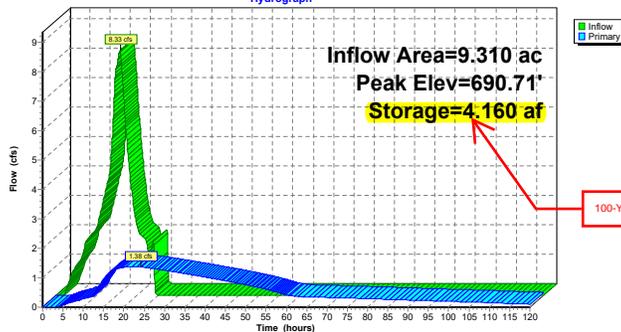
Volume	Invert	Avail.Storage	Storage Description
#1	685.50'	5.493 af	Custom Stage Data (Prismatic) Listed below (Recalc)
Elevation (feet)	Surf.Area (acres)	Inc.Store (acre-feet)	Cum.Store (acre-feet)
685.50	0.610	0.000	0.000
692.00	1.080	5.493	5.493

Device	Routing	Invert	Outlet Devices
#1	Primary	685.50'	3.2" Vert. Orifice/Grate C= 0.610 Limited to weir flow at low heads
#2	Primary	687.36'	4.0" Vert. Orifice/Grate C= 0.610 Limited to weir flow at low heads

Primary OutFlow Max=1.38 cfs @ 24.16 hrs HW=690.71' (Free Discharge)
 1=Orifice/Grate (Orifice Controls 0.62 cfs @ 11.03 fps)
 2=Orifice/Grate (Orifice Controls 0.76 cfs @ 8.73 fps)

Pond 4P: East Basin

Hydrograph



100-YR 24-HR REQUIRED VOLUME

Summary for Pond 6P: West Basin

Inflow Area = 2.860 ac, 0.00% Impervious, Inflow Depth = 7.01" for 100YR-024.00HR event
 Inflow = 2.56 cfs @ 15.75 hrs, Volume= 1.670 af
 Outflow = 1.37 cfs @ 17.91 hrs, Volume= 1.670 af, Atten= 46%, Lag= 129.6 min
 Primary = 1.37 cfs @ 17.91 hrs, Volume= 1.670 af
 Routed to Pond 2P : North Basin

Routing by Stor-Ind method, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Peak Elev= 705.45' @ 17.91 hrs Surf.Area= 0.162 ac Storage= 0.447 af

Plug-Flow detention time= 149.9 min calculated for 1.669 af (100% of inflow)
 Center-of-Mass det. time= 150.1 min (1,036.2 - 886.1)

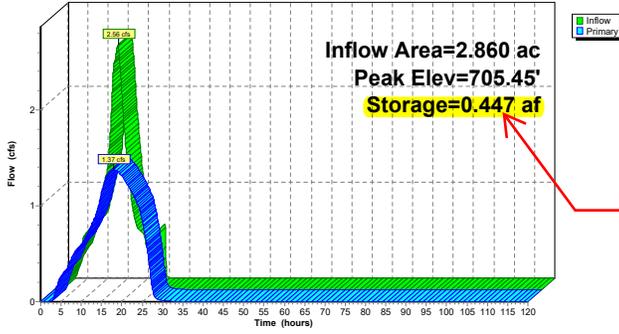
Volume	Invert	Avail.Storage	Storage Description
#1	701.00'	1.628 af	Custom Stage Data (Prismatic) Listed below (Recalc)
Elevation (feet)	Surf.Area (acres)	Inc.Store (acre-feet)	Cum.Store (acre-feet)
701.00	0.039	0.000	0.000
706.00	0.177	0.540	0.540
707.00	2.000	1.088	1.628

Device	Routing	Invert	Outlet Devices
#1	Primary	701.00'	5.0" Vert. Orifice/Grate C= 0.610 Limited to weir flow at low heads

Primary OutFlow Max=1.37 cfs @ 17.91 hrs HW=705.45' (Free Discharge)
 1=Orifice/Grate (Orifice Controls 1.37 cfs @ 10.08 fps)

Pond 6P: West Basin

Hydrograph

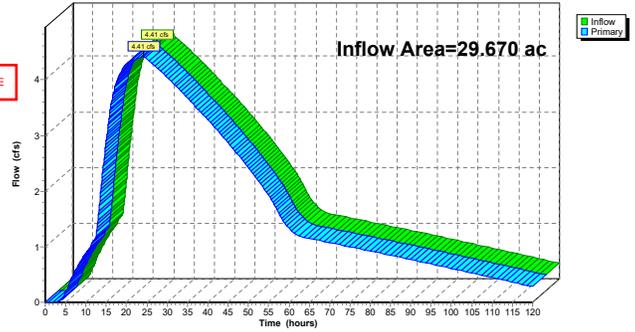


Summary for Link 5L: TOTAL SITE

Inflow Area = 29.670 ac, 0.00% Impervious, Inflow Depth > 6.78" for 100YR-024.00HR event
 Inflow = 4.41 cfs @ 24.19 hrs, Volume= 16.773 af
 Primary = 4.41 cfs @ 24.19 hrs, Volume= 16.773 af, Atten= 0%, Lag= 0.0 min
 Primary outflow = Inflow, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs

Link 5L: TOTAL SITE

Hydrograph



PROPOSED CONDITIONS - THEORETICAL MODEL (DETENTION VOLUME SIZING)

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Multi-Event Tables

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Page 1

Events for Subcatchment 1S: Subcatchment A

Event	Rainfall (inches)	Runoff (cfs)	Volume (acre-feet)	Depth (inches)
002YR-024.00HR	3.34	0.84	0.486	2.04
100YR-024.00HR	8.57	2.56	1.670	7.01

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Multi-Event Tables

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Events for Subcatchment 7S: Subcatchment B

Event	Rainfall (inches)	Runoff (cfs)	Volume (acre-feet)	Depth (inches)
002YR-024.00HR	3.34	5.13	2.974	2.04
100YR-024.00HR	8.57	15.66	10.216	7.01

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Multi-Event Tables

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Events for Subcatchment 8S: Subcatchment C

Event	Rainfall (inches)	Runoff (cfs)	Volume (acre-feet)	Depth (inches)
002YR-024.00HR	3.34	2.73	1.582	2.04
100YR-024.00HR	8.57	8.33	5.435	7.01

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Multi-Event Tables

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Events for Pond 2P: North Basin

Event	Inflow (cfs)	Primary (cfs)	Elevation (feet)	Storage (acre-feet)
002YR-024.00HR	5.75	0.81	695.95	2.755
100YR-024.00HR	16.91	3.04	699.61	8.973

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Multi-Event Tables

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Page 5

Events for Pond 4P: East Basin

Event	Inflow (cfs)	Primary (cfs)	Elevation (feet)	Storage (acre-feet)
002YR-024.00HR	2.73	0.36	687.36	1.263
100YR-024.00HR	8.33	1.38	690.71	4.160

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Multi-Event Tables

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Events for Pond 6P: West Basin

Event	Inflow (cfs)	Primary (cfs)	Elevation (feet)	Storage (acre-feet)
002YR-024.00HR	0.84	0.66	702.20	0.067
100YR-024.00HR	2.56	1.37	705.45	0.447

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Multi-Event Tables

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Page 7

Events for Link 5L: TOTAL SITE

Event	Inflow (cfs)	Primary (cfs)	Elevation (feet)
002YR-024.00HR	1.17	1.17	0.00
100YR-024.00HR	4.41	4.41	0.00

PROPOSED CONDITIONS - ACTUAL MODEL (DETENTION VOLUME SIZING)

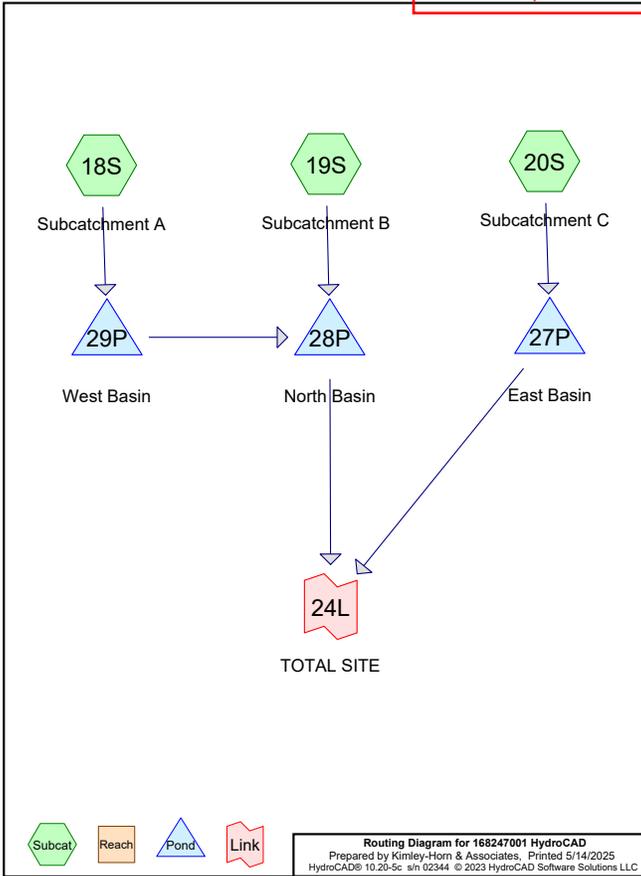
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Page 2

Rainfall Events Listing (selected events)

Event#	Event Name	Storm Type	Curve	Mode	Duration (hours)	B/B	Depth (inches)	AMC
1	002YR-024.00HR	Huff 0-10sm	3Q	Scale	24.00	1	3.34	2
2	100YR-024.00HR	Huff 0-10sm	3Q	Scale	24.00	1	8.57	2



168247001 HydroCAD Huff 0-10sm 3Q scaled to 24.00 hrs 002YR-024.00HR Rainfall=3.34"
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Summary for Subcatchment 18S: Subcatchment A

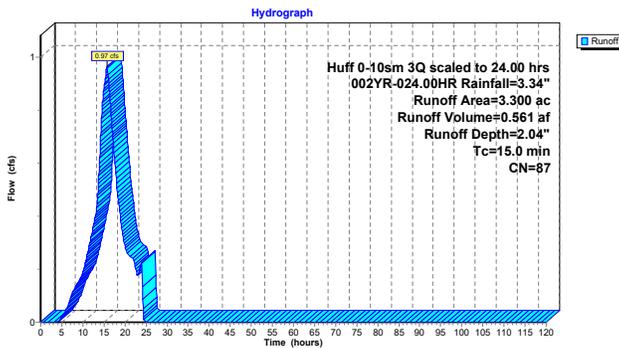
Runoff = 0.97 cfs @ 15.78 hrs, Volume= 0.561 af, Depth= 2.04"
Routed to Pond 29P : West Basin

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
Huff 0-10sm 3Q scaled to 24.00 hrs 002YR-024.00HR Rainfall=3.34"

Area (ac)	CN	Description
* 3.300	87	
3.300		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.0					Direct Entry,

Subcatchment 18S: Subcatchment A



168247001 HydroCAD Huff 0-10sm 3Q scaled to 24.00 hrs 002YR-024.00HR Rainfall=3.34"
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Summary for Subcatchment 19S: Subcatchment B

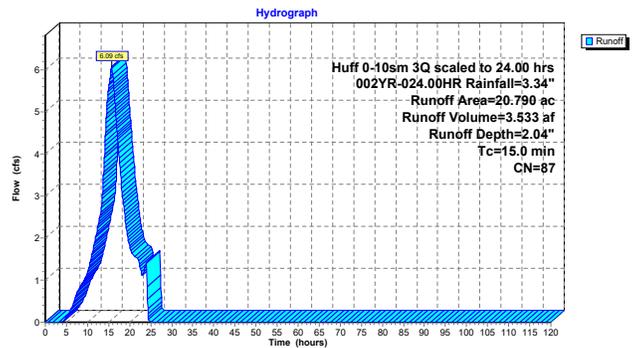
Runoff = 6.09 cfs @ 15.78 hrs, Volume= 3.533 af, Depth= 2.04"
Routed to Pond 28P : North Basin

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
Huff 0-10sm 3Q scaled to 24.00 hrs 002YR-024.00HR Rainfall=3.34"

Area (ac)	CN	Description
* 20.790	87	
20.790		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.0					Direct Entry,

Subcatchment 19S: Subcatchment B



Summary for Subcatchment 20S: Subcatchment C

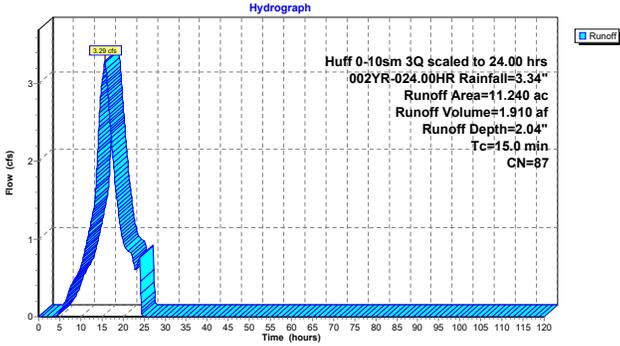
Runoff = 3.29 cfs @ 15.78 hrs, Volume= 1.910 af, Depth= 2.04"
 Routed to Pond 27P : East Basin

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 002YR-024.00HR Rainfall=3.34"

Area (ac)	CN	Description
11.240	87	
11.240		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.0					Direct Entry,

Subcatchment 20S: Subcatchment C



Summary for Pond 27P: East Basin

Inflow Area = 11.240 ac, 0.00% Impervious, Inflow Depth = 2.04" for 002YR-024.00HR event
 Inflow = 3.29 cfs @ 15.78 hrs, Volume= 1.910 af
 Outflow = 0.44 cfs @ 24.21 hrs, Volume= 1.868 af, Atten= 87%, Lag= 505.3 min
 Primary = 0.44 cfs @ 24.21 hrs, Volume= 1.868 af
 Routed to Link 24L : TOTAL SITE

Routing by Stor-Ind method, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Peak Elev= 687.69' @ 24.21 hrs Surf.Area= 0.770 ac Storage= 1.514 af

Plug-Flow detention time= 1,728.5 min calculated for 1.868 af (98% of inflow)
 Center-of-Mass det. time= 1,717.6 min (2,666.9 - 949.3)

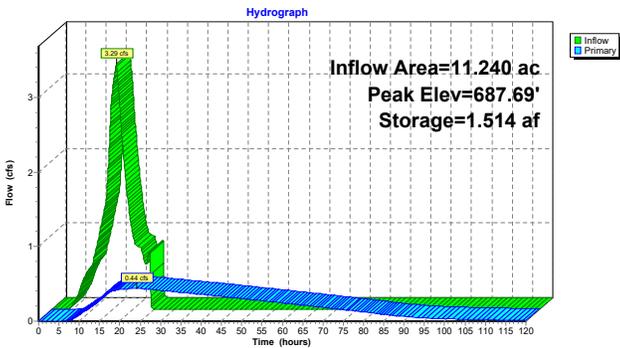
Volume	Invert	Avail.Storage	Storage Description
#1	685.50'	5.502 af	Custom Stage Data (Prismatic) Listed below (Recalc)

Elevation (feet)	Surf.Area (acres)	Inc.Store (acre-feet)	Cum.Store (acre-feet)
685.50	0.610	0.000	0.000
692.00	1.083	5.502	5.502

Device	Routing	Invert	Outlet Devices
#1	Primary	685.50'	3.4" Vert. Orifice/Gate C= 0.610 Limited to weir flow at low heads
#2	Primary	687.69'	4.3" Vert. Orifice/Gate C= 0.610 Limited to weir flow at low heads

Primary OutFlow Max=0.44 cfs @ 24.21 hrs HW=687.69' (Free Discharge)
 1=Orifice/Gate (Orifice Controls 0.44 cfs @ 7.01 fps)
 2=Orifice/Gate (Orifice Controls 0.00 cfs @ 0.22 fps)

Pond 27P: East Basin



Summary for Pond 28P: North Basin

Inflow Area = 24.090 ac, 0.00% Impervious, Inflow Depth = 2.04" for 002YR-024.00HR event
 Inflow = 6.84 cfs @ 15.80 hrs, Volume= 4.094 af
 Outflow = 0.96 cfs @ 24.22 hrs, Volume= 3.953 af, Atten= 86%, Lag= 505.2 min
 Primary = 0.96 cfs @ 24.22 hrs, Volume= 3.953 af
 Routed to Link 24L : TOTAL SITE

Routing by Stor-Ind method, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Peak Elev= 696.28' @ 24.22 hrs Surf.Area= 1.544 ac Storage= 3.253 af

Plug-Flow detention time= 1,729.2 min calculated for 3.952 af (97% of inflow)
 Center-of-Mass det. time= 1,713.9 min (2,670.0 - 956.1)

Volume	Invert	Avail.Storage	Storage Description
#1	694.00'	11.690 af	Custom Stage Data (Prismatic) Listed below (Recalc)

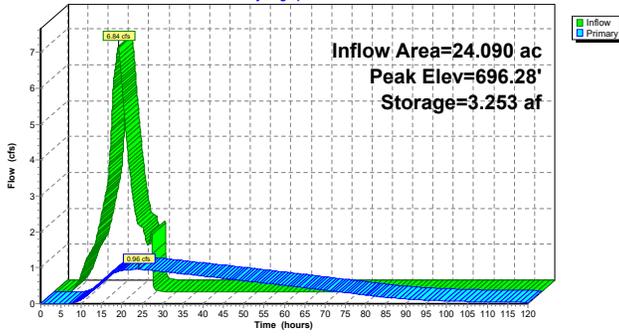
Elevation (feet)	Surf.Area (acres)	Inc.Store (acre-feet)	Cum.Store (acre-feet)
694.00	1.310	0.000	0.000
701.00	2.030	11.690	11.690

Device	Routing	Invert	Outlet Devices
#1	Primary	694.00'	5.0" Vert. Orifice/Gate C= 0.610 Limited to weir flow at low heads
#2	Primary	696.28'	5.2" Vert. Orifice/Gate C= 0.610 Limited to weir flow at low heads

Primary OutFlow Max=0.96 cfs @ 24.22 hrs HW=696.28' (Free Discharge)
 1=Orifice/Gate (Orifice Controls 0.96 cfs @ 7.04 fps)
 2=Orifice/Gate (Controls 0.00 cfs)

Pond 28P: North Basin

Hydrograph



Summary for Pond 29P: West Basin

Inflow Area = 3.300 ac, 0.00% Impervious, Inflow Depth = 2.04" for 002YR-024.00HR event
 Inflow = 0.97 cfs @ 15.78 hrs, Volume= 0.561 af
 Outflow = 0.80 cfs @ 16.67 hrs, Volume= 0.561 af, Atten= 18%, Lag= 52.9 min
 Primary = 0.80 cfs @ 16.67 hrs, Volume= 0.561 af
 Routed to Pond 28P : North Basin

Routing by Stor-Ind method, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Peak Elev= 702.20' @ 16.67 hrs Surf.Area= 0.072 ac Storage= 0.067 af

Plug-Flow detention time= 50.9 min calculated for 0.561 af (100% of inflow)
 Center-of-Mass det. time= 50.1 min (999.4 - 949.3)

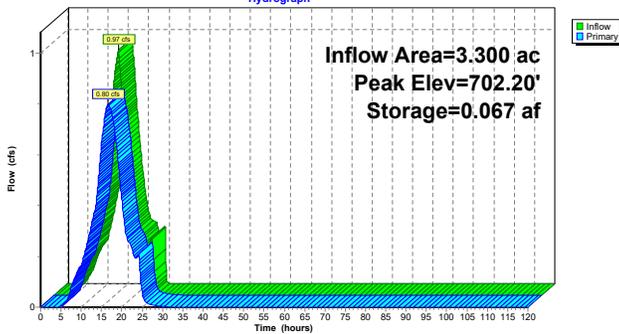
Volume	Invert	Avail.Storage	Storage Description
#1	701.00'	0.878 af	Custom Stage Data (Prismatic) Listed below (Recalc)
Elevation (feet)	Surf.Area (acres)	Inc.Store (acre-feet)	Cum.Store (acre-feet)
701.00	0.039	0.000	0.000
706.00	0.177	0.540	0.540

Device	Routing	Invert	Outlet Devices
#1	Primary	701.00'	5.5" Vert. Orifice/Grate C= 0.610 Limited to weir flow at low heads

Primary OutFlow Max=0.80 cfs @ 16.67 hrs HW=702.20' (Free Discharge)
 1=Orifice/Grate (Orifice Controls 0.80 cfs @ 4.83 fps)

Pond 29P: West Basin

Hydrograph



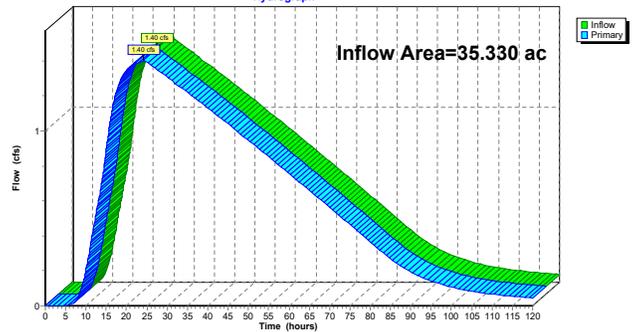
Summary for Link 24L: TOTAL SITE

Inflow Area = 35.330 ac, 0.00% Impervious, Inflow Depth > 1.98" for 002YR-024.00HR event
 Inflow = 1.40 cfs @ 24.21 hrs, Volume= 5.821 af
 Primary = 1.40 cfs @ 24.21 hrs, Volume= 5.821 af, Atten= 0%, Lag= 0.0 min

Primary outflow = Inflow, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs

Link 24L: TOTAL SITE

Hydrograph



Summary for Subcatchment 18S: Subcatchment A

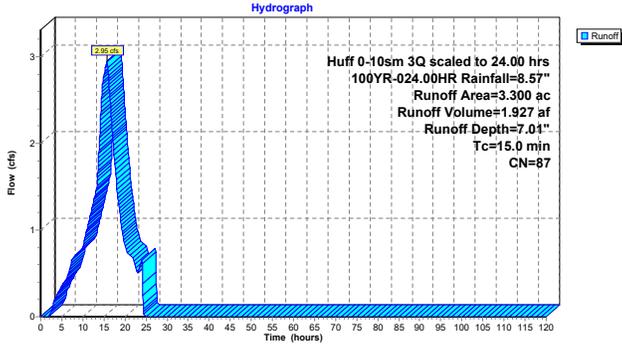
Runoff = 2.95 cfs @ 15.75 hrs, Volume= 1.927 af, Depth= 7.01"
 Routed to Pond 29P : West Basin

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 100YR-024.00HR Rainfall=8.57"

Area (ac)	CN	Description
* 3.300	87	
3.300		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.0					Direct Entry,

Subcatchment 18S: Subcatchment A



Summary for Subcatchment 19S: Subcatchment B

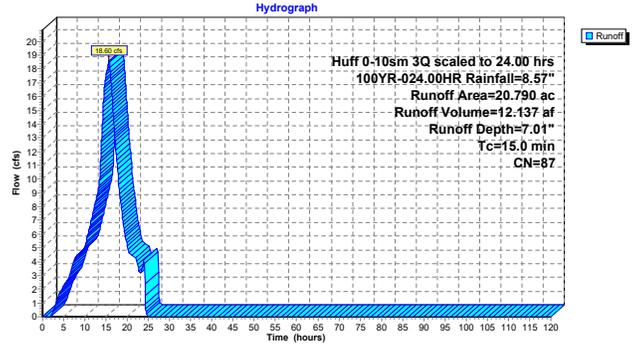
Runoff = 18.60 cfs @ 15.75 hrs, Volume= 12.137 af, Depth= 7.01"
 Routed to Pond 28P : North Basin

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 100YR-024.00HR Rainfall=8.57"

Area (ac)	CN	Description
* 20.790	87	
20.790		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.0					Direct Entry,

Subcatchment 19S: Subcatchment B



Summary for Subcatchment 20S: Subcatchment C

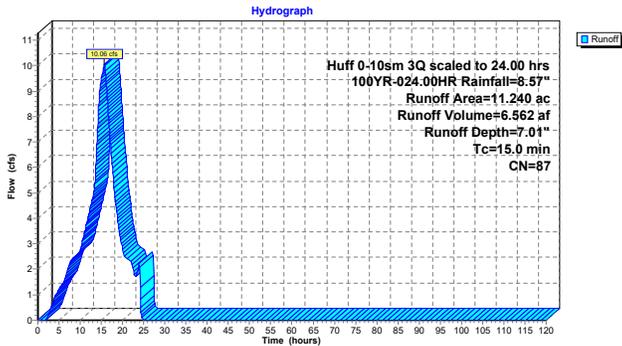
Runoff = 10.06 cfs @ 15.75 hrs, Volume= 6.562 af, Depth= 7.01"
 Routed to Pond 27P : East Basin

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Huff 0-10sm 3Q scaled to 24.00 hrs 100YR-024.00HR Rainfall=8.57"

Area (ac)	CN	Description
* 11.240	87	
11.240		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.0					Direct Entry,

Subcatchment 20S: Subcatchment C



Summary for Pond 27P: East Basin

Inflow Area = 11.240 ac, 0.00% Impervious, Inflow Depth = 7.01" for 100YR-024.00HR event
 Inflow = 10.06 cfs @ 15.75 hrs, Volume= 6.562 af
 Outflow = 1.69 cfs @ 24.16 hrs, Volume= 6.398 af, Atten= 83%, Lag= 504.4 min
 Primary = 1.69 cfs @ 24.16 hrs, Volume= 6.398 af
 Routed to Link 24L : TOTAL SITE

Routing by Stor-Ind method, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Peak Elev= 691.52' @ 24.16 hrs Surf.Area= 1.048 ac Storage= 4.989 af

Plug-Flow detention time= 1,759.0 min calculated for 6.398 af (97% of inflow)
 Center-of-Mass det. time= 1,745.1 min (2,631.2 - 886.1)

Volume	Invert	Avail.Storage	Storage	Description
#1	685.50'	5.502 af	Custom Stage Data (Prismatic),	Listed below (Recalc)

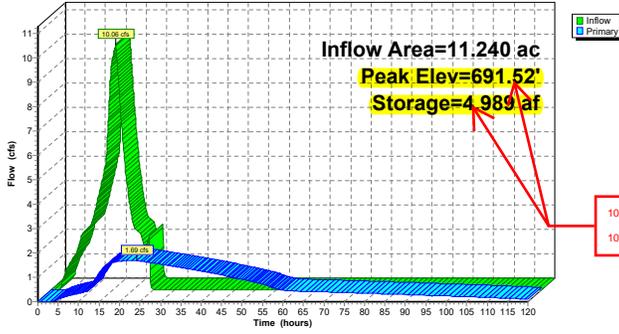
Elevation (feet)	Surf.Area (acres)	Inc.Store (acre-feet)	Cum.Store (acre-feet)
685.50	0.610	0.000	0.000
692.00	1.083	5.502	5.502

Device	Routing	Invert	Outlet Devices
#1	Primary	685.50'	3.4" Vert. Orifice/Grate C= 0.610 Limited to weir flow at low heads
#2	Primary	687.69'	4.3" Vert. Orifice/Grate C= 0.610 Limited to weir flow at low heads

Primary OutFlow Max=1.69 cfs @ 24.16 hrs HW=691.52' (Free Discharge)
 1=Orifice/Grate (Orifice Controls 0.75 cfs @ 11.87 fps)
 2=Orifice/Grate (Orifice Controls 0.94 cfs @ 9.35 fps)

Pond 27P: East Basin

Hydrograph



Summary for Pond 28P: North Basin

Inflow Area = 24.090 ac, 0.00% Impervious, Inflow Depth = 7.01" for 100YR-024.00HR event
 Inflow = 20.14 cfs @ 15.76 hrs, Volume= 14.064 af
 Outflow = 3.17 cfs @ 24.22 hrs, Volume= 13.521 af, Atten= 84%, Lag= 507.4 min
 Primary = 3.17 cfs @ 24.22 hrs, Volume= 13.521 af
 Routed to Link 24L : TOTAL SITE

Routing by Stor-Ind method, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Peak Elev= 700.66' @ 24.22 hrs Surf.Area= 1.995 ac Storage= 11.001 af

Plug-Flow detention time= 1,919.8 min calculated for 13.521 af (96% of inflow)
 Center-of-Mass det. time= 1,897.9 min (2,801.2 - 903.4)

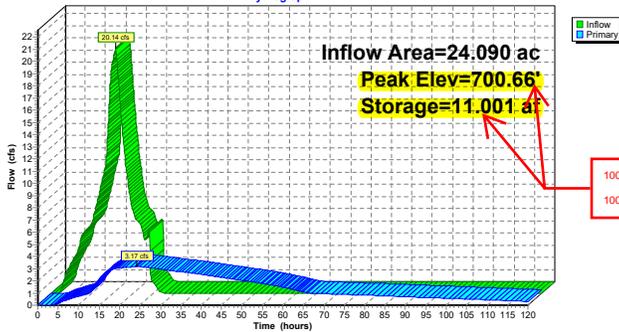
Volume	Invert	Avail.Storage	Storage Description
#1	694.00'	11.690 af	Custom Stage Data (Prismatic), Listed below (Recalc)
Elevation (feet)	Surf.Area (acres)	Inc.Store (acre-feet)	Cum.Store (acre-feet)
694.00	1.310	0.000	0.000
701.00	2.030	11.690	11.690

Device	Routing	Invert	Outlet Devices
#1	Primary	694.00'	5.0" Vert. Orifice/Grate C= 0.610 Limited to weir flow at low heads
#2	Primary	696.28'	5.2" Vert. Orifice/Grate C= 0.610 Limited to weir flow at low heads

Primary OutFlow Max=3.17 cfs @ 24.22 hrs HW=700.66' (Free Discharge)
 1=Orifice/Grate (Orifice Controls 1.70 cfs @ 12.43 fps)
 2=Orifice/Grate (Orifice Controls 1.47 cfs @ 9.99 fps)

Pond 28P: North Basin

Hydrograph



Summary for Pond 29P: West Basin

Inflow Area = 3.300 ac, 0.00% Impervious, Inflow Depth = 7.01" for 100YR-024.00HR event
 Inflow = 2.95 cfs @ 15.75 hrs, Volume= 1.927 af
 Outflow = 1.68 cfs @ 17.74 hrs, Volume= 1.927 af, Atten= 43%, Lag= 119.5 min
 Primary = 1.68 cfs @ 17.74 hrs, Volume= 1.927 af
 Routed to Pond 28P : North Basin

Routing by Stor-Ind method, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs
 Peak Elev= 705.57' @ 17.74 hrs Surf.Area= 0.165 ac Storage= 0.466 af

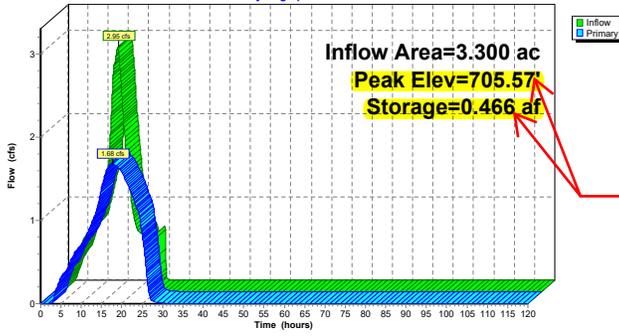
Plug-Flow detention time= 127.0 min calculated for 1.927 af (100% of inflow)
 Center-of-Mass det. time= 126.3 min (1,012.4 - 886.1)

Volume	Invert	Avail.Storage	Storage Description
#1	701.00'	0.878 af	Custom Stage Data (Prismatic), Listed below (Recalc)
Elevation (feet)	Surf.Area (acres)	Inc.Store (acre-feet)	Cum.Store (acre-feet)
701.00	0.039	0.000	0.000
706.00	0.177	0.540	0.540

Device	Routing	Invert	Outlet Devices
#1	Primary	701.00'	5.5" Vert. Orifice/Grate C= 0.610 Limited to weir flow at low heads

Primary OutFlow Max=1.68 cfs @ 17.74 hrs HW=705.57' (Free Discharge)
 1=Orifice/Grate (Orifice Controls 1.68 cfs @ 10.20 fps)

Pond 29P: West Basin
 Hydrograph

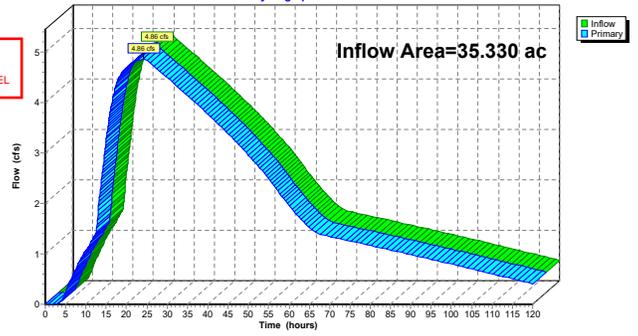


100-YR 24-HR ACTUAL VOLUME
 100-YR 24-HR HIGH WATER LEVEL

Summary for Link 24L: TOTAL SITE
 Hydrograph

Inflow Area = 35.330 ac, 0.00% Impervious, Inflow Depth > 6.77" for 100YR-024.00HR event
 Inflow = 4.86 cfs @ 24.19 hrs, Volume= 19.918 af
 Primary = 4.86 cfs @ 24.19 hrs, Volume= 19.918 af, Atten= 0%, Lag= 0.0 min
 Primary outflow = Inflow, Time Span= 0.00-120.00 hrs, dt= 0.05 hrs

Link 24L: TOTAL SITE
 Hydrograph



PROPOSED CONDITIONS - ACTUAL MODEL (DETENTION VOLUME SIZING)

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Page 1

Rainfall Events Listing

Event#	Event Name	Storm Type	Curve	Mode	Duration (hours)	B/B	Depth (inches)	AMC
1	002YR-024.00HR	Huff 0-10sm	3Q	Scale	24.00	1	3.34	2
2	100YR-001.00HR	Huff 0-10sm	1Q	Scale	1.00	1	4.03	2
3	100YR-002.00HR	Huff 0-10sm	1Q	Scale	2.00	1	4.97	2
4	100YR-003.00HR	Huff 0-10sm	1Q	Scale	3.00	1	5.49	2
5	100YR-006.00HR	Huff 0-10sm	1Q	Scale	6.00	1	6.43	2
6	100YR-012.00HR	Huff 0-10sm	2Q	Scale	12.00	1	7.46	2
7	100YR-018.00HR	Huff 0-10sm	3Q	Scale	18.00	1	8.06	2
8	100YR-024.00HR	Huff 0-10sm	3Q	Scale	24.00	1	8.57	2
9	100YR-048.00HR	Huff 0-10sm	4Q	Scale	48.00	1	9.28	2
10	100YR-072.00HR	Huff 0-10sm	4Q	Scale	72.00	1	9.85	2
11	100YR-120.00HR	Huff 0-10sm	4Q	Scale	120.00	1	10.66	2

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Page 2

Events for Subcatchment 18S: Subcatchment A

Event	Rainfall (inches)	Runoff (cfs)	Volume (acre-feet)	Depth (inches)
002YR-024.00HR	3.34	0.97	0.561	2.04
100YR-001.00HR	4.03	15.68	0.733	2.66
100YR-002.00HR	4.97	12.66	0.973	3.54
100YR-003.00HR	5.49	10.79	1.108	4.03
100YR-006.00HR	6.43	7.87	1.356	4.93
100YR-012.00HR	7.46	4.42	1.629	5.92
100YR-018.00HR	8.06	3.67	1.790	6.51
100YR-024.00HR	8.57	2.95	1.927	7.01
100YR-048.00HR	9.28	1.66	2.118	7.70
100YR-072.00HR	9.85	1.18	2.271	8.26
100YR-120.00HR	10.66	0.77	2.482	9.03

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Events for Subcatchment 19S: Subcatchment B

Event	Rainfall (inches)	Runoff (cfs)	Volume (acre-feet)	Depth (inches)
002YR-024.00HR	3.34	6.09	3.533	2.04
100YR-001.00HR	4.03	98.77	4.616	2.66
100YR-002.00HR	4.97	79.76	6.131	3.54
100YR-003.00HR	5.49	67.99	6.983	4.03
100YR-006.00HR	6.43	49.57	8.541	4.93
100YR-012.00HR	7.46	27.86	10.265	5.92
100YR-018.00HR	8.06	23.12	11.275	6.51
100YR-024.00HR	8.57	18.60	12.137	7.01
100YR-048.00HR	9.28	10.48	13.340	7.70
100YR-072.00HR	9.85	7.46	14.309	8.26
100YR-120.00HR	10.66	4.88	15.636	9.03

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Multi-Event Tables

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Events for Subcatchment 20S: Subcatchment C

Event	Rainfall (inches)	Runoff (cfs)	Volume (acre-feet)	Depth (inches)
002YR-024.00HR	3.34	3.29	1.910	2.04
100YR-001.00HR	4.03	53.40	2.495	2.66
100YR-002.00HR	4.97	43.12	3.315	3.54
100YR-003.00HR	5.49	36.76	3.776	4.03
100YR-006.00HR	6.43	26.80	4.617	4.93
100YR-012.00HR	7.46	15.06	5.550	5.92
100YR-018.00HR	8.06	12.50	6.096	6.51
100YR-024.00HR	8.57	10.06	6.562	7.01
100YR-048.00HR	9.28	5.67	7.212	7.70
100YR-072.00HR	9.85	4.03	7.736	8.26
100YR-120.00HR	10.66	2.64	8.454	9.03

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Multi-Event Tables

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Events for Pond 27P: East Basin

Event	Inflow (cfs)	Primary (cfs)	Elevation (feet)	Storage (acre-feet)
002YR-024.00HR	3.29	0.44	687.69	1.514
100YR-001.00HR	53.40	1.03	688.81	2.421
100YR-002.00HR	43.12	1.28	689.64	3.153
100YR-003.00HR	36.76	1.38	690.04	3.521
100YR-006.00HR	26.80	1.50	690.60	4.061
100YR-012.00HR	15.06	1.62	691.15	4.605
100YR-018.00HR	12.50	1.68	691.48	4.947
100YR-024.00HR	10.06	1.69	691.52	4.989
100YR-048.00HR	5.67	1.70	691.54	5.013
100YR-072.00HR	4.03	1.61	691.13	4.585
100YR-120.00HR	2.64	1.44	690.31	3.775

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Events for Pond 28P: North Basin

Event	Inflow (cfs)	Primary (cfs)	Elevation (feet)	Storage (acre-feet)
002YR-024.00HR	6.84	0.96	696.28	3.253
100YR-001.00HR	100.01	1.74	697.16	4.660
100YR-002.00HR	80.99	2.21	698.05	6.149
100YR-003.00HR	69.14	2.41	698.52	6.970
100YR-006.00HR	50.74	2.71	699.30	8.381
100YR-012.00HR	29.40	2.98	700.07	9.837
100YR-018.00HR	24.73	3.11	700.48	10.650
100YR-024.00HR	20.14	3.17	700.66	11.001
100YR-048.00HR	11.63	3.17	700.67	11.034
100YR-072.00HR	8.41	3.06	700.33	10.349
100YR-120.00HR	5.58	2.79	699.50	8.765

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Events for Pond 29P: West Basin

Event	Inflow (cfs)	Primary (cfs)	Elevation (feet)	Storage (acre-feet)
002YR-024.00HR	0.97	0.80	702.20	0.067
100YR-001.00HR	15.68	1.81	706.24	0.591
100YR-002.00HR	12.66	1.87	706.59	0.701
100YR-003.00HR	10.79	1.88	706.62	0.712
100YR-006.00HR	7.87	1.86	706.54	0.681
100YR-012.00HR	4.42	1.84	706.40	0.636
100YR-018.00HR	3.67	1.78	706.08	0.556
100YR-024.00HR	2.95	1.68	705.57	0.466
100YR-048.00HR	1.66	1.33	703.96	0.236
100YR-072.00HR	1.18	1.07	703.00	0.133
100YR-120.00HR	0.77	0.75	702.09	0.059

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Multi-Event Tables

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Page 8

Events for Link 24L: TOTAL SITE

Event	Inflow (cfs)	Primary (cfs)	Elevation (feet)
002YR-024.00HR	1.40	1.40	0.00
100YR-001.00HR	2.76	2.76	0.00
100YR-002.00HR	3.48	3.48	0.00
100YR-003.00HR	3.79	3.79	0.00
100YR-006.00HR	4.22	4.22	0.00
100YR-012.00HR	4.60	4.60	0.00
100YR-018.00HR	4.80	4.80	0.00
100YR-024.00HR	4.86	4.86	0.00
100YR-048.00HR	4.87	4.87	0.00
100YR-072.00HR	4.68	4.68	0.00
100YR-120.00HR	4.23	4.23	0.00

BASIN 1: WEIR OVERFLOW - PEAK DISCHARGE EVENT

Worksheet for Overflow Weir_Basin 1

Project Description	
Solve For	Headwater Elevation
Input Data	
Discharge	100.01 cfs
Crest Elevation	0.00 ft
Tailwater Elevation	0.00 ft
Crest Surface Type	Gravel
Crest Breadth	5.00 ft
Crest Length	200.0 ft
Results	
Headwater Elevation	0.33 ft
Headwater Height Above Crest	0.33 ft
Tailwater Height Above Crest	0.00 ft
Weir Coefficient	2.64 ft ^(1/2) /s
Submergence Factor	1.000
Adjusted Weir Coefficient	2.64 ft ^(1/2) /s
Flow Area	66.0 ft ²
Velocity	1.51 ft/s
Wetted Perimeter	200.7 ft
Top Width	200.00 ft

BASIN 2: WEIR OVERFLOW - PEAK DISCHARGE EVENT

Worksheet for Overflow Weir_Basin 2

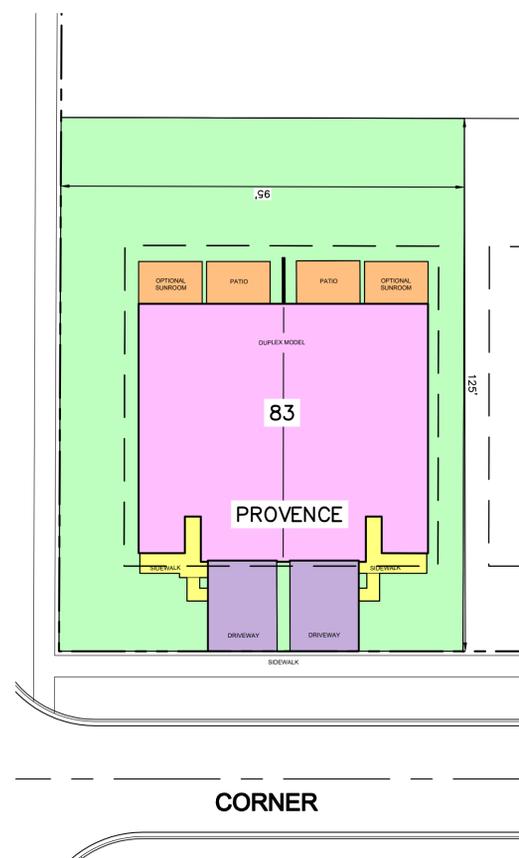
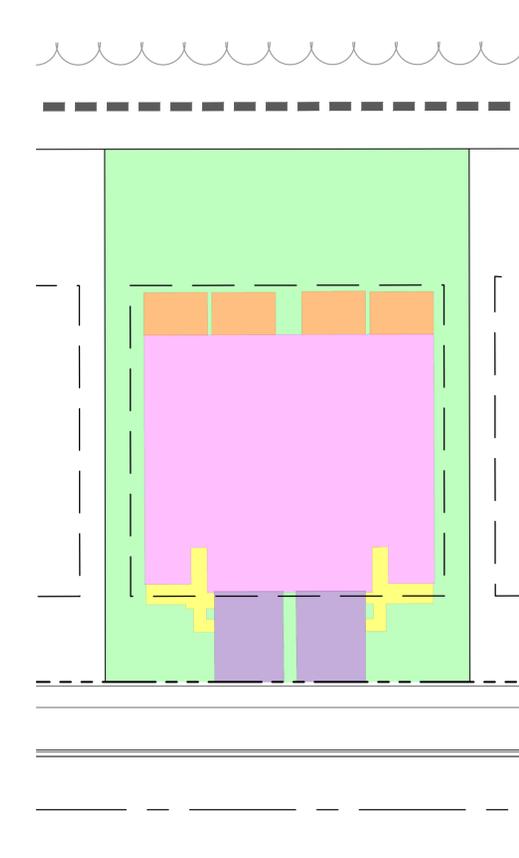
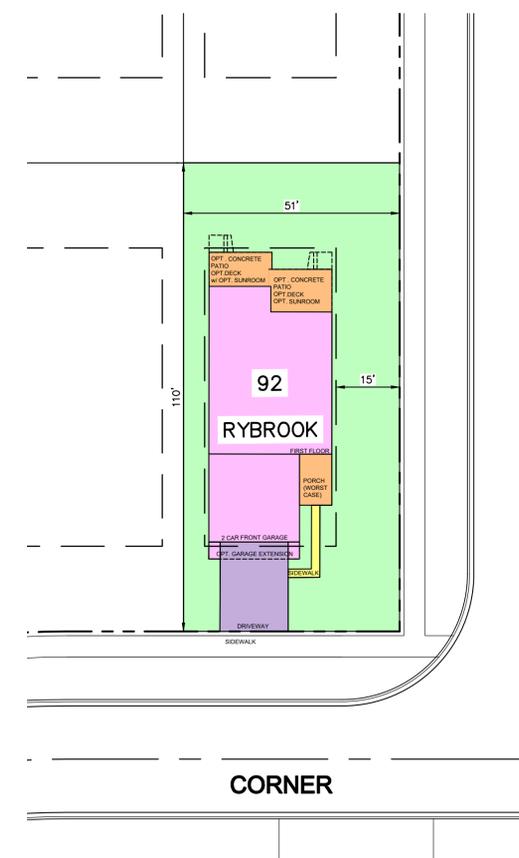
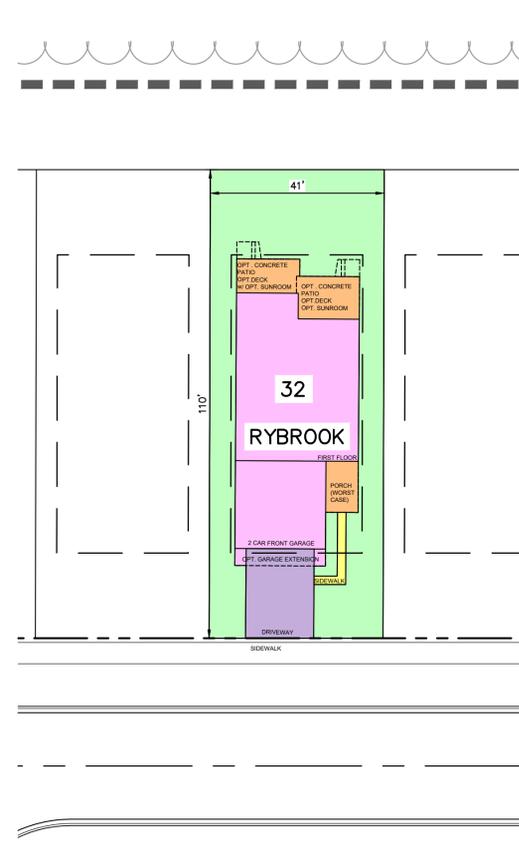
Project Description	
Solve For	Headwater Elevation
Input Data	
Discharge	53.40 cfs
Crest Elevation	0.00 ft
Tailwater Elevation	0.00 ft
Crest Surface Type	Gravel
Crest Breadth	5.00 ft
Crest Length	125.0 ft
Results	
Headwater Elevation	0.30 ft
Headwater Height Above Crest	0.30 ft
Tailwater Height Above Crest	0.00 ft
Weir Coefficient	2.62 ft ^(1/2) /s
Submergence Factor	1.000
Adjusted Weir Coefficient	2.62 ft ^(1/2) /s
Flow Area	37.3 ft ²
Velocity	1.43 ft/s
Wetted Perimeter	125.6 ft
Top Width	125.00 ft

BASIN 3: WEIR OVERFLOW - PEAK DISCHARGE EVENT

Worksheet for Overflow Weir_Basin 3

Project Description	
Solve For	Headwater Elevation
Input Data	
Discharge	15.68 cfs
Crest Elevation	0.00 ft
Tailwater Elevation	0.00 ft
Crest Surface Type	Gravel
Crest Breadth	5.00 ft
Crest Length	25.0 ft
Results	
Headwater Elevation	0.38 ft
Headwater Height Above Crest	0.38 ft
Tailwater Height Above Crest	0.00 ft
Weir Coefficient	2.65 ft ^(1/2) /s
Submergence Factor	1.000
Adjusted Weir Coefficient	2.65 ft ^(1/2) /s
Flow Area	9.6 ft ²
Velocity	1.64 ft/s
Wetted Perimeter	25.8 ft
Top Width	25.00 ft

Drawing name: K:\CHS_DEV\168247001_Pulte_Libertyville_A_V2_Design\CAD\Exhibits\168247001-Lot Coverage Exhibit.dwg EXH May 19, 2025 12:10pm by: Ian.Science
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SINGLE FAMILY (RYBROOK) LOTS
1" = 20'

DUPLEX (PROVENCE) LOTS
1" = 20'

SINGLE-FAMILY (RYBROOK) INTERIOR LOT 32
(TOTAL LOT: 4,510 S.F.)

BUILDING STRUCTURE	1,582 S.F.
DRIVEWAY	273 S.F.
PORCH/PATIO	353 S.F.
WALK	45 S.F.
TOTAL IMPERVIOUS	2,253 S.F.
TOTAL OPEN SPACE	2,257 S.F. (50.04%)
BUILDING COVERAGE	35.10%
LOT COVERAGE	49.90%
LOT COVERAGE REQUESTED VARIANCE:	55%

SINGLE-FAMILY (RYBROOK) CORNER LOT 92
(TOTAL LOT: 5,610 S.F.)

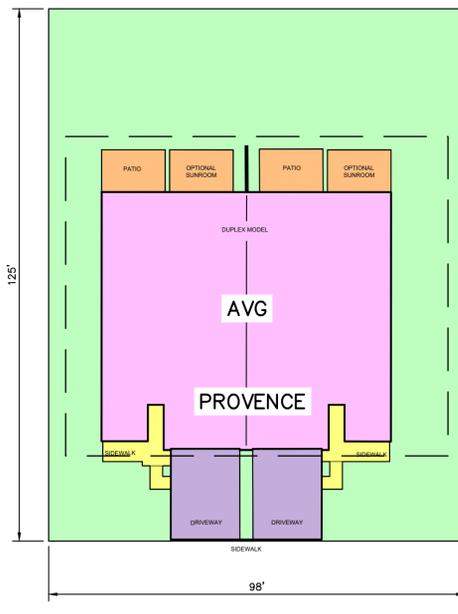
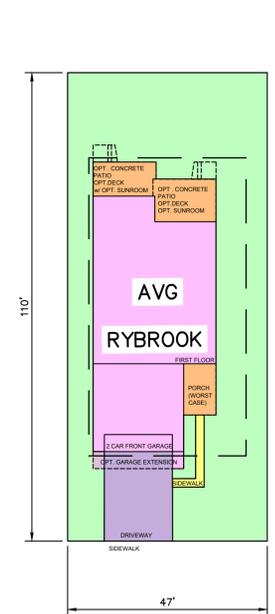
BUILDING STRUCTURE	1,582 S.F.
DRIVEWAY	273 S.F.
PORCH/PATIO	353 S.F.
WALK	45 S.F.
TOTAL IMPERVIOUS	2,253 S.F.
TOTAL OPEN SPACE	3,356 S.F. (59.8%)
BUILDING COVERAGE	28.20%
LOT COVERAGE	40.20%
LOT COVERAGE REQUESTED VARIANCE:	45%

DUPLEX FAMILY (PROVENCE) INTERIOR LOT 13
(TOTAL LOT: 10,750 S.F.)

BUILDING STRUCTURE	4,050 S.F.
DRIVEWAY	692 S.F.
PORCH/PATIO	598 S.F.
SIDEWALK	264 S.F.
TOTAL IMPERVIOUS	5,604 S.F.
TOTAL OPEN SPACE	5,146 S.F. (48%)
BUILDING COVERAGE	37.70%
LOT COVERAGE	52.10%
LOT COVERAGE REQUESTED VARIANCE:	55%

DUPLEX FAMILY (PROVENCE) CORNER LOT 83
(TOTAL LOT: 11,923 S.F.)

BUILDING STRUCTURE	4,050 S.F.
DRIVEWAY	692 S.F.
PORCH/PATIO	598 S.F.
SIDEWALK	264 S.F.
TOTAL IMPERVIOUS	5,604 S.F.
TOTAL OPEN SPACE	6,319 S.F. (53%)
BUILDING COVERAGE	33.99%
LOT COVERAGE	47.00%
LOT COVERAGE REQUESTED VARIANCE:	50%



SINGLE-FAMILY AVERAGE LOT
(TOTAL LOT: 5,179 S.F.)

BUILDING STRUCTURE	1,582 S.F.
DRIVEWAY	273 S.F.
PORCH/PATIO	353 S.F.
SIDEWALK	45 S.F.
TOTAL IMPERVIOUS	2,253 S.F.
TOTAL OPEN SPACE	2,936 S.F. (56.7%)
BUILDING COVERAGE	30.50%
LOT COVERAGE	43.50%

DUPLEX FAMILY AVERAGE LOT
(TOTAL LOT: 12,275 S.F.)

BUILDING STRUCTURE	4,009 S.F.
DRIVEWAY	692 S.F.
PORCH/PATIO	598 S.F.
SIDEWALK	264 S.F.
TOTAL IMPERVIOUS	5,563 S.F.
TOTAL OPEN SPACE	6,707 S.F. (54.6%)
BUILDING COVERAGE	32.70%
LOT COVERAGE	45.30%

LEGEND

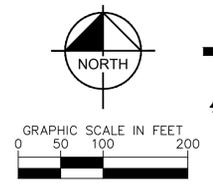
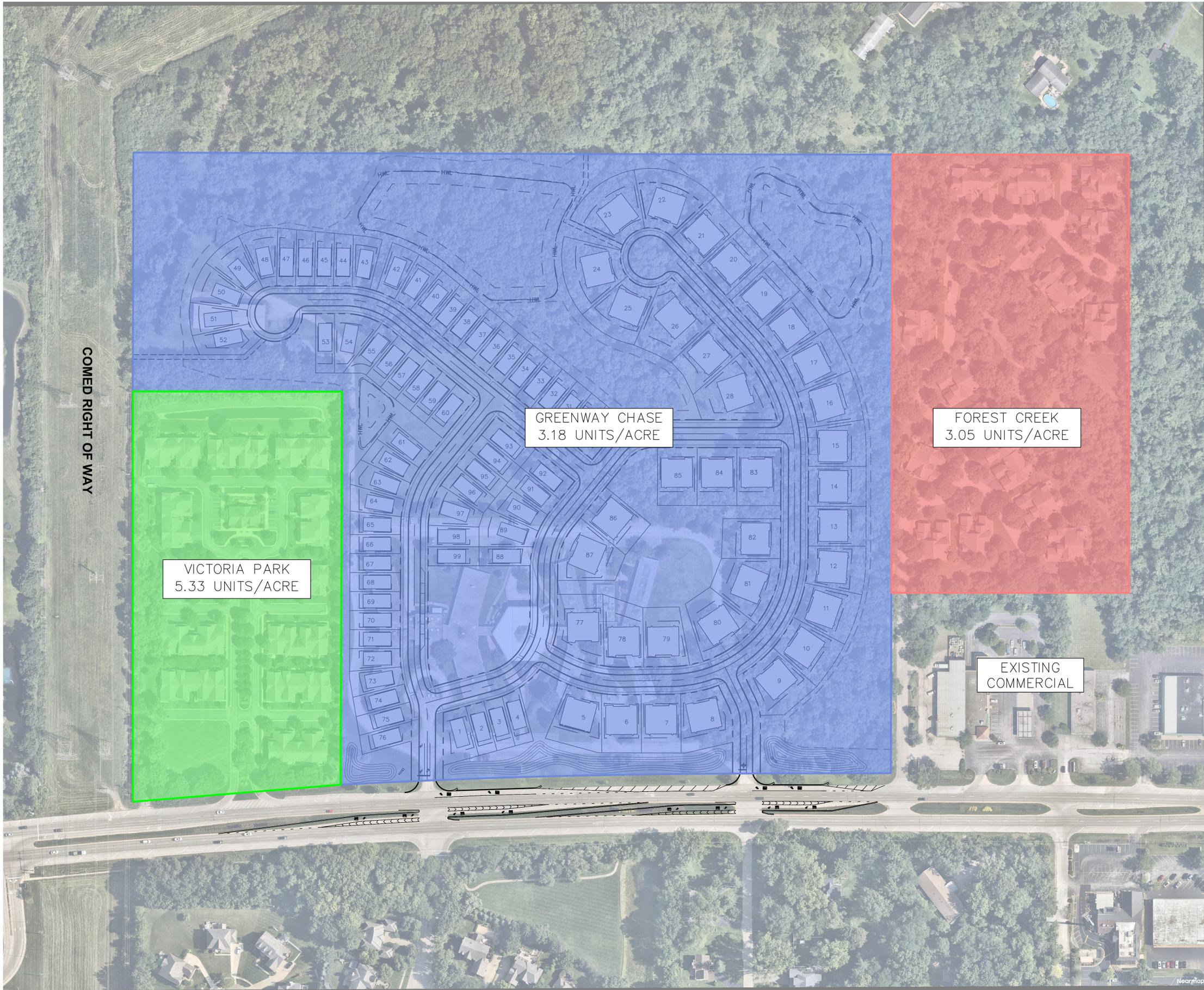
	DRIVEWAY
	PORCH/PATIO
	SIDEWALK
	BUILDING STRUCTURE
	GREEN SPACE

Greenway Chase		Lot Coverage		Building Coverage	
		Maximum Allowed	Requested	Maximum Allowed	Requested
Detached Single Family	Interior	45%	55%	40%	
	Corner	40%	45%	40%	
Attached (Duplexes)	Interior	50%	55%	35%	38%
	Corner	45%	50%	40%	

SINGLE FAMILY & DUPLEX AVERAGE LOTS
1" = 20'

INS	05/19/25	VILLAGE RESUBMITTAL					
RNM	03/28/25	VILLAGE RESUBMITTAL					
DC	03/07/25	ARC RE-SUBMITTAL					
DATE	DATE	DATE	DATE	DATE	DATE	DATE	DATE
BY	BY	BY	BY	BY	BY	BY	BY
Kimley»Horn							
© 2025 KIMLEY-HORN AND ASSOCIATES, INC. 570 LAKE COOK ROAD, SUITE 200 LIBERTYVILLE, IL 60048 PHONE: 847-260-7804 WWW.KIMLEY-HORN.COM							
SCALE:	DESIGNED BY: INS	DRAWN BY: KTRM	CHECKED BY: RNM				
PULTE HOME COMPANY, LLC							
LOT COVERAGE							
GREENWAY CHASE 610 PETERSON ROAD LIBERTYVILLE, IL 60048							
ORIGINAL ISSUE: 01/03/2025 KHA PROJECT NO. 168247001 SHEET NUMBER EXH.							

Drawing name: K:\GIS_DEV\168247001_Pulte_Libertyville_IL_V2_Design\CAD\Exhibits\168247001-Density Exhibit.dwg EKH, May 16, 2025 1:12pm by: Ian.Spence
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1-800-892-0123

NO.	REVISIONS	DATE	BY
1	INS	05/19/25	
2	RNM	03/28/25	
3	DC	03/07/25	

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SCALE: AS NOTED
 DESIGNED BY: INS
 DRAWN BY: KTRM
 CHECKED BY: RNM

PULTE HOME COMPANY, LLC
DENSITY EXHIBIT

GREENWAY CHASE
 610 PETERSON ROAD
 LIBERTYVILLE, IL 60048

ORIGINAL ISSUE: 01/03/2025
 KHA PROJECT NO. 168247001
 SHEET NUMBER
EXH.

Drawing name: K:\GIS_DEVELOPMENT\168247001_Pulte_Libertyville_IL_V2_Design\CAD\PlanSheets\C2.0 SITE PLAN.dwg C2.0 May 14, 2025 4:59pm by: Ian.Spence
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 1-800-892-0123

NORTH
 GRAPHIC SCALE IN FEET
 0 40 80 160

- ### GENERAL NOTES
- ALL DIMENSIONS REFER TO THE BACK OF CURB UNLESS OTHERWISE NOTED.
 - BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 - REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.
 - REFER TO LANDSCAPE ARCHITECT PLANS FOR MONUMENT SIGN DETAILS.

PROPERTY SUMMARY TABLE

SITE AREA	= 42.11 ACRES
R/W DEDICATION	= 6.55 ACRES
LOT AREA (CUMULATIVE)	= 17.69 ACRES
PARK DISTRICT DEDICATION	= 1.14 ACRES
PRIVATE AMENITY SPACE	= 1.41 ACRES
OPEN SPACE/DETENTION	= 2.92 ACRES
OPEN SPACE/UNDISTURBED AREA	= 12.40 ACRES
TOTAL LOTS PROVIDED	= 99 LOTS
UNIT COUNT	= 134 UNITS

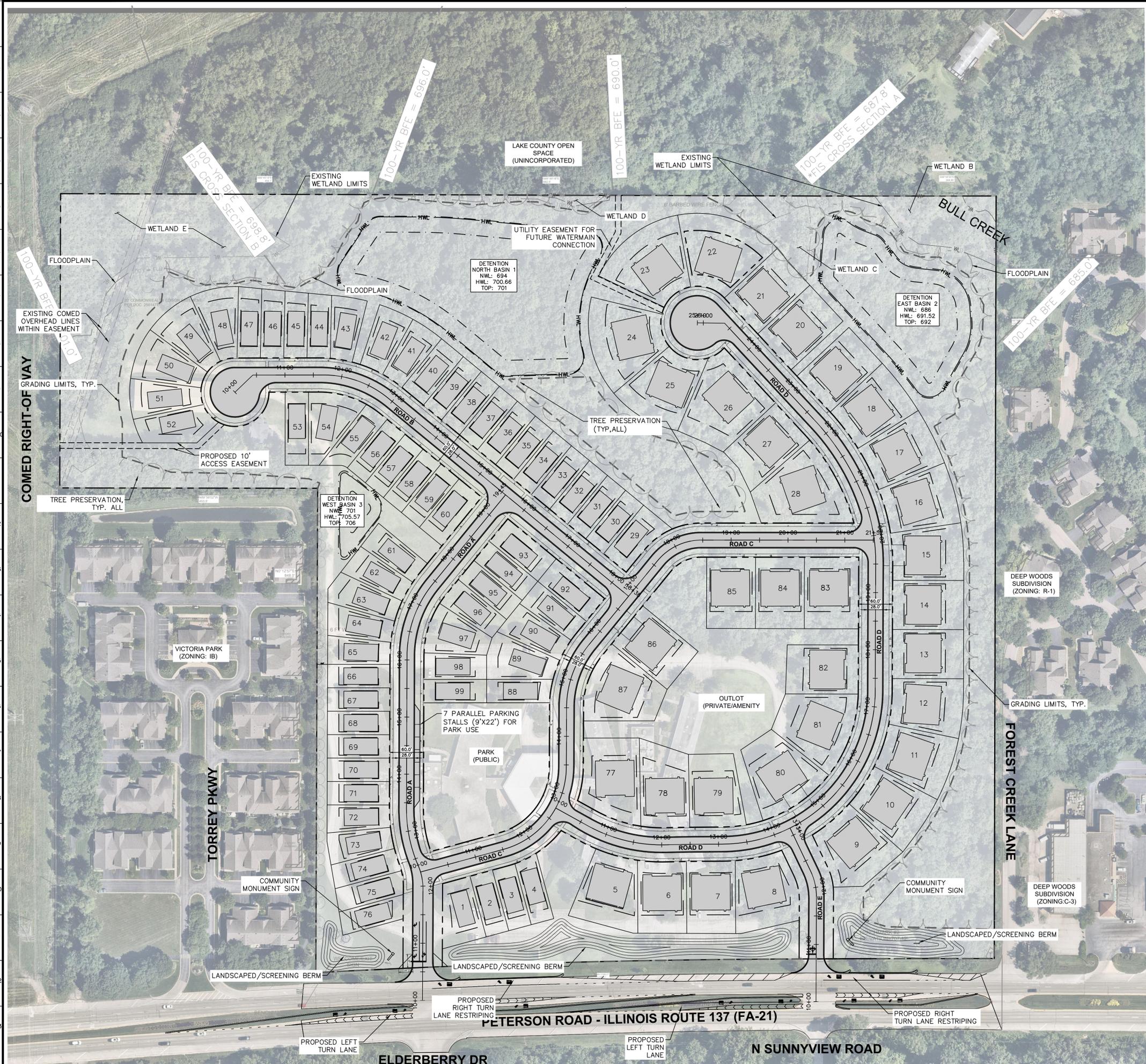
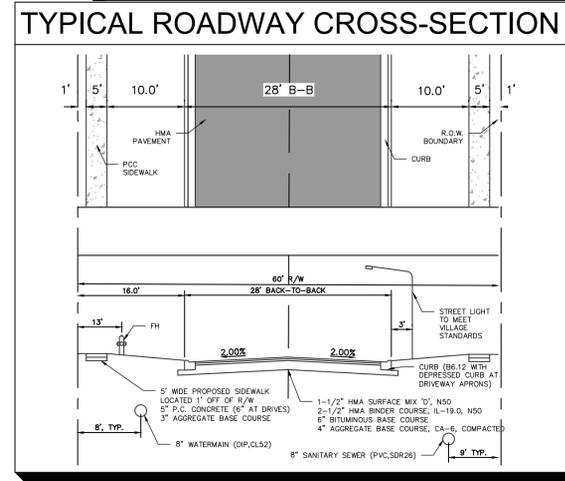
ZONING SUMMARY:
 EXISTING ZONING DISTRICT = MIXED (IB/R1/C3/UNINCORPORATED)
 PROPOSED ZONING DISTRICT = R-7 PUD

SINGLE FAMILY (41' X 110')	= 64 UNITS
GREENWAY CHASE SPRINGS	= 20 FEET
FRONT YARD SETBACK	= 20 FEET
REAR YARD SETBACK	= 15 FEET
CORNER SIDE YARD SETBACK	= 5 FEET*
SIDE YARD SETBACK	= 5 FEET*
*(MINIMUM 10 FT BUILDING SEPARATION)	
DUPLEXES (86' X 125')	= 70 UNITS
GREENWAY CHASE LANDINGS	= 20 FEET
FRONT YARD SETBACK	= 20 FEET
REAR YARD SETBACK	= 30 FEET
CORNER SIDE YARD SETBACK	= 15 FEET
SIDE YARD SETBACK	= 6 FEET*
*(MINIMUM 12 FT BUILDING SEPARATION)	

MAXIMUM BUILDING COVERAGE SINGLE FAMILY	= 45%
MAXIMUM IMPERVIOUS COVERAGE SINGLE FAMILY	= 50%
MAXIMUM BUILDING COVERAGE DUPLEX	= 50%
MAXIMUM IMPERVIOUS COVERAGE DUPLEX	= 55%

FEMA NOTE

PER FLOOD INSURANCE RATE MAP PANEL NO. 17097C0161K EFFECTIVE DATE 9/19/2013, A PORTION OF THE SITE IS LOCATED IN ZONE "AE" WITH A VARYING BASE FLOOD ELEVATION RANGING FROM 701.0 ON THE WEST SIDE TO 684.0 ON THE EAST SIDE. AS NOTED HEREIN, THE REMAINDER OF THE SITE IS LOCATED WITHIN ZONE "X" (AREA OF MINIMAL FLOOD HAZARD).

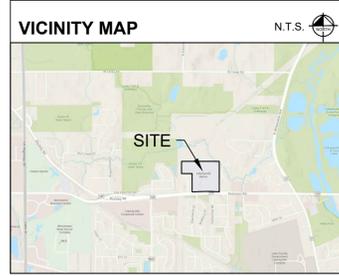


Kimley»Horn <small>© 2025 KIMLEY-HORN AND ASSOCIATES, INC. 576 LAKE COOK ROAD, SUITE 200 LIBERTYVILLE, IL 60048 WWW.KIMLEY-HORN.COM</small>	PULTE HOME COMPANY, LLC
SCALE: AS NOTED DESIGNED BY: INS DRAWN BY: KTHM CHECKED BY: RNM	OVERALL SITE PLAN
GREENWAY CHASE <small>610 PETERSON ROAD LIBERTYVILLE, IL 60048</small>	ORIGINAL ISSUE: 01/03/2025 KHA PROJECT NO. 168247001 SHEET NUMBER C2.0
VILLAGE RESUBMITTAL VILLAGE RESUBMITTAL ARC RE-SUBMITTAL REVISIONS DATE 05/19/25 03/28/25 03/07/25	BY INS RNM DC

PRELIMINARY PLAT OF SUBDIVISION GREENWAY CHASE

THAT PART OF THE NORTHEAST QUARTER OF SECTION 8,
TOWNSHIP 44 NORTH, RANGE 11 EAST OF THE
THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS.

Tax PINs:
11-08-100-012
11-08-100-014
11-08-100-035
11-08-100-036
11-08-200-001



Plat Prepared For:
Pulte Group
1900 East Golf Road, Suite 300
Schaumburg, IL 60173

Site Address:
540-610 Peterson Rd.
Libertyville, IL 60048

SURVEY NOTES:

Underground utilities shown hereon are from record drawings obtained from the Village of Libertyville and the engineer of record and the surveyor cannot guarantee the locations of said utilities, except those that are observed on the surface at the time of this survey.

Measurements are made in feet and decimal feet, measurements shown in parentheses ex. (100.00) are record dimensions.

This service meets the Illinois minimum requirements for a boundary and topographic survey.

Field work was completed on October 30, 2024.

Parking Summary: There are 159 parking spaces and 5 handicapped parking spaces.

Iron Pipes set at all corners unless otherwise denoted.

AREA SUMMARY

PARCEL 1: 413,510 SQ.FT.	9.493 AC.
PARCEL 2: 368,588 SQ.FT.	8.461 AC.
PARCEL 3: 233,588 SQ.FT.	5.362 AC.
PARCEL 4: 271,940 SQ.FT.	6.243 AC.
PARCEL 5: 352,772 SQ.FT.	8.099 AC.
PARCEL 6: 193,703 SQ.FT.	4.447 AC.

TOTAL AREA: 1,834,101 SQ.FT. 42.105 AC.

REFERENCE DATUM

North American Vertical Datum 1988

Benchmark #1 Elevation = 755.97'

Chiseled square cut on east side of concrete traffic signal post at the Northwest corner of the intersection of N. Prospect Ave. and the entrance to Baytowne Shopping Square (Southeast corner of site).

Benchmark #2 Elevation = 759.02'

Chiseled square cut on south edge of concrete light pole base at the Northwest corner of site.

FLOOD STATEMENT:

According to Federal Emergency Management Agency's Flood Insurance Rate Map No. 17097C0161K, for Lake County, Illinois and incorporated areas, both dated September 18, 2013, this property is located within

Zone AE defined as "Special Flood Hazard Areas" Areas determined to be within the 1.0% annual chance flood plain with base flood elevations established.

Zone X (unshaded) defined as "Areas determined to be outside the 0.2% annual chance floodplain"

If this site is not within an identified special flood hazard area, this flood statement does not imply that the property and/or the structures thereon will be free from flooding or flood damage. On rare occasions, greater floods can and will occur and flood heights may be increased by man-made or natural causes. This flood statement shall not create liability on the part of the surveyor.

BASIS OF BEARINGS

North American Datum of 1983 (2011)
Illinois State Plane East Zone (1201)

LINE TABLE		
NO.	BEARING	LENGTH
L1	S61°06'44"E	63.97'
L2	N88°50'17"E	48.71'
L3	N89°40'35"E	52.78'
L4	N74°47'13"E	28.14'
L6	S61°06'44"E	36.47'
L7	S89°49'18"E	7.91'
L8	N88°50'17"E	37.24'
L9	N88°50'17"E	11.25'
L10	N89°40'35"E	52.56'
L11	S89°49'18"E	9.67'
L12	N36°19'42"W	13.48'
L13	N80°56'26"W	40.67'
L14	S86°14'27"W	44.97'
L15	S45°50'35"W	44.97'
L16	S06°55'23"W	44.79'
L17	S33°18'16"E	44.79'
L18	S44°58'21"E	33.09'
L19	S73°57'04"E	33.85'
L20	S42°59'38"E	39.49'
L21	S26°04'16"E	46.46'
L22	N50°38'08"W	6.45'
L23	N52°52'49"W	36.59'
L24	N59°05'49"W	33.91'
L25	N64°57'35"W	31.55'
L26	N71°17'08"W	39.07'
L27	N77°52'51"W	34.56'
L28	N84°11'50"W	35.96'

LINE TABLE		
NO.	BEARING	LENGTH
L29	N85°06'47"W	41.09'
L30	N84°35'59"W	41.11'
L31	N84°35'59"W	41.11'
L32	N84°35'59"W	41.10'
L33	S85°03'05"W	43.51'
L34	S71°43'26"W	43.51'
L35	S57°27'00"W	48.38'
L36	S43°05'42"W	44.10'
L37	S29°04'21"W	46.23'
L38	S14°43'03"W	46.23'
L39	S02°03'26"W	36.16'
L40	S09°24'30"E	36.16'
L41	S21°40'27"E	44.07'
L42	N89°54'57"E	3.84'
L43	N89°54'57"E	28.42'
L44	N89°54'57"E	18.84'
L45	S89°47'29"E	18.72'
L46	S89°47'29"E	37.18'
L47	S70°37'01"E	13.14'
L48	N50°38'44"W	14.55'
L49	N14°52'55"W	40.79'
L51	S61°06'44"E	36.51'
L52	N88°50'17"E	44.39'
L53	N88°50'17"E	4.53'
L54	S89°40'35"W	53.00'
L55	N16°55'51"E	32.09'

CURVE TABLE				
NO.	RADIUS	LENGTH	CHORD BEARING	CHORD
C1	100.75'	29.63'	N08°18'22"W	29.52'
C2	100.75'	11.81'	N20°05'14"W	11.80'
C3	250.00'	103.21'	N11°39'41"W	102.48'
C4	199.91'	137.60'	N19°38'04"E	134.90'
C5	180.00'	123.90'	S70°21'54"E	121.47'
C6	150.00'	120.27'	S51°49'05"W	117.07'
C7	150.00'	75.75'	S14°22'57"W	74.94'
C8	175.00'	119.54'	S19°47'07"W	117.23'
C9	100.00'	88.39'	S64°40'38"W	85.54'
C10	270.00'	141.61'	S76°08'14"E	139.99'
C11	270.00'	123.00'	N76°38'24"E	121.94'
C12	270.00'	293.01'	N32°30'01"E	278.84'
C13	284.95'	36.95'	N03°51'43"W	36.93'
C14	303.29'	79.72'	N14°31'33"W	79.49'
C15	434.50'	151.62'	N32°08'08"W	150.85'
C16	100.00'	45.14'	N12°42'58"W	44.76'
C17	135.75'	8.84'	N01°43'55"W	8.84'
C18	180.00'	12.87'	N72°44'18"E	12.87'
C19	180.00'	41.85'	N64°01'46"E	41.75'
C20	180.00'	59.44'	N47°54'33"E	59.17'
C21	300.00'	32.53'	S64°13'06"E	32.51'
C22	300.00'	75.89'	S74°34'16"E	75.69'
C23	300.00'	48.93'	S86°29'24"E	48.87'
C24	299.98'	22.21'	N87°34'07"E	22.21'
C25	299.17'	84.01'	N77°24'30"E	83.73'

CURVE TABLE				
NO.	RADIUS	LENGTH	CHORD BEARING	CHORD
C26	70.00'	31.60'	N12°42'58"W	31.33'
C27	130.00'	16.46'	N03°24'43"W	16.45'
C28	130.00'	42.22'	N16°20'38"W	42.04'
C29	300.00'	85.06'	N49°47'58"E	84.78'
C30	300.00'	79.28'	N34°06'20"E	79.05'
C31	300.00'	78.46'	N19°02'31"E	78.24'
C32	298.89'	53.86'	N06°23'47"E	53.79'
C33	332.78'	49.20'	N04°29'27"W	49.16'
C34	345.88'	79.14'	N15°15'21"W	78.97'
C35	464.50'	80.93'	N27°07'37"W	80.83'
C36	464.50'	80.44'	N37°04'45"W	80.34'
C37	65.00'	65.28'	N71°45'16"W	62.57'
C38	65.00'	61.13'	S52°32'08"W	58.90'
C39	65.00'	13.22'	S19°46'10"W	13.20'
C40	65.00'	60.75'	S12°49'50"E	58.56'
C41	65.00'	75.44'	S72°51'15"E	71.28'
C42	65.00'	35.50'	N58°15'00"E	35.06'
C43	404.50'	12.76'	N41°11'51"W	12.76'
C44	404.50'	128.15'	N31°13'04"W	127.62'
C45	273.29'	37.55'	N18°06'53"W	37.52'
C46	131.29'	52.41'	S78°30'24"W	52.06'
C47	130.00'	62.53'	S53°08'06"W	61.93'
C48	210.00'	39.69'	N56°03'36"W	39.63'
C49	210.00'	38.48'	N66°43'26"W	38.43'
C50	210.00'	39.17'	N81°20'56"W	39.11'

CURVE TABLE				
NO.	RADIUS	LENGTH	CHORD BEARING	CHORD
C51	210.00'	12.43'	N88°23'17"W	12.43'
C52	60.00'	26.23'	S77°23'31"W	26.02'
C53	60.00'	30.07'	S50°30'47"W	29.75'
C54	60.00'	30.07'	S21°48'11"W	29.75'
C55	60.00'	30.07'	S06°54'25"E	29.75'
C56	60.00'	41.98'	S41°18'26"E	41.13'
C57	60.00'	124.34'	N59°16'54"E	103.25'
C58	150.00'	80.27'	N74°45'14"W	79.31'
C59	196.73'	30.14'	N53°59'27"W	30.11'
C60	229.91'	20.20'	S36°50'10"W	20.20'
C61	229.91'	37.70'	S29°37'14"W	37.66'
C62	229.91'	37.70'	S20°13'28"W	37.66'
C63	229.91'	37.70'	S10°49'41"W	37.66'
C64	229.91'	24.86'	S03°01'58"W	24.84'
C65	279.94'	33.42'	S03°15'06"E	33.40'
C66	279.94'	37.74'	S10°31'59"E	37.71'
C67	100.00'	31.30'	N08°47'53"W	31.17'
C68	180.00'	60.69'	N09°36'05"E	60.41'
C69	240.00'	84.06'	S71°08'47"E	83.63'
C70	240.00'	41.81'	S86°10'17"E	41.76'
C71	240.02'	42.18'	N84°39'26"E	42.12'
C72	240.00'	143.47'	N62°29'52"E	141.34'
C73	240.00'	143.45'	N28°14'54"E	141.33'
C74	240.00'	40.39'	N06°18'16"E	40.34'
C75	254.95'	6.83'	N00°56'06"W	6.83'
C76	70.00'	61.88'	S64°40'38"W	59.88'
C77	145.41'	96.75'	S20°14'56"W	94.98'
C78	169.91'	18.82'	S36°10'48"W	18.81'
C79	169.91'	91.76'	S17°32'06"W	90.65'
C80	169.91'	6.45'	S00°58'36"W	6.45'
C81	205.00'	14.20'	S37°22'11"W	14.20'
C82	207.85'	40.30'	S29°45'11"W	40.24'
C83	205.00'	35.48'	S19°09'45"W	35.44'
C84	205.00'	41.48'	S08°24'27"W	41.41'
C85	205.00'	8.57'	S01°24'49"W	8.57'
C86	120.00'	156.88'	N37°20'01"E	145.95'
C87	220.06'	76.81'	S09°50'12"E	76.42'
C88	210.00'	7.00'	S75°03'06"E	6.99'
C89	240.00'	30.36'	S76°00'00"W	30.34'
C90	240.00'	142.38'	N55°22'52"E	140.30'
C91	240.00'	154.58'	S19°56'06"W	151.92'
C92	240.00'	42.18'	N84°39'28"E	42.12'

LOT TABLE		
LOT NO.	ACRES	SQ. FT.
1	0.163	7,087
2	0.104	4,510
3	0.104	4,513
4	0.107	4,656
5	0.289	12,569
6	0.248	10,793
7	0.249	10,866
8	0.304	13,262
9	0.303	13,186
10	0.270	11,773
11	0.274	11,947
12	0.266	11,592
13	0.247	10,765
14	0.247	10,750
15	0.262	11,429
16	0.269	11,716
17	0.263	11,466
18	0.265	11,534
19	0.247	10,750
20	0.247	10,749
21	0.245	10,665
22	0.372	16,211
23	0.350	15,250
24	0.342	14,883
25	0.295	12,849
26	0.385	16,767

LOT TABLE		
LOT NO.	ACRES	SQ. FT.
27	0.255	11,112
28	0.378	16,465
29	0.135	5,895
30	0.104	4,510
31	0.104	4,510
32	0.104	4,510
33	0.104	4,510
34	0.104	4,510
35	0.104	4,510
36	0.104	4,510
37	0.104	4,510
38	0.104	4,510
39	0.104	4,510
40	0.124	5,409
41	0.129	5,600
42	0.136	5,909
43	0.130	5,644
44	0.105	4,571
45	0.108	4,723
46	0.112	4,885
47	0.116	5,044
48	0.167	7,257
49	0.174	7,583
50	0.174	7,583
51	0.146	6,365
52	0.158	6,901

LOT TABLE		
LOT NO.	ACRES	SQ. FT.
53	0.120	5,247
54	0.187	8,130
56	0.104	4,509
57	0.104	4,510
58	0.104	4,510
59	0.104	4,510
60	0.129	5,607
61	0.118	5,156
62	0.124	5,382
63	0.137	5,976
64	0.126	5,500
65	0.114	4,949
66	0.104	4,510
67	0.104	4,510
68	0.104	4,510
69	0.104	4,510
70	0.104	4,510
71	0.104	4,510
72	0.112	4,899

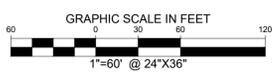
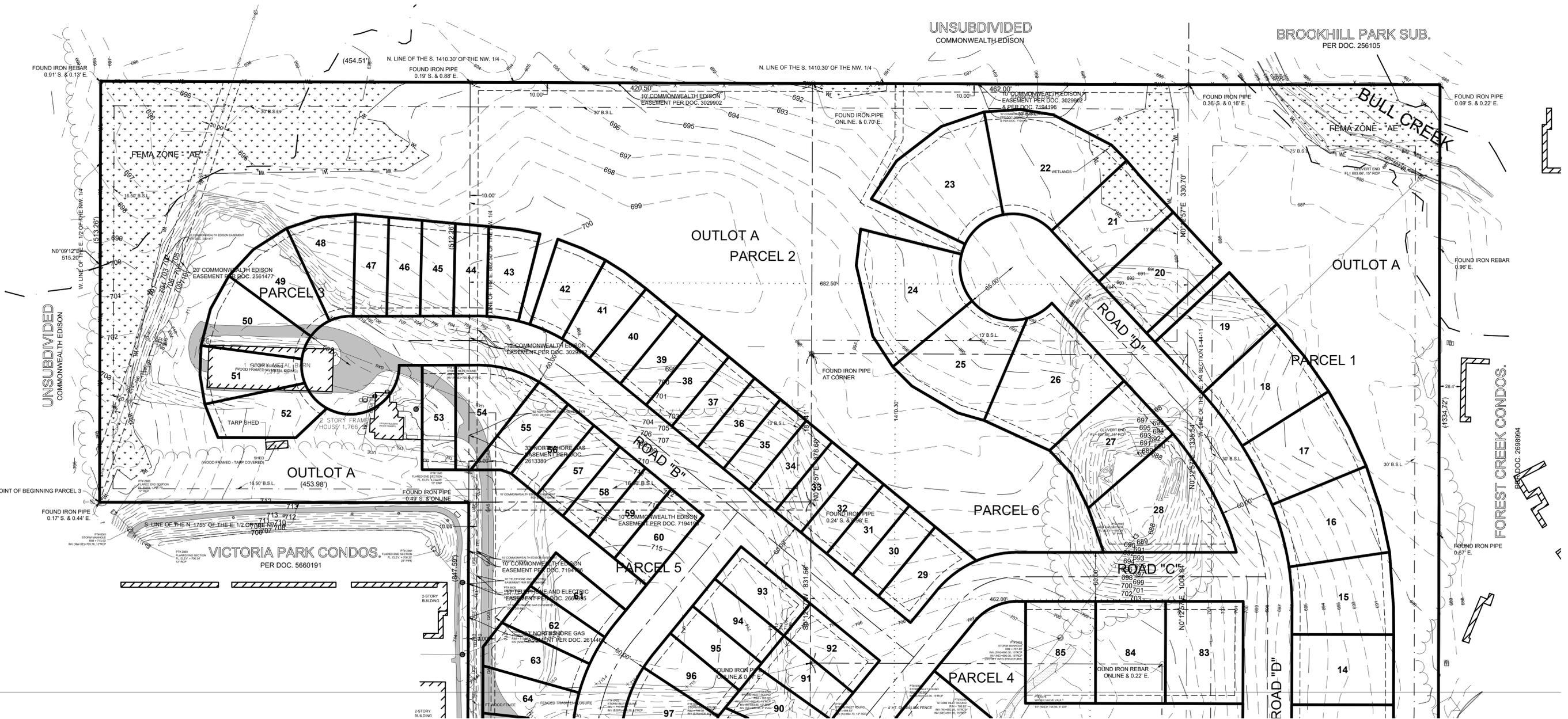
**PRELIMINARY PLAT OF SUBDIVISION
GREENWAY CHASE**

THAT PART OF THE NORTHEAST QUARTER OF SECTION 8,
TOWNSHIP 44 NORTH, RANGE 11 EAST OF THE
THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS.

Tax PINs:
11-08-100-012
11-08-100-014
11-08-100-035
11-08-100-036
11-08-200-001

Plat Prepared For:
Pulte Group
1900 East Golf Road, Suite 300
Schaumburg, IL 60173

Site Address:
540-610 Peterson Rd.
Libertyville, IL 60048



No.	DATE	REVISION DESCRIPTION	Scale	Drawn by	Checked by	Date	Project No.	Sheet No.
1	3/31/25	REVISED OUTLOT GEOMETRY	1"=60'	MGJ	BAS	01/06/25	168247001	2 OF 6

Kimley»Horn

4201 Winfield Road
Warrenville, Illinois 60555
DESIGN FIRM # 184002012-0006
Tel. No. (630) 487-5550
www.kimley-horn.com

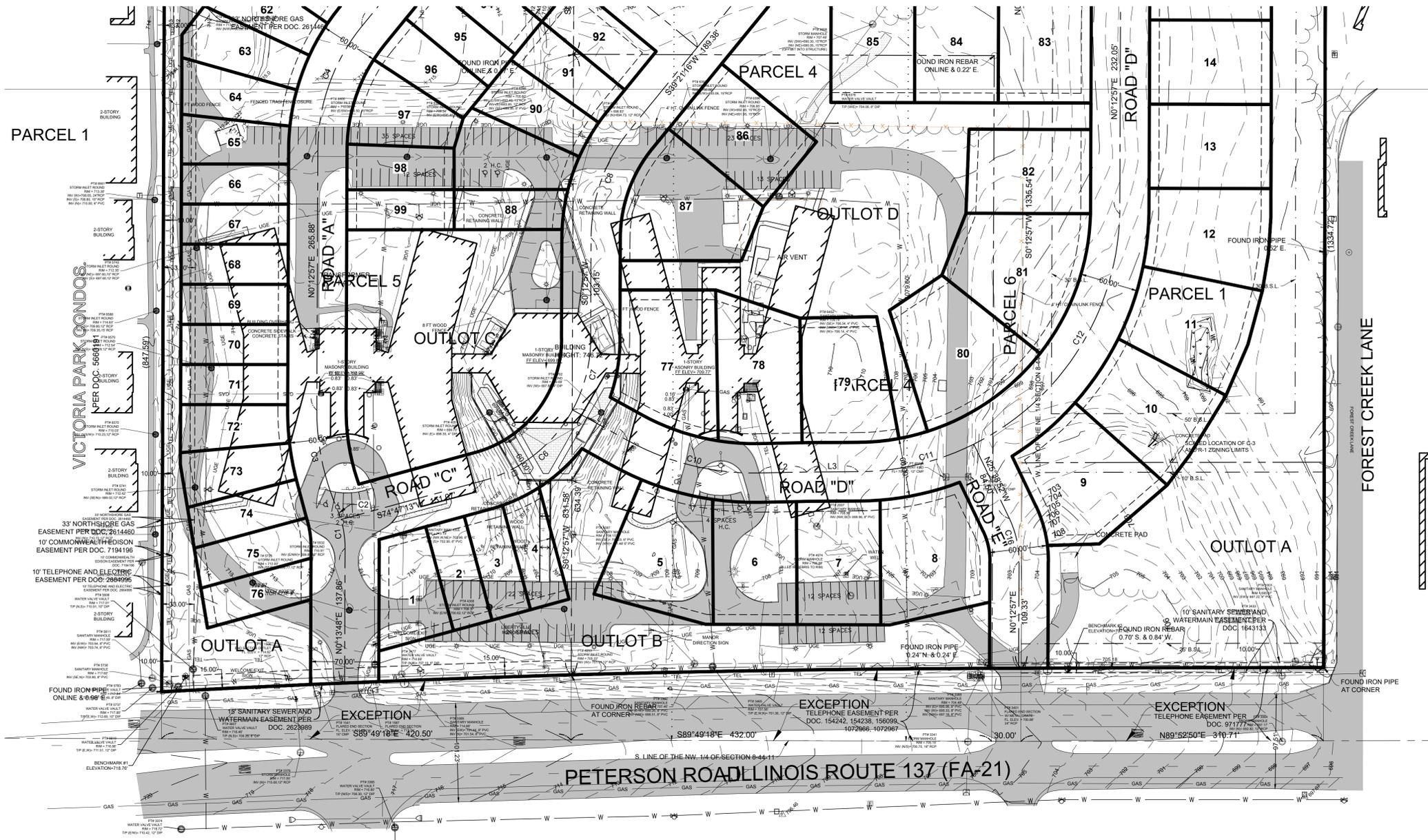
PRELIMINARY PLAT OF SUBDIVISION
GREENWAY CHASE

THAT PART OF THE NORTHEAST QUARTER OF SECTION 8,
TOWNSHIP 44 NORTH, RANGE 11 EAST OF THE
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1	3/31/25	REVISED OUTLOT GEOMETRY	1"=60'	MGJ	BAS	01/06/25	168247001	3 OF 6

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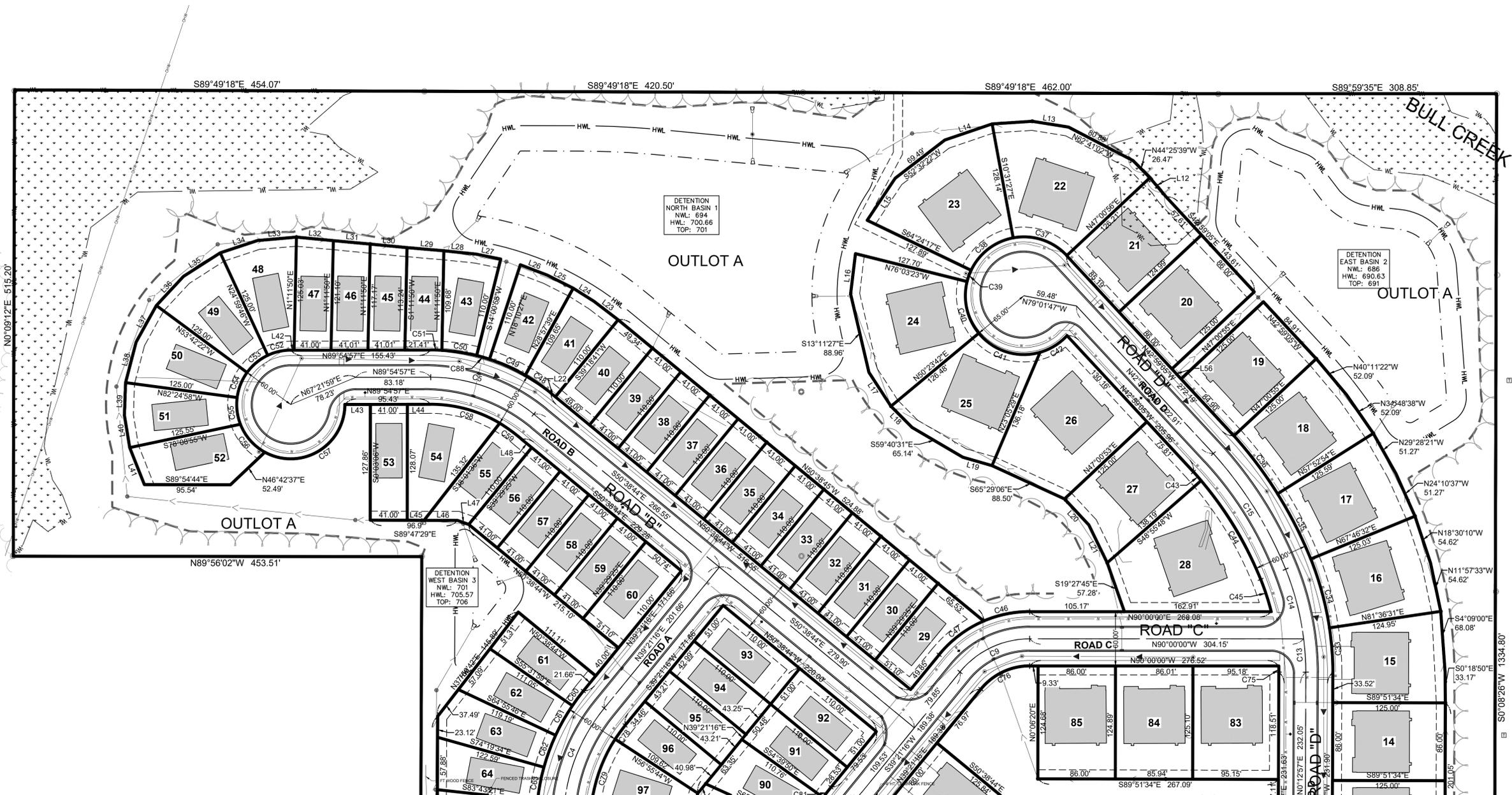
**PRELIMINARY PLAT OF SUBDIVISION
GREENWAY CHASE**

THAT PART OF THE NORTHEAST QUARTER OF SECTION 8,
TOWNSHIP 44 NORTH, RANGE 11 EAST OF THE
THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS.

Tax PINs:
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1900 East Golf Road, Suite 300
Schaumburg, IL 60173

Site Address:
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Libertyville, IL 60048



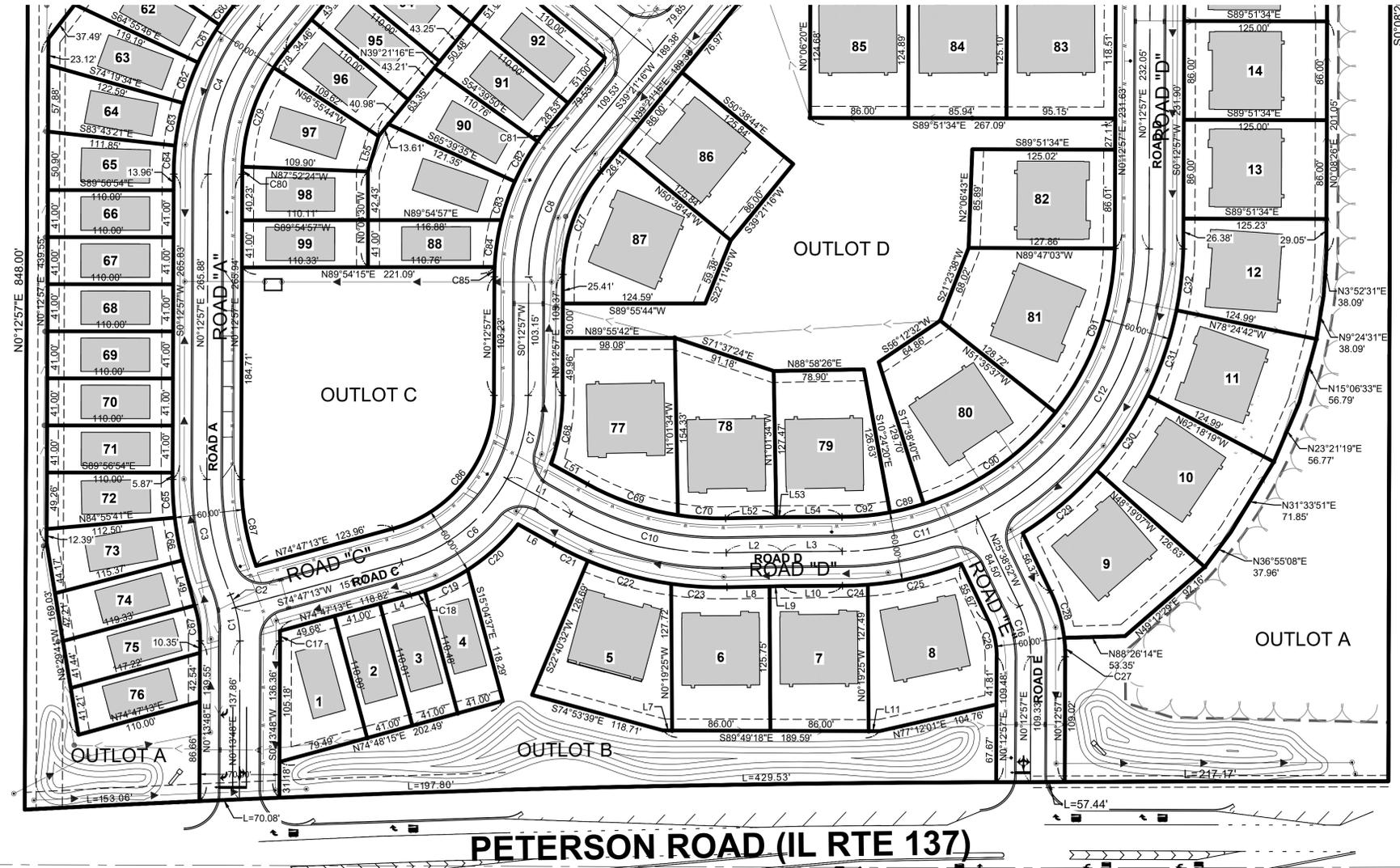
PRELIMINARY PLAT OF SUBDIVISION
GREENWAY CHASE

THAT PART OF THE NORTHEAST QUARTER OF SECTION 8,
TOWNSHIP 44 NORTH, RANGE 11 EAST OF THE
THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS.

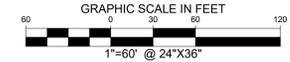
Tax PINs:
11-08-100-012
11-08-100-014
11-08-100-035
11-08-100-036
11-08-200-001

Plat Prepared For:
Pulte Group
1900 East Golf Road, Suite 300
Schaumburg, IL 60173

Site Address:
540-610 Peterson Rd.
Libertyville, IL 60048



PETERSON ROAD (IL RTE 137)



No.	DATE	REVISION DESCRIPTION	Scale	Drawn by	Checked by	Date	Project No.	Sheet No.
1	3/31/25	REVISED OUTLOT GEOMETRY	1"=60'	MGJ	BAS	01/06/25	168247001	3 OF 6

Kimley»Horn

4201 Winfield Road
Warrenville, Illinois 60555
Tel. No. (630) 487-5550
www.kimley-horn.com

PRELIMINARY PLAT OF SUBDIVISION GREENWAY CHASE

THAT PART OF THE NORTHEAST QUARTER OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS.

LAKE COUNTY RECORDER'S CERTIFICATE

STATE OF ILLINOIS)
) S.S.
COUNTY OF LAKE)

THIS INSTRUMENT NUMBER _____ WAS FILED FOR RECORD IN THE RECORDER'S OFFICE OF LAKE COUNTY, ILLINOIS, THIS _____ DAY OF _____, A.D. 20____, AT _____ O'CLOCK ____ M.

RECORDER

COUNTY CLERK CERTIFICATE

STATE OF ILLINOIS)
) S.S.
COUNTY OF LAKE)

I, _____ COUNTY CLERK OF LAKE COUNTY, ILLINOIS, DO HEREBY CERTIFY THAT THERE ARE NO DELINQUENT GENERAL TAXES, NO UNPAID, NO UNPAID FORFEITED TAXES AND NO REDEEMABLE TAX SALES AGAINST ANY OF THE LAND INCLUDED IN THE ATTACHED PLAT. I FURTHER CERTIFY THAT I HAVE RECEIVED ALL STATUTORY FEES IN CONNECTION WITH THE ANNEXED PLAT.

DATED THIS _____ DAY OF _____, A.D. 20____.

BY: _____
COUNTY CLERK

PERMISSION TO RECORD

STATE OF ILLINOIS)
) S.S.
COUNTY OF DUPAGE)

I, BRADLEY A. STROHL, ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3686, HEREBY GRANT PERMISSION TO A REPRESENTATIVE OF THE VILLAGE OF LIBERTYVILLE, ILLINOIS, TO RECORD THIS PLAT ON OR BEFORE DECEMBER 31, 2025. SHALL SHOW PROPER IDENTIFICATION AND PROVIDE THIS SURVEYOR WITH A RECORDED COPY OF SAID PLAT.

DATED THIS _____ DAY OF _____, A.D. 20____.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3686
LICENSE EXPIRES: NOVEMBER 30, 2026

DESIGN FIRM PROFESSIONAL REGISTRATION NO. 184002012-0006
EXPIRES APRIL 30, 2025

SURVEYOR'S CERTIFICATE

STATE OF ILLINOIS)
) S.S.
COUNTY OF DUPAGE)

THIS IS TO DECLARE THAT THE FOLLOWING DESCRIBED PROPERTY WAS SURVEYED AND SUBDIVIDED BY KIMLEY-HORN, INC., UNDER THE SUPERVISION OF AN ILLINOIS PROFESSIONAL LAND SURVEYOR AND THAT THE PLAT HEREON DRAWN IS A CORRECT REPRESENTATION OF SAID SURVEY.

LEGAL DESCRIPTION OF PROPERTY BEING SUBDIVIDED INCLUDED HEREON

SUBDIVIDED PROPERTY CONTAINS 42.105 ACRES, MORE OR LESS AND ALL DISTANCES ARE SHOWN IN FEET AND DECIMAL PARTS THEREOF.

1/2 DIAMETER BY 24" LONG IRON PIPES WILL BE SET AT ALL SUBDIVISION CORNERS, LOT CORNERS, POINTS OF CURVATURE AND POINTS OF TANGENCY IN COMPLIANCE WITH ILLINOIS STATUTES AND APPLICABLE ORDINANCES, UNLESS OTHERWISE NOTED.

THIS IS ALSO TO DECLARE THAT THE PROPERTY AS DESCRIBED ON THE ANNEXED PLAT LIES WITHIN THE CORPORATE LIMITS OF THE VILLAGE OF LIBERTYVILLE, ILLINOIS WHICH HAS ADOPTED A VILLAGE PLAN AND IS EXERCISING THE SPECIAL POWER AUTHORIZED BY DIVISION 12 OF ARTICLE 11 OF THE ILLINOIS MUNICIPAL CODE.

GIVEN UNDER MY HAND AND SEAL THIS _____ DAY OF _____, A.D. 20____.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3686
LICENSE EXPIRES: NOVEMBER 30, 2026

DESIGN FIRM PROFESSIONAL REGISTRATION NO. 184002012-0006
EXPIRES APRIL 30, 2025

Tax PINs:
11-08-100-012
11-08-100-014
11-08-100-035
11-08-100-036
11-08-200-001

Plat Prepared For:
Pulte Group
1900 East Golf Road, Suite 300
Schaumburg, IL 60173

Site Address:
540-610 Peterson Rd.
Libertyville, IL 60048

OWNER'S CONSENT

STATE OF _____)
) S.S.
COUNTY OF _____)

THE UNDERSIGNED, _____, HEREBY CERTIFIES THAT HE/SHE/THEY/IT IS THE HOLDER OF THE LEGAL TITLE OF ALL OF THE PROPERTY DESCRIBED HEREON AND THAT IT HAS CAUSED SAID PROPERTY TO BE SURVEYED AND SUBDIVIDED AS

SHOWN ON THE PLAT HEREON DRAWN. THIS IS TO ALSO CERTIFY THAT _____

AS OWNER OF THE PROPERTY DESCRIBED AS _____ AND LEGALLY DESCRIBED ON THE PLAT OF THE SAME NAME, HAVE DETERMINED TO THE BEST OF OUR KNOWLEDGE THE SCHOOL DISTRICT IN WHICH EACH OF THE FOLLOWING LOTS LIE.

LOT NUMBER(S) SCHOOL DISTRICT

ALL GRADE SCHOOL DISTRICT NO. 70 (LIBERTYVILLE)
HIGH SCHOOL DISTRICT NO. 128 (LIBERTYVILLE)
JUNIOR COLLEGE DISTRICT NO. C03532 (LAKE CO. COMM. COLL.)

DATED THIS _____ DAY OF _____, A.D. 20____.

BY: _____

BY: _____

NOTARY PUBLIC

STATE OF _____)
) S.S.
COUNTY OF _____)

I, _____, A NOTARY PUBLIC IN AND FOR THE COUNTY AND STATE

AFORSAID, DO HEREBY CERTIFY THAT _____ AND _____

OF _____ WHO IS/ARE PERSONALLY KNOWN TO ME TO BE THE SAME WHOSE NAME(S) IS/ARE SUBSCRIBED TO THE FOREGOING CERTIFICATE, APPEARED BEFORE ME THIS DAY IN PERSON AND ACKNOWLEDGED THAT HE/SHE/THEY DID SIGN AND DELIVER THIS INSTRUMENT AS A FREE AND VOLUNTARY ACT FOR THE USES AND PURPOSES HEREIN SET FORTH.

GIVEN UNDER MY HAND AND NOTORIAL SEAL THIS _____ DAY OF _____, A.D. 20____.

NOTARY PUBLIC

COMMONWEALTH EDISON AND SBC EASEMENT PROVISIONS

AN EASEMENT FOR SERVING THE SUBDIVISION AND OTHER PROPERTY WITH ELECTRIC AND COMMUNICATION SERVICE IS HEREBY RESERVED FOR AND GRANTED TO:

COMMONWEALTH EDISON COMPANY
AND
SBC ILLINOIS, A.K.A. AMERITECH ILLINOIS,
A.K.A. ILLINOIS BELL TELEPHONE COMPANY, GRANTEEES,

THEIR RESPECTIVE LICENSEES, SUCCESSORS, AND ASSIGNS, JOINTLY AND SEVERALLY, TO CONSTRUCT, OPERATE, REPAIR, MAINTAIN, MODIFY, RECONSTRUCT, REPLACE, SUPPLEMENT, RELOCATE AND REMOVE, FROM TIME TO TIME, POLES, GUYS, ANCHORS, WIRES, CABLES, CONDUITS, MANHOLES, TRANSFORMERS, PEDESTALS, EQUIPMENT CABINETS OR OTHER FACILITIES USED IN CONNECTION WITH OVERHEAD AND UNDERGROUND TRANSMISSION AND DISTRIBUTION OF ELECTRICITY COMMUNICATIONS, SOUNDS AND SIGNALS IN, OVER, UNDER, ACROSS, ALONG AND UPON THE SURFACE OF THE PROPERTY SHOWN WITHIN THE DASHED OR DOTTED LINES (OR SIMILAR DESIGNATION) ON THE PLAT AND MARKED "EASEMENT", "UTILITY EASEMENT", "PUBLIC UTILITY EASEMENT", "P.U.E." (OR SIMILAR DESIGNATION), THE PROPERTY DESIGNATED IN THE DECLARATION OF CONDOMINIUM AND/OR ON THIS PLAT AS "COMMON ELEMENTS", AND THE PROPERTY DESIGNATED ON THE PLAT AS "COMMON AREA OR AREAS", AND THE PROPERTY DESIGNATED ON THE PLAT FOR STREETS AND ALLEYS, WHETHER PUBLIC OR PRIVATE, TOGETHER WITH THE RIGHT TO INSTALL REQUIRED SERVICE CONNECTIONS OVER OR UNDER THE SURFACE OF EACH LOT AND COMMON AREA OR AREAS TO SERVE IMPROVEMENTS THEREON, OR ON ADJACENT LOTS, AND COMMON AREA OR AREAS, THE RIGHT TO CUT, TRIM OR REMOVE TREES, BUSHES, ROOTS AND SAPLINGS AND TO CLEAR OBSTRUCTIONS FROM THE SURFACE AND SUBSURFACE AS MAY BE REASONABLY REQUIRED INCIDENT TO THE RIGHTS HEREIN GIVEN, AND THE RIGHT TO ENTER UPON THE SUBDIVIDED PROPERTY FOR ALL SUCH PURPOSES. OBSTRUCTIONS SHALL NOT BE PLACED OVER GRANTEEES' FACILITIES OR IN, UPON OR OVER THE PROPERTY WITHIN THE DASHED OR DOTTED LINES (OR SIMILAR DESIGNATION) MARKED "EASEMENT", "UTILITY EASEMENT", "PUBLIC UTILITY EASEMENT", "P.U.E." (OR SIMILAR DESIGNATION), WITHOUT THE PRIOR WRITTEN CONSENT OF GRANTEEES. AFTER INSTALLATION OF ANY SUCH FACILITIES, THE GRADE OF THE SUBDIVIDED PROPERTY SHALL NOT BE ALTERED IN A MANNER SO AS TO INTERFERE WITH THE PROPER OPERATION AND MAINTENANCE THEREOF.

THE TERM "COMMON ELEMENTS" SHALL HAVE THE MEANING SET FORTH IN SUCH TERM IN THE "CONDOMINIUM PROPERTY ACT", CHAPTER 765 ILCS 605/2, AS AMENDED FROM TIME TO TIME.

THE TERM "COMMON AREA OR AREAS" IS DEFINED AS A LOT, PARCEL OR AREA OF REAL PROPERTY, THE BENEFICIAL USE AND ENJOYMENT OF WHICH IS RESERVED IN WHOLE AS AN APPORTIONMENT TO THE SEPARATELY OWNED LOTS, PARCEL OR AREAS WITHIN THE PLANNED DEVELOPMENT, EVEN THOUGH SUCH BE OTHERWISE DESIGNATED ON THE PLAT BY TERMS SUCH AS "OUTLOTS", "COMMON ELEMENTS", "OPEN SPACE", "OPEN AREA", "COMMON GROUND", "PARKING" AND "COMMON AREA". THE TERM "COMMON AREA OR AREAS", AND "COMMON ELEMENTS" INCLUDES REAL PROPERTY SURFACED WITH INTERIOR DRIVEWAYS AND WALKWAYS, BUT EXCLUDES REAL PROPERTY PHYSICALLY OCCUPIED BY A BUILDING, SERVICE BUSINESS DISTRICT OR STRUCTURES SUCH AS A POOL OR RETENTION POND OR MECHANICAL EQUIPMENT.

RELOCATION OF FACILITIES WILL BE DONE BY GRANTEEES AT COST OF GRANTOR/LOT OWNER, UPON WRITTEN REQUEST.

VILLAGE ENGINEER/PLAT OFFICER CERTIFICATE

STATE OF ILLINOIS)
) S.S.
COUNTY OF LAKE)

I, _____ VILLAGE PLAT OFFICER/ENGINEER OF THE VILLAGE OF LIBERTYVILLE, DO HEREBY CERTIFY THAT ALL PROVISIONS PERTAINING TO THE LIBERTYVILLE SUBDIVISION ORDINANCE, INsofar AS THEY PERTAIN TO THE ACCOMPANYING PLAT, HAVE BEEN SATISFACTORILY COMPLIED WITH.

ATTESTED TO THIS _____ DAY OF _____, AD 20____.

VILLAGE PLAT OFFICER/ENGINEER VILLAGE OF LIBERTYVILLE

VILLAGE BOARD CERTIFICATE

STATE OF ILLINOIS)
) S.S.
COUNTY OF LAKE)

APPROVED BY THE PRESIDENT AND BOARD OF TRUSTEES OF THE VILLAGE OF LIBERTYVILLE, LAKE COUNTY, ILLINOIS AT A MEETING, HELD THIS _____ DAY OF _____, AD 20____.

VILLAGE PRESIDENT

PRINTED NAME

VILLAGE CLERK

PRINTED NAME

PLAN COMMISSION CERTIFICATE

STATE OF ILLINOIS)
) S.S.
COUNTY OF LAKE)

APPROVED BY THE PLAN COMMISSION OF THE VILLAGE OF LIBERTYVILLE, LAKE COUNTY, ILLINOIS AT A MEETING, HELD THIS _____ DAY OF _____, AD 20____.

CHAIRMAN

PRINTED NAME

SECRETARY

PRINTED NAME

PERPETUAL EASEMENT

A PERPETUAL EASEMENT APPURTENANT IS HEREBY GRANTED TO THE VILLAGE OF LIBERTYVILLE, ITS SUCCESSORS AND ASSIGNS, OVER, UPON, ACROSS, THROUGH AND UNDER THOSE PORTIONS OF THE ABOVE DESCRIBED REAL ESTATE DESIGNATED AS PUBLIC UTILITY AND/OR DRAINAGE EASEMENT (P.U. & D.E.) ON THIS PLAT FOR THE PURPOSE OF INSTALLING, LAYING, CONSTRUCTING, OPERATING, MAINTAINING, REPAIRING, RENEWING AND REPLACING WATER MAINS, SANITARY SEWER LINES, FORCE MAIN LINES, STORM SEWER LINES, PIPES, STREET LIGHT POWER CABLES, DITCHES, SWALES, STORM WATER DETENTION FACILITIES, AND ANY OTHER VILLAGE UTILITIES, TOGETHER WITH ALL APPURTENANT STRUCTURES, INCLUDING, BUT NOT LIMITED TO, MANHOLES, WET WELLS, LIFT STATIONS, FIRE HYDRANTS, VALVE VAULTS, STREET LIGHTING EQUIPMENT AND ANY AND ALL OTHER FIXTURES AND EQUIPMENT REQUIRED FOR THE PURPOSE OF SERVING THE ABOVE DESCRIBED REAL ESTATE WITH WATER SERVICE, SANITARY SEWER SERVICE, STORM WATER MANAGEMENT, STREET LIGHTING AND OTHER MUNICIPAL SERVICES AND FOR THE PURPOSE OF PROVIDING INGRESS TO AND EGRESS FROM ALL OF THE LOTS IN THE SUBDIVISION FOR EMERGENCY VEHICLES OF ANY AND ALL TYPES WHATSOEVER. IN NO EVENT SHALL ANY PERMANENT BUILDING BE PLACED UPON THE SAID EASEMENT AREAS, BUT THEY MAY BE USED FOR GARDENS, SHRUBS, LANDSCAPING AND SUCH OTHER PURPOSES THAT DO NOT, AND WILL NOT IN THE FUTURE, INTERFERE UNREASONABLY WITH THE EASEMENT RIGHTS HEREIN GRANTED.

NORTHERN ILLINOIS GAS COMPANY EASEMENT PROVISIONS

AN EASEMENT IS HEREBY RESERVED FOR AND GRANTED TO NORTHERN ILLINOIS GAS COMPANY, ITS SUCCESSORS AND ASSIGNS ("N-GAS") TO INSTALL, OPERATE, MAINTAIN, REPAIR, REPLACE AND REMOVE, FACILITIES USED IN CONNECTION WITH THE TRANSMISSION AND DISTRIBUTION OF NATURAL GAS IN, OVER, UNDER, ACROSS, ALONG AND UPON THE SURFACE OF THE PROPERTY SHOWN ON THIS PLAT MARKED "EASEMENT", "COMMON AREA OR AREAS" AND STREETS AND ALLEYS, WHETHER PUBLIC OR PRIVATE, AND THE PROPERTY DESIGNATED IN THE DECLARATION OF CONDOMINIUM AND/OR ON THIS PLAT AS "COMMON ELEMENTS", TOGETHER WITH THE RIGHT TO INSTALL REQUIRED SERVICE CONNECTIONS OVER OR UNDER THE SURFACE OF EACH LOT AND COMMON AREA OR AREAS TO SERVE IMPROVEMENTS THEREON, OR ON ADJACENT LOTS, AND COMMON AREA OR AREAS, AND THE RIGHT TO REMOVE OBSTRUCTIONS, INCLUDING BUT NOT LIMITED TO, TREES, BUSHES, ROOTS AND FENCES AS MAY BE REASONABLY REQUIRED INCIDENT TO THE RIGHTS HEREIN GIVEN, AND THE RIGHT TO ENTER UPON THE PROPERTY FOR ALL SUCH PURPOSES. OBSTRUCTIONS SHALL NOT BE PLACED OVER N-GAS FACILITIES OR IN, UPON OR OVER THE PROPERTY IDENTIFIED ON THIS PLAT FOR UTILITY PURPOSES WITHOUT THE PRIOR WRITTEN CONSENT OF N-GAS. AFTER INSTALLATION OF ANY SUCH FACILITIES, THE GRADE OF THE SUBDIVIDED PROPERTY SHALL NOT BE ALTERED IN A MANNER SO AS TO INTERFERE WITH THE PROPER OPERATION AND MAINTENANCE THEREOF.

THE TERM "COMMON ELEMENTS" SHALL HAVE THE MEANING SET FORTH FOR SUCH TERM IN THE "CONDOMINIUM PROPERTY ACT", CHAPTER 765 ILCS 605/2, AS AMENDED FROM TIME TO TIME.

THE TERM "COMMON AREA OR AREAS" IS DEFINED AS A LOT, PARCEL OR AREA OF REAL PROPERTY, INCLUDING REAL PROPERTY SURFACED WITH INTERIOR DRIVEWAYS AND WALKWAYS, THE BENEFICIAL USE AND ENJOYMENT OF WHICH IS RESERVED IN WHOLE AS AN APPURTENANCE TO THE SEPARATELY OWNED LOTS, PARCELS OR AREAS WITHIN THE PROPERTY, EVEN THOUGH SUCH BE OTHERWISE DESIGNATED ON THE PLAT BY TERMS.

PARCEL 1:

THAT PART OF THE NORTHEAST QUARTER OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE SOUTH LINE OF AND 6.82 CHAINS WEST FROM THE SOUTHEAST CORNER OF SAID NORTHEAST QUARTER; THENCE WEST ALONG SAID SOUTH LINE, 2229.68 FEET TO THE SOUTHWEST CORNER OF SAID NORTHEAST QUARTER, THENCE NORTH ALONG THE WEST LINE OF SAID NORTHEAST QUARTER, 1410.30 FEET; THENCE EAST 2230.38 FEET TO A POINT WHICH IS 6.82 CHAINS WEST FROM THE EAST LINE OF SAID NORTHEAST QUARTER AND 1405.2 FEET NORTH FROM THE SOUTH LINE OF SAID NORTHEAST QUARTER; THENCE SOUTH TO THE PLACE OF BEGINNING (EXCEPTING THEREFROM THE FOLLOWING: (A) THE EAST 1920.68 FEET THEREOF; (B) THAT PART THEREOF, IF ANY, FALLING IN BROOKHILL PARK, A SUBDIVISION OF PARTS OF SECTIONS 8 AND 9, TOWNSHIP AND RANGE AFORESAID, RECORDED APRIL 24, 1925, AS DOCUMENT 256105, IN BOOK "M" OF PLATS, PAGE 100; AND ALSO (C) EXCEPTING THEREFROM THAT PART CONVEYED BY WARRANTY DEED DATED SEPTEMBER 25, 1967, TO THE STATE OF ILLINOIS FOR THE USE OF DEPARTMENT OF PUBLIC WORKS AND BUILDINGS DESCRIBED AS FOLLOWS: PART OF THE NORTHEAST QUARTER OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, BOUNDED AND FULLY DESCRIBED AS FOLLOWS: BEGINNING AT THE SOUTHWEST CORNER OF SAID NORTHEAST QUARTER, THENCE EASTERLY ALONG THE SOUTH LINE OF SAID NORTHEAST QUARTER, 309.00 FEET; THENCE NORTHERLY PARALLEL TO THE EAST LINE OF SAID NORTHEAST QUARTER, 74.81 FEET; THENCE WESTERLY TOWARD A POINT ON THE WEST LINE OF SAID NORTHEAST QUARTER, SAID POINT BEING 75.00 FEET NORTHERLY FROM THE SOUTHWEST CORNER OF SAID NORTHEAST QUARTER, 217.17 FEET TO A POINT OF CURVE TOWARD THE SOUTH; THENCE ON SAID CURVE TO THE SOUTH, THE RADIUS OF WHICH IS 17,263.74 FEET, 91.83 FEET TO A POINT ON THE WEST LINE OF SAID NORTHEAST QUARTER, THIS POINT BEING 74.76 FEET NORTHERLY FROM THE SOUTHWEST CORNER OF SAID NORTHEAST QUARTER; THENCE SOUTHERLY ON THE WEST LINE OF SAID NORTHEAST QUARTER, 74.76 FEET TO SAID POINT OF BEGINNING), IN LAKE COUNTY, ILLINOIS.

PARCEL 2:

THE EAST 462.0 FEET OF THE SOUTH 1410.30 FEET (AS MEASURED ALONG THE EAST AND SOUTH LINES, RESPECTIVELY) OF THE NORTH WEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, (EXCEPT THE SOUTH 1079.60 FEET THEREOF), ALL IN LAKE COUNTY, ILLINOIS, AND THE WEST 420.5 FEET OF THE EAST 882.50 FEET OF THE SOUTH 1410.30 FEET (AS MEASURED ALONG THE EAST AND SOUTH LINES, RESPECTIVELY) OF THE NORTH WEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN (EXCEPT THAT PART DESCRIBED AS FOLLOWS, TO-WIT: BEGINNING AT THE SOUTH EAST CORNER OF THE SAID WEST 420.5 FEET OF THE EAST 882.50 FEET OF THE SOUTH 1410.30 FEET OF THE NORTH WEST 1/4 OF SECTION 8; THENCE NORTHERLY ALONG THE EAST LINE OF THE SAID WEST 420.5 FEET OF THE EAST 882.50 FEET OF THE SOUTH 1410.30 FEET OF THE NORTH WEST 1/4 OF SECTION 8 FOR A DISTANCE OF 898.19 FEET; THENCE WESTERLY ALONG A LINE TO THE WEST LINE OF THE SAID EAST 882.50 FEET OF THE NORTH WEST 1/4 OF SECTION 8 TO A POINT WHICH IS 896.80 FEET NORTHERLY OF THE SOUTH LINE OF THE SAID NORTH WEST 1/4 OF SECTION 8 (AS MEASURED ALONG THE SAID WEST LINE OF THE EAST 882.50 FEET); THENCE SOUTHERLY ALONG THE SAID WEST LINE OF THE EAST 882.50 FEET OF THE SOUTH 1410.30 FEET OF THE NORTH WEST 1/4 OF SECTION 8 FOR A DISTANCE OF 896.80 FEET TO THE SAID SOUTH LINE OF THE NORTH WEST 1/4 OF SECTION 8; THENCE EASTERLY ALONG THE SAID SOUTH LINE OF THE NORTH WEST 1/4 OF SECTION 8 FOR A DISTANCE OF 420.5 FEET TO THE POINT OF BEGINNING OF THIS EXCEPTION), IN LAKE COUNTY, ILLINOIS.

PARCEL 3:

THAT PART OF THE EAST 1/2 OF THE NORTH WEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS BEGINNING AT A POINT ON THE WEST LINE OF THE EAST 1/2 OF SAID NORTH WEST 1/4 WHICH IS 1755 FEET SOUTH OF THE NORTH WEST CORNER THEREOF; THENCE EAST ALONG THE SOUTH LINE OF THE NORTH 1755 FEET OF SAID EAST 1/2 OF THE NORTH WEST 1/4, A DISTANCE OF 453.98 FEET TO THE WEST LINE OF THE EAST 882.50 FEET OF THE NORTH WEST 1/4 OF SAID SECTION 8; THENCE NORTH ALONG SAID WEST LINE OF THE EAST 882.50 FEET, A DISTANCE OF 512.66 FEET TO THE NORTH LINE OF THE SOUTH 1410.30 FEET OF THE NORTH WEST 1/4 OF SAID SECTION 8; THENCE WEST ALONG SAID NORTH LINE OF THE SOUTH 1410.30 FEET OF THE NORTH WEST 1/4, 454.51 FEET TO THE WEST LINE OF THE EAST 1/2 OF THE NORTH WEST 1/4 OF SAID SECTION 8; THENCE SOUTH ALONG SAID WEST LINE OF THE EAST 1/2 OF SAID NORTH WEST 1/4, A DISTANCE OF 513.26 FEET TO THE POINT OF BEGINNING, IN LAKE COUNTY, ILLINOIS.

PARCEL 4:

THE WEST 432 FEET OF THE EAST 462 FEET OF THE SOUTH 701 FEET OF THE NORTHWEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, EXCEPT THEREFROM THAT PART OF THE NORTHWEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, BOUNDED AND FULLY DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE EAST LINE OF SAID NORTHWEST 1/4 SAID POINT BEING 74.76 FEET NORTH OF THE SOUTHEAST CORNER THEREOF; THENCE SOUTH ON THE EAST LINE OF SAID NORTHWEST 1/4, 74.76 FEET TO THE SOUTHEAST CORNER THEREOF; THENCE WESTERLY ON THE SOUTH LINE OF SAID NORTHWEST 1/4, 462.00 FEET; THENCE NORTHERLY PARALLEL WITH THE EAST LINE OF SAID NORTHWEST 1/4, 66.61 FEET; THENCE EASTERLY ON A CURVE VARYING TOWARD THE SOUTH, THE RADIUS OF WHICH IS 17,263.74 FEET TO SAID POINT OF BEGINNING), IN LAKE COUNTY, ILLINOIS.

PARCEL 5:

THE WEST 420.5 FEET OF THE EAST 882.5 FEET OF THAT PART OF THE EAST 1/2 OF THE NORTH WEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING SOUTH OF THE NORTH 1755 FEET THEREOF, (EXCEPT THAT PART THEREOF DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE EAST LINE OF SAID PARCEL, 66.61 FEET NORTHERLY FROM THE SOUTH LINE OF SAID NORTH WEST 1/4; THENCE SOUTHERLY ALONG SAID EAST LINE, 66.61 FEET TO THE SOUTH LINE OF SAID NORTH WEST 1/4; THENCE WESTERLY ON THE SOUTH LINE OF SAID NORTH WEST 1/4, 420.50 FEET; THENCE NORTHERLY PARALLEL TO THE EAST LINE OF SAID NORTH WEST 1/4, 47.99 FEET; THENCE EASTERLY ON A CURVE VARYING TOWARD THE SOUTH, THE RADIUS OF WHICH IS 17,263.74 FEET TO THE POINT OF BEGINNING) IN LAKE COUNTY, ILLINOIS.

PARCEL 6:

THE EAST 462.0 FEET OF THE SOUTH 1079.6 FEET OF THE NORTHWEST 1/4 OF SECTION 8, TOWNSHIP 44 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, (EXCEPT THE WEST 432.0 FEET OF THE SOUTH 701.0 FEET THEREOF AND ALSO EXCEPTING ANY PART THEREOF FALLING WITHIN THE RIGHT OF WAY OF FEDERAL AID ROUTE 22 (ILLINOIS ROUTE 137)), IN LAKE COUNTY, ILLINOIS.

ILLINOIS DEPARTMENT OF TRANSPORTATION CERTIFICATE

THIS PLAT HAS BEEN APPROVED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION WITH RESPECT TO ROADWAY ACCESS PURSUANT TO PARAGRAPH 2 OF "AN ACT TO REVISE THE LAW IN RELATION TO PLATS" AS AMENDED, A PLAN THAT MEETS THE REQUIREMENTS CONTAINED IN THE DEPARTMENTS "POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS" WILL BE REQUIRED BY THE DEPARTMENT.

DIRECTOR OF HIGHWAYS
REGION ONE ENGINEER



BASIS OF BEARINGS

North American Datum of 1983 (2011)
Illinois State Plane East Zone (1201)



Table with 6 columns: No., DATE, REVISION DESCRIPTION, Scale, Drawn by, Checked by, Date, Project No., Sheet No. Row 1: 1, 3/31/25, REVISED OUTLOT GEOMETRY, 1"=60', M/GJ, BAS, 01/06/25, 168247001, 6 OF 6



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PRELIMINARY ENGINEERING PLANS

GREENWAY CHASE

610 PETERSON ROAD
LIBERTYVILLE, IL 60048



INS	05/19/25	VILLAGE RESUBMITTAL
RNM	03/28/25	VILLAGE RESUBMITTAL
DC	03/07/25	ARC RE-SUBMITTAL
DATE		REVISIONS

UTILITY AND GOVERNING AGENCY CONTACTS

PUBLIC WORKS DEPARTMENT
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EMAIL: JCOOPER@LIBERTYVILLE.COM

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LIBERTYVILLE, IL 60048
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STORM SEWER SERVICE
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FIRE DEPARTMENT
VILLAGE OF LIBERTYVILLE
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LIBERTYVILLE, IL 60048
TEL: (847) 362-5664

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SCHAUMBURG, IL 60196
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COMED
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NATURAL GAS COMPANY
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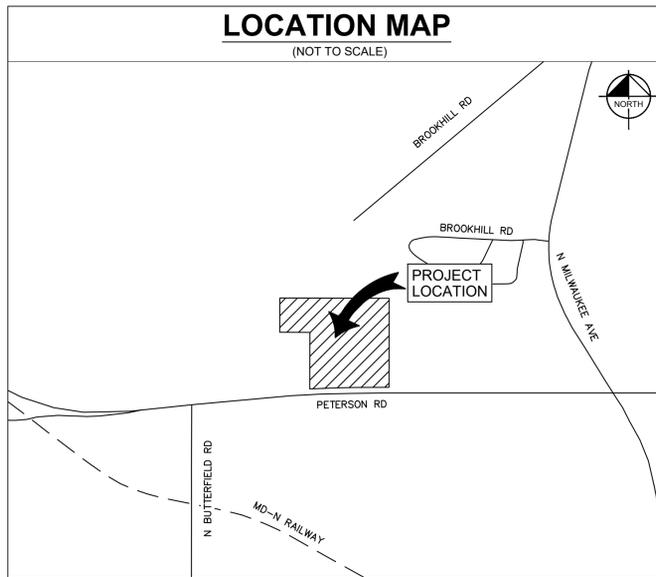
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CONTACT: DANIEL J. KRILL

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Sheet List Table	
Sheet Number	Sheet Title
C0.0	COVER SHEET
C1.0	EXISTING CONDITIONS & DEMOLITION PLAN
C2.0	OVERALL SITE PLAN
C3.0	OVERALL GRADING & DRAINAGE PLAN
C3.1	PRELIMINARY GRADING & DRAINAGE PLAN
C3.2	PRELIMINARY GRADING & DRAINAGE PLAN
C3.3	PRELIMINARY GRADING & DRAINAGE PLAN
C3.4	PRELIMINARY GRADING & DRAINAGE PLAN
C3.5	PRELIMINARY GRADING & DRAINAGE PLAN
C3.6	PRELIMINARY GRADING & DRAINAGE PLAN
C4.0	UTILITY PLAN



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SCALE: AS NOTED
DESIGNED BY: INS
DRAWN BY: KTRM
CHECKED BY: RNM

PULTE HOME
COMPANY, LLC

COVER SHEET

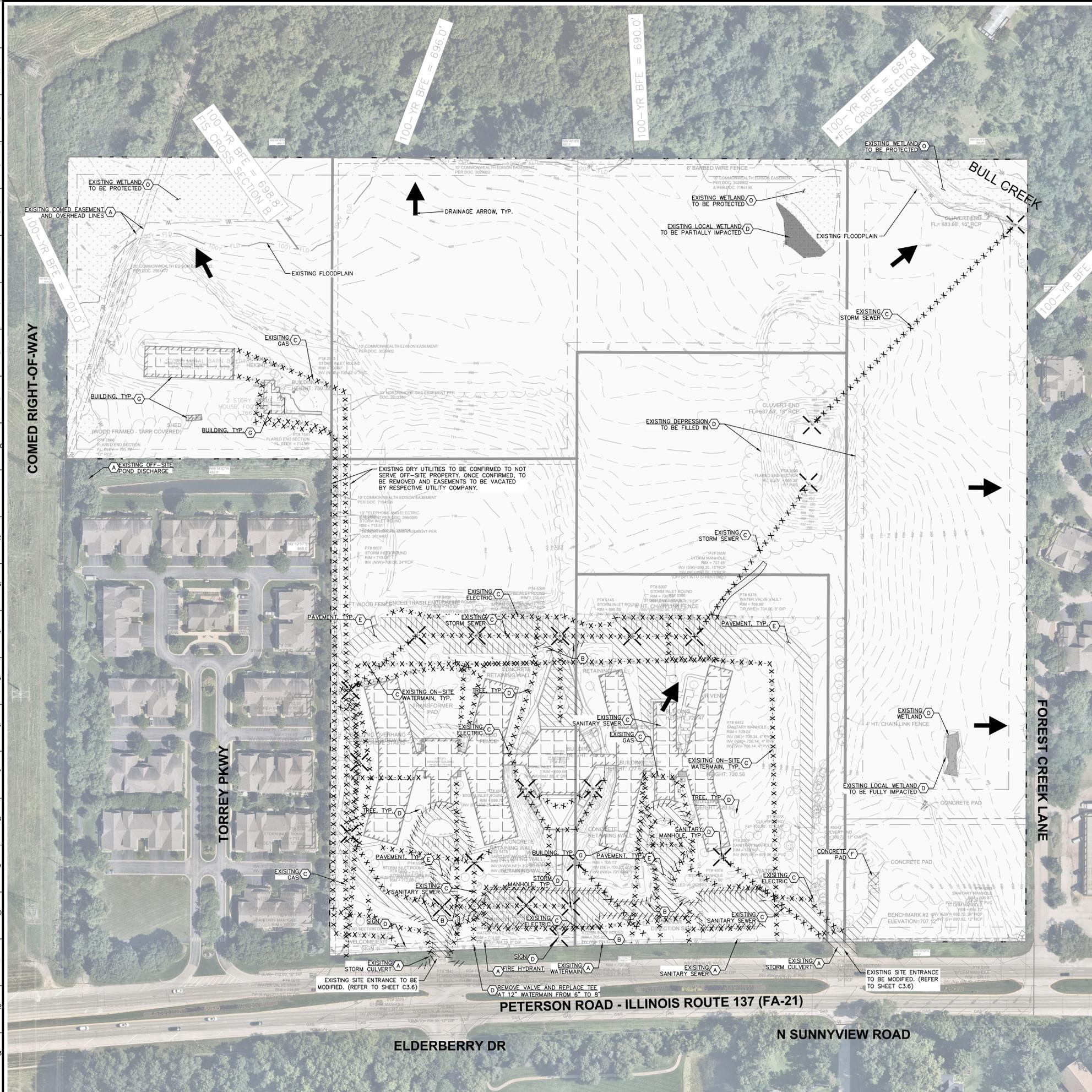
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ORIGINAL ISSUE:
01/03/2025
KHA PROJECT NO.
168247001

SHEET NUMBER
C0.0

Drawing name: K:\CHS_DEVA\168247001_Pulte_Libertyville_IL\2_Design\CAD\PlanSheets\C0.0 COVER SHEET.dwg CO.0 May 14, 2025 4:50pm by: Ian.Spence
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Drawing name: K:\GIS\DEV\168247001_Pulte_Libertyville_IL_V2_Design\CAD\PlanSheets\C1.0 EXISTING CONDITIONS & DEMOLITION PLAN.dwg C1.0 May 14, 2025 4:50pm by: an.singh
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WARNING: CONTRACTOR TO VERIFY PRESENCE AND EXACT LOCATION OF ALL UTILITIES PRIOR TO CONSTRUCTION.



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GRAPHIC SCALE IN FEET
 0 40 80 160

- ### DEMOLITION NOTES
1. CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVAL OF THE EXISTING STRUCTURES, RELATED UTILITIES, PAVING, AND ANY OTHER EXISTING IMPROVEMENTS AS NOTED.
 2. CONTRACTOR IS TO REMOVE AND DISPOSE OF ALL DEBRIS, RUBBISH AND OTHER MATERIALS RESULTING FROM PREVIOUS AND CURRENT DEMOLITION OPERATIONS. DISPOSAL WILL BE IN ACCORDANCE WITH ALL LOCAL, STATE AND/OR FEDERAL REGULATIONS GOVERNING SUCH OPERATIONS.
 3. THE GENERAL CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT. THE CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR ANY DAMAGES TO THE ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASES OF THIS PROJECT. CONTRACTOR SHALL NOT DEMOLISH ANYTHING OUTSIDE THE OWNERS LEASE/PROPERTY LINE UNLESS SPECIFICALLY MENTIONED ON THIS SHEET.
 4. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED UPON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES.
 5. IF DEMOLITION OR CONSTRUCTION ON SITE WILL INTERFERE WITH THE ADJACENT PROPERTY OWNER'S TRAFFIC FLOW, THE CONTRACTOR SHALL COORDINATE WITH ADJACENT PROPERTY OWNER, TO MINIMIZE THE IMPACT ON TRAFFIC FLOW. TEMPORARY RE-ROUTING OF TRAFFIC IS TO BE ACCOMPLISHED BY USING IDOT APPROVED TRAFFIC BARRICADES, BARRELS, AND/OR CONES. TEMPORARY SIGNAGE AND FLAGMEN MAY BE ALSO NECESSARY.
 6. QUANTITIES DEPICTED ON THIS SHEET SHALL SERVE AS A GUIDE ONLY. CONTRACTOR TO VERIFY ALL DEMOLITION QUANTITIES.
 7. REFER TO GEOTECHNICAL REPORT PROVIDED BY OTHERS FOR ALL SUBSURFACE INFORMATION.
 8. CONTRACTOR SHALL BEGIN CONSTRUCTION OF ANY LIGHT POLE BASES FOR RELOCATED LIGHT FIXTURES AND RELOCATION OF ELECTRICAL SYSTEM AS SOON AS DEMOLITION BEGINS. CONTRACTOR SHALL BE AWARE THAT INTERRUPTION OF POWER TO ANY LIGHT POLES OR SIGNS SHALL NOT EXCEED 24 HOURS.
 9. EROSION CONTROL MUST BE ESTABLISHED PRIOR TO ANY WORK ON SITE INCLUDING DEMOLITION.
 10. THE EXTENT OF SITE DEMOLITION WORK IS AS SHOWN ON THE CONTRACT DOCUMENTS AND AS SPECIFIED HEREIN.
 11. CONTRACTOR MUST RECEIVE APPROVAL FROM CIVIL ENGINEER AND GEOTECHNICAL ENGINEER FOR THE MATERIAL TYPE AND USE IF CONTRACTOR DESIRES TO REUSE DEMOLISHED SITE PAVEMENT AS STRUCTURAL FILL.
 12. EXISTING UTILITIES, WHICH DO NOT SERVICE STRUCTURES BEING DEMOLISHED, ARE TO BE KEPT IN SERVICE AND PROTECTED AGAINST DAMAGE DURING DEMOLITION OPERATIONS. CONTRACTOR SHALL ARRANGE FOR SHUT-OFF OF UTILITIES SERVING STRUCTURES TO BE DEMOLISHED. CONTRACTOR IS RESPONSIBLE FOR TURNING OFF, DISCONNECTING, AND SEALING INDICATED UTILITIES BEFORE STARTING DEMOLITION OPERATIONS. EXISTING UTILITIES TO BE ABANDONED ARE TO BE CAPPED AT BOTH ENDS AND FILLED WITH FA-1 OR APPROVED EQUAL. ALL UNDERGROUND UTILITIES TO BE REMOVED ARE TO BE BACKFILLED WITH ENGINEERED FILL OR SELECT EXCAVATED MATERIAL, AS APPROVED BY THE GEOTECHNICAL ENGINEER, TO 95% OF MODIFIED PROCTOR DENSITY WITHIN PAVED AREAS AND TO 90% OF MODIFIED PROCTOR DENSITY FOR GREEN SPACE AREAS, IN ACCORDANCE WITH THE EARTHWORK SPECIFICATIONS. ALL PRIVATE UTILITIES (ELECTRIC, CABLE, TELEPHONE, FIBER OPTIC, GAS) SHALL BE REMOVED AND RELOCATED PER THE UTILITY OWNER AND THE LOCAL MUNICIPALITY'S REQUIREMENTS.
 13. UNDERGROUND UTILITIES SHOWN ARE BASED ON ATLAS AND AVAILABLE INFORMATION PRESENTED AT THE TIME OF SURVEY. CONTRACTOR SHOULD CALL 'JULIE' (1-800-892-0123) TO COORDINATE FIELD LOCATIONS OF EXISTING UNDERGROUND UTILITIES BEFORE ORDERING MATERIALS OR COMMENCING CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES IMMEDIATELY. CONTRACTOR SHALL LOCATE AND PROTECT EXISTING UNDERGROUND AND OVERHEAD UTILITIES DURING CONSTRUCTION. UTILITY PROTECTION SHALL BE COORDINATED WITH THE RESPECTIVE UTILITY OWNER AND AS DIRECTED BY THE GOVERNING MUNICIPALITY. DAMAGED CABLES/CONDUITS SHALL BE REPLACED IMMEDIATELY. ALL EXISTING STRUCTURES TO REMAIN SHALL BE PROTECTED THROUGHOUT THE CONSTRUCTION PROCESS. ALL DAMAGED STRUCTURES SHALL BE REPLACED IN-KIND AND THEIR REPLACEMENT COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. PROPER NOTIFICATION TO THE OWNERS OF THE EXISTING UTILITIES SHALL BE MADE AT LEAST 48 HOURS BEFORE CONSTRUCTION COMMENCES.
 14. USE WATER SPRINKLING, TEMPORARY ENCLOSURES, AND OTHER SUITABLE METHODS TO LIMIT DUST AND DIRT RISING AND SCATTERING IN THE AIR TO THE LOWEST LEVEL. COMPLY WITH ALL GOVERNING REGULATIONS PERTAINING TO ENVIRONMENTAL PROTECTION. SEE EROSION CONTROL SHEETS FOR FURTHER EROSION CONTROL REQUIREMENTS.
 15. COMPLETELY FILL BELOW-GRADE AREAS AND VOIDS RESULTING FROM DEMOLITION STRUCTURES TO THE FINAL LINES AND GRADES SHOWN ON THE CONTRACT DOCUMENTS. BACKFILL MATERIAL SHALL BE IDOT APPROVED CRUSHED LIMESTONE (CA-6) OR APPROVED EQUAL. USE SATISFACTORY SOIL MATERIALS CONSISTING OF STONE, GRAVEL AND SAND, FREE FROM DEBRIS, TRASH, FROZEN MATERIALS, ROOTS AND OTHER ORGANIC MATTER. PRIOR TO PLACEMENT OF FILL MATERIALS, ENSURE THAT AREAS TO BE FILLED ARE FREE OF STANDING WATER, FROST, FROZEN MATERIAL, TRASH AND DEBRIS. PLACE FILL MATERIALS IN HORIZONTAL LAYERS NOT EXCEEDING 9" IN LOOSE DEPTH. COMPACT EACH LAYER AT OPTIMUM MOISTURE CONTENT OF FILL MATERIAL TO 95% OF MODIFIED PROCTOR DENSITY UNLESS SUBSEQUENT EXCAVATION FOR NEW WORK IS REQUIRED.
 16. TREE CLEARING OPERATIONS MUST BE COMPLETED PRIOR TO IMPLEMENTATION OF EROSION CONTROL MEASURES. SELECTIVE TREE CLEARING OPERATIONS IN PRESERVED AREAS MUST BE COMPLETED PRIOR TO IMPLEMENTATION OF EROSION CONTROL MEASURES.

DEMOLITION LEGEND

(A)	ITEM TO REMAIN, PROTECT DURING CONSTRUCTION
(B)	CURB REMOVAL
(C)	UTILITY REMOVAL
(D)	ITEM TO BE REMOVED
(E)	FULL-DEPTH ASPHALT PAVEMENT REMOVAL
(F)	CONCRETE REMOVAL
(G)	BUILDING REMOVAL
(H)	ASPHALT 1.5" MILL
(I)	REMOVE & REPLACE IN-KIND
(J)	SAWCUT LINE
(K)	FENCE REMOVAL
(L)	ABANDON UTILITY IN PLACE
(M)	TREE PROTECTION FENCING
(N)	EXISTING 100-YR BASE FLOOD ELEVATION
(O)	EXISTING WETLAND

GREENWAY CHASE
 610 PETERSON ROAD
 LIBERTYVILLE, IL 60048

EXISTING CONDITIONS & DEMOLITION PLAN

Kimley»Horn
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DESIGNED BY: INS	VILLAGE RESUBMITTAL
DRAWN BY: KTRM	ARC RESUBMITTAL
CHECKED BY: RNM	ARC RE-SUBMITTAL
SCALE: AS NOTED	NO.
DATE: 05/19/25	DATE:
DATE: 03/28/25	DATE:
DATE: 03/07/25	DATE:

ORIGINAL ISSUE:
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 KHA PROJECT NO.
 168247001

SHEET NUMBER
C1.0

Drawing name: K:\GIS_DEVELOPMENT\168247001_Pulte_Libertyville_IL_V2_Design\CAD\PlanSheets\C2.0 SITE PLAN.dwg C2.0 May 14, 2025 4:51pm by: Ian.Spence
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NORTH
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 0 40 80 160

- ### GENERAL NOTES
- ALL DIMENSIONS REFER TO THE BACK OF CURB UNLESS OTHERWISE NOTED.
 - BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 - REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.
 - REFER TO LANDSCAPE ARCHITECT PLANS FOR MONUMENT SIGN DETAILS.

PROPERTY SUMMARY TABLE

SITE AREA	= 42.11 ACRES
R/W DEDICATION	= 6.55 ACRES
LOT AREA (CUMULATIVE)	= 17.69 ACRES
PARK DISTRICT DEDICATION	= 1.14 ACRES
PRIVATE AMENITY SPACE	= 1.41 ACRES
OPEN SPACE/DETENTION	= 2.92 ACRES
OPEN SPACE/UNDISTURBED AREA	= 12.40 ACRES
TOTAL LOTS PROVIDED	= 99 LOTS
UNIT COUNT	= 134 UNITS

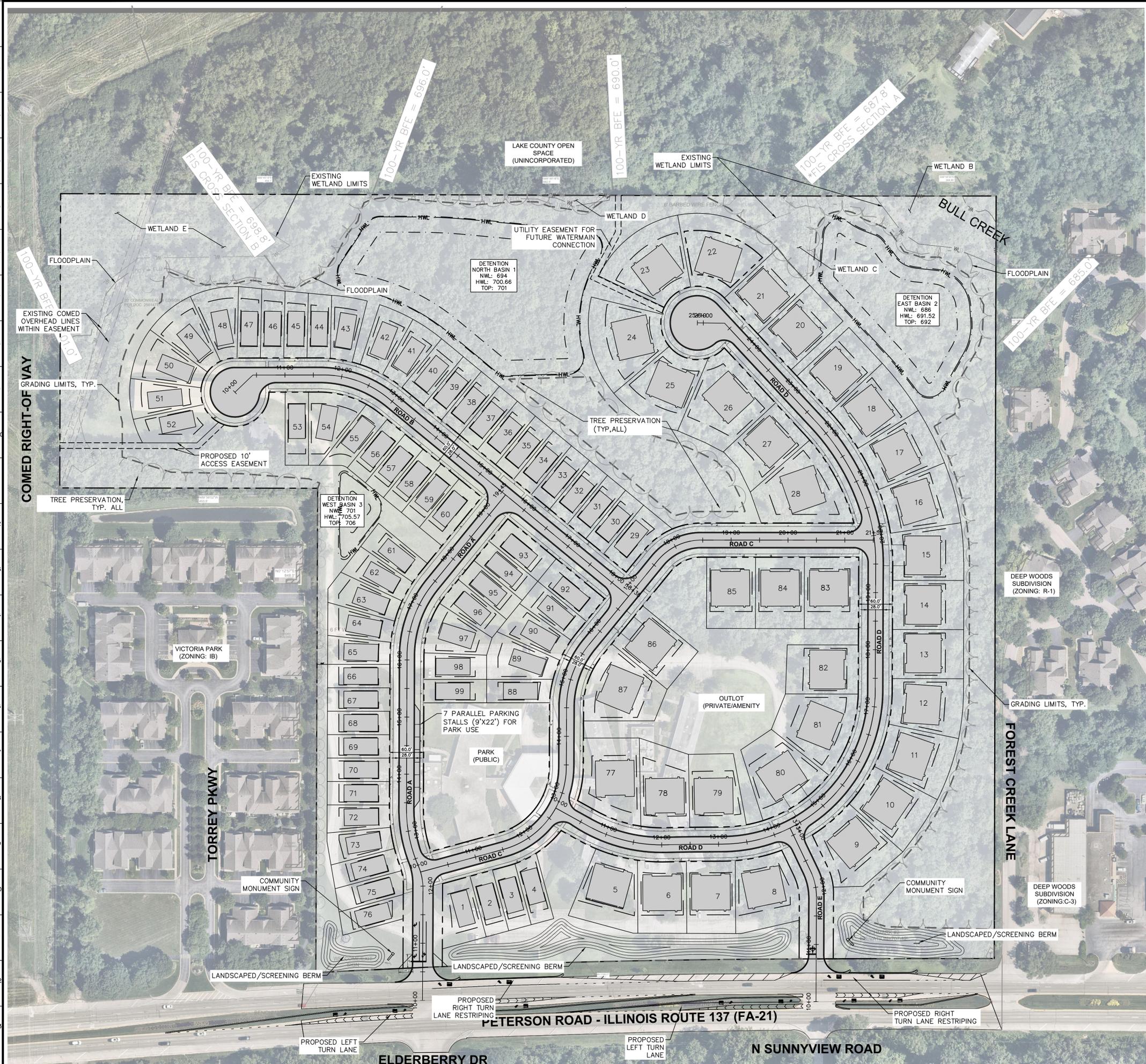
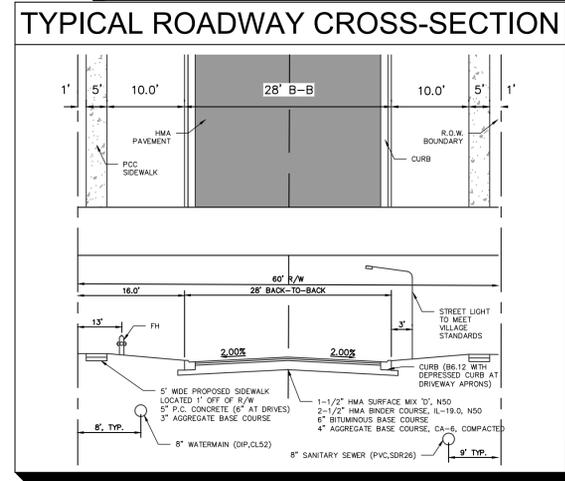
ZONING SUMMARY:
 EXISTING ZONING DISTRICT = MIXED (IB/R1/C3/UNINCORPORATED)
 PROPOSED ZONING DISTRICT = R-7 PUD

SINGLE FAMILY (41' X 110')	= 64 UNITS
GREENWAY CHASE SPRINGS	= 20 FEET
FRONT YARD SETBACK	= 20 FEET
REAR YARD SETBACK	= 15 FEET
CORNER SIDE YARD SETBACK	= 5 FEET*
SIDE YARD SETBACK	= 5 FEET*
*(MINIMUM 10 FT BUILDING SEPARATION)	
DUPLEXES (86' X 125')	= 70 UNITS
GREENWAY CHASE LANDINGS	= 20 FEET
FRONT YARD SETBACK	= 20 FEET
REAR YARD SETBACK	= 30 FEET
CORNER SIDE YARD SETBACK	= 15 FEET
SIDE YARD SETBACK	= 6 FEET*
*(MINIMUM 12 FT BUILDING SEPARATION)	

MAXIMUM BUILDING COVERAGE SINGLE FAMILY	= 45%
MAXIMUM IMPERVIOUS COVERAGE SINGLE FAMILY	= 50%
MAXIMUM BUILDING COVERAGE DUPLEX	= 50%
MAXIMUM IMPERVIOUS COVERAGE DUPLEX	= 55%

FEMA NOTE

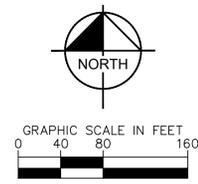
PER FLOOD INSURANCE RATE MAP PANEL NO. 17097C0161K EFFECTIVE DATE 9/19/2013, A PORTION OF THE SITE IS LOCATED IN ZONE "AE" WITH A VARYING BASE FLOOD ELEVATION RANGING FROM 701.0 ON THE WEST SIDE TO 684.0 ON THE EAST SIDE. AS NOTED HEREIN, THE REMAINDER OF THE SITE IS LOCATED WITHIN ZONE "X" (AREA OF MINIMAL FLOOD HAZARD).



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OVERALL SITE PLAN	GREENWAY CHASE <small>610 PETERSON ROAD LIBERTYVILLE, IL 60048</small>
<small>SCALE: AS NOTED DESIGNED BY: INS DRAWN BY: KTRM CHECKED BY: RNM</small>	<small>ORIGINAL ISSUE: 01/03/2025 KHA PROJECT NO. 168247001 SHEET NUMBER C2.0</small>
<small>REVISIONS</small>	<small>DATE</small>

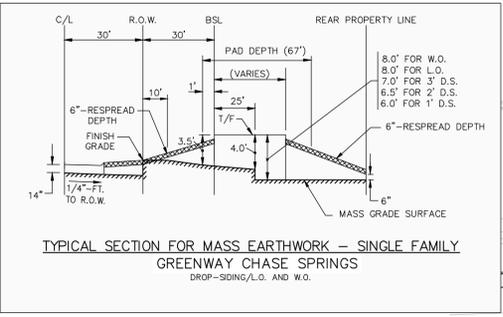
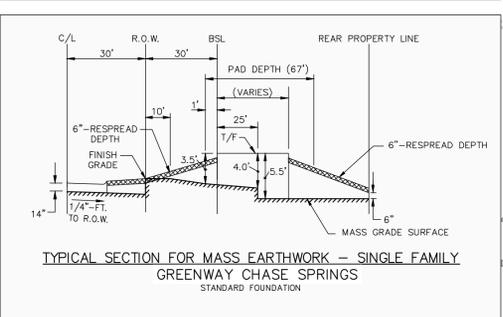
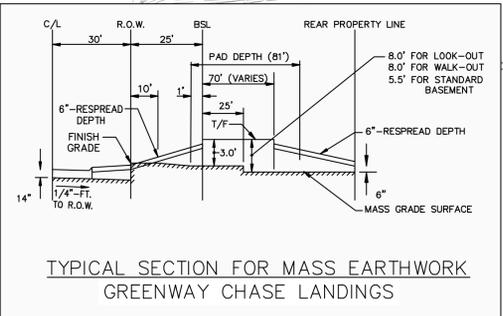
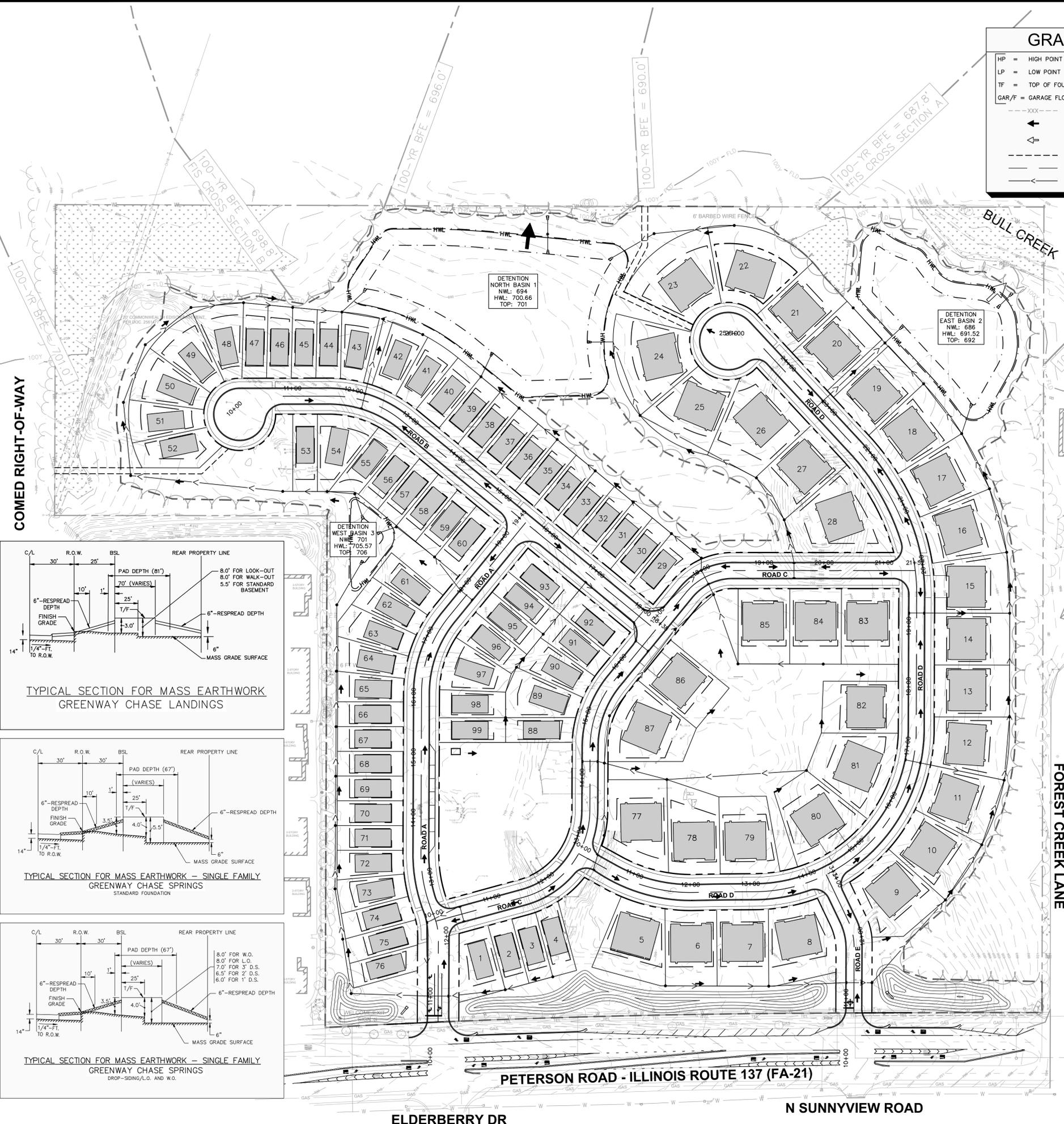
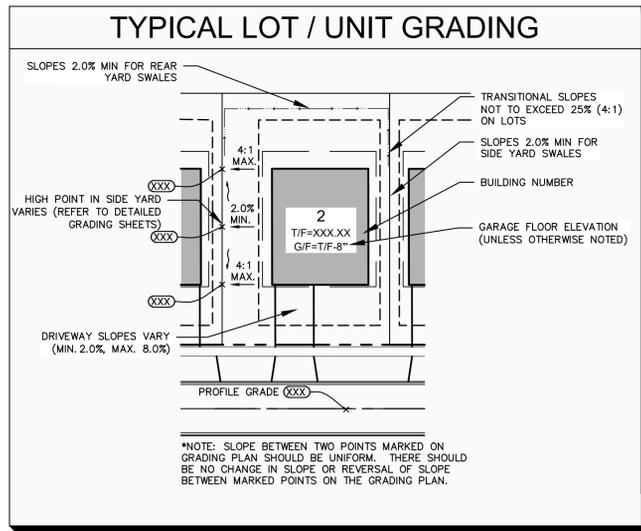
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GRADING LEGEND	
HP =	HIGH POINT
LP =	LOW POINT
TF =	TOP OF FOUNDATION
GAR/F =	GARAGE FLOOR ELEVATION
---	EXISTING CONTOUR
→	100-YEAR OVERLAND OVERFLOW ROUTE
⇄	DETENTION BASIN 100-YEAR EMERGENCY OVERLAND OVERFLOW ROUTE
- - -	EASEMENT LINE
---	BUILDING SETBACK LINE
→	PROPOSED STORM SEWER LINE

- ### GRADING NOTES
- CONTRACTOR TO VERIFY ALL EXISTING TOPOGRAPHY AND STRUCTURES IDENTIFIED WITHIN THE TOPOGRAPHIC EXHIBIT AND IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES PRIOR TO STARTING WORK.
 - ALL PAVEMENT SPOT GRADE ELEVATIONS AND RIM ELEVATIONS WITHIN OR ALONG CURB AND GUTTER REFER TO FLOW LINE ELEVATIONS UNLESS OTHERWISE NOTED.
 - ALL CURB ELEVATIONS ARE TO BE TOP OF CURB. ALL GUTTER ELEVATIONS ARE 0.25' BELOW TOP OF CURB ELEVATION UNLESS OTHERWISE NOTED.
 - ALL ELEVATIONS SHOWN DEPICT FINISHED GRADE UNLESS OTHERWISE NOTED. GENERAL CONTRACTOR TO COORDINATE WITH EXCAVATION, LANDSCAPE AND PAVING SUBCONTRACTORS REGARDING TOPSOIL THICKNESS FOR LANDSCAPE AREAS AND PAVEMENT SECTION THICKNESS FOR PAVED AREAS TO PROPERLY ENSURE ADEQUATE CUT TO ESTABLISH SUBGRADE ELEVATIONS.
 - NO EARTHEN SLOPE SHALL BE GREATER THAN 3:1, EXCEPT FOR WITHIN THE DETENTION BASIN OR FOR LANDSCAPE SCREENING BERMS.
 - MINIMUM SLOPE SHALL BE 2% ON ALL PRIVATE LOTS.
 - MAXIMUM RUNNING SLOPE SHALL NOT EXCEED 5% AND CROSS SLOPE SHALL NOT EXCEED 2% ON ALL SIDEWALKS AND ACCESSIBLE ROUTES.
 - CONTRACTOR TO MATCH EXISTING ELEVATIONS AT THE PROPERTY LIMITS. UNLESS OTHERWISE NOTED AS GRADING LIMITS, CONTRACTOR TO NOTIFY ENGINEER OF ANY DISCREPANCIES WITH FIELD CONDITIONS PRIOR TO GRADING.
 - WINDOW WELLS ARE NOT ALLOWED ALONG SIDE YARD OVERLAND FLOOD ROUTES.
 - ALL DISTURBED AREAS SHALL BE RESTORED WITH MINIMUM 6-INCHES OF TOPSOIL AND SEED, UNLESS OTHERWISE NOTED.
 - SUMP PUMP CONNECTIONS TO THE REAR YARD STORM SEWER ARE REQUIRED FOR ALL LOTS.
 - DRIVEWAY SLOPES FROM RIGHT-OF-WAY SHALL BE A MINIMUM OF 2% AND A MAXIMUM OF 8%. FINAL HOUSE LOCATION TO BE SET BACK UP TO 5' FROM BUILDING SETBACK LINE IF REQUIRED TO MEET MAXIMUM DRIVEWAY SLOPE.
 - ALL DRAIN TILE ENCOUNTERED DURING MASS GRADING / UTILITY WORK MUST BE CONNECTED TO THE PROPOSED STORM SEWER SYSTEM. A RECORD MUST BE KEPT, OF ANY DRAIN TILE ENCOUNTERED, TO BE INCLUDED IN RECORD DRAWINGS.
 - DO NOT INTERRUPT DRAINAGE FROM OFF-SITE DURING CONSTRUCTION OPERATIONS. PROVIDE TEMPORARY DRAINAGE DITCHES WHERE REQUIRED.
 - GRADING INDICATED MAY NEED TO BE ADJUSTED BASED ON FIELD CONDITIONS. CONTRACTOR SHALL NOTIFY ENGINEER OF ANY DISCREPANCIES WITH FIELD CONDITIONS PRIOR TO GRADING.



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3	03/07/25	DC	ARC RE-SUBMITTAL
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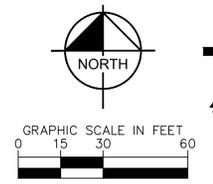
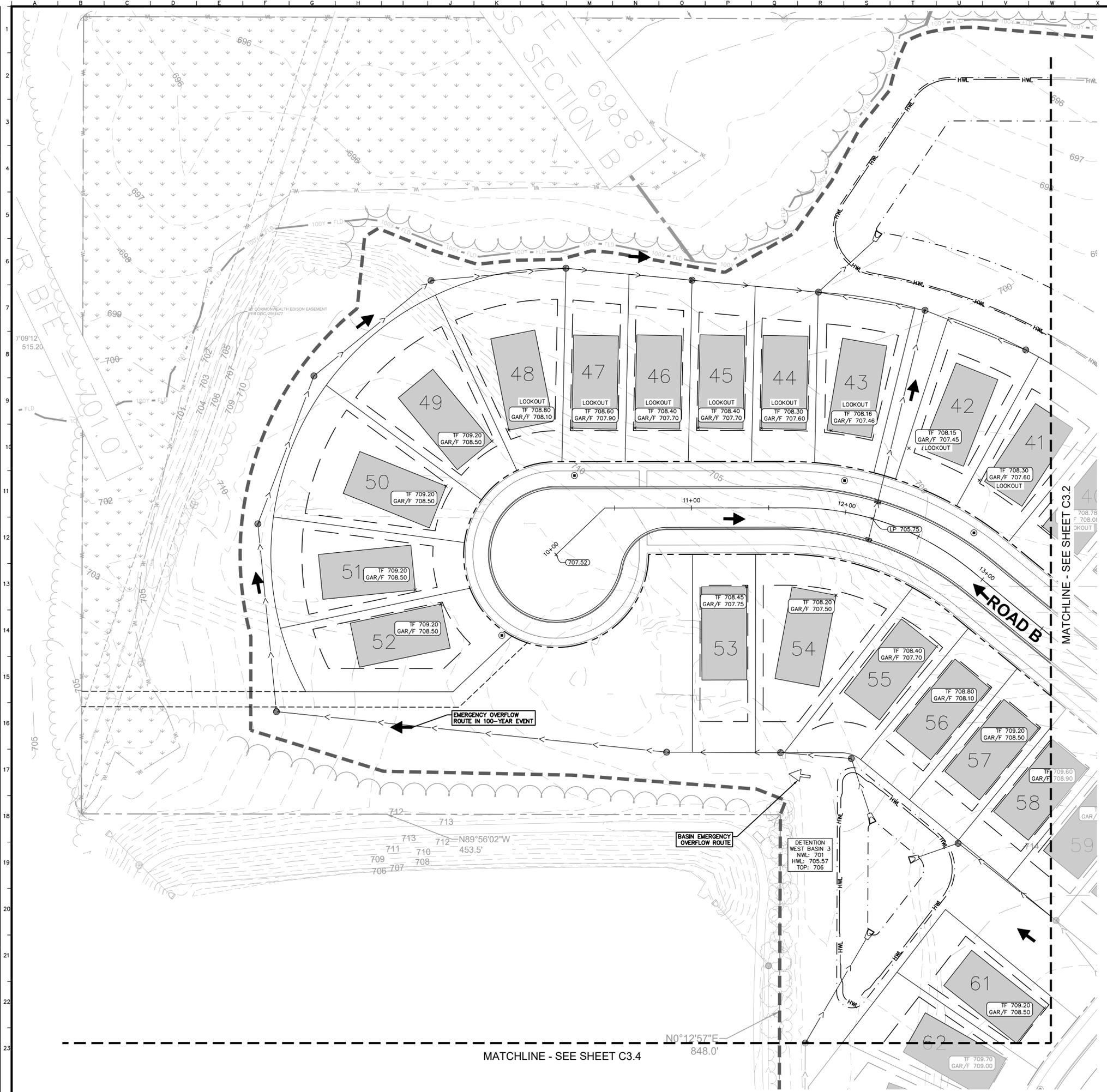
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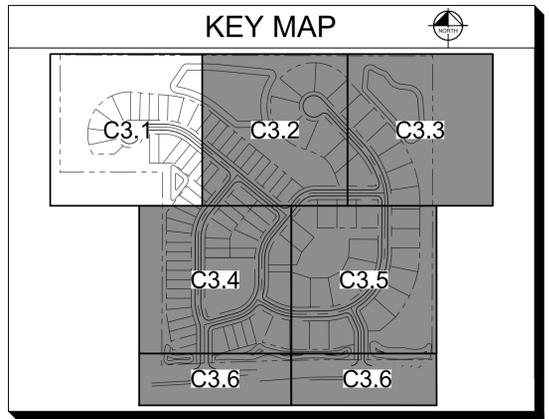
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KHA PROJECT NO.	168247001
SHEET NUMBER	C3.0

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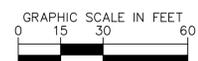
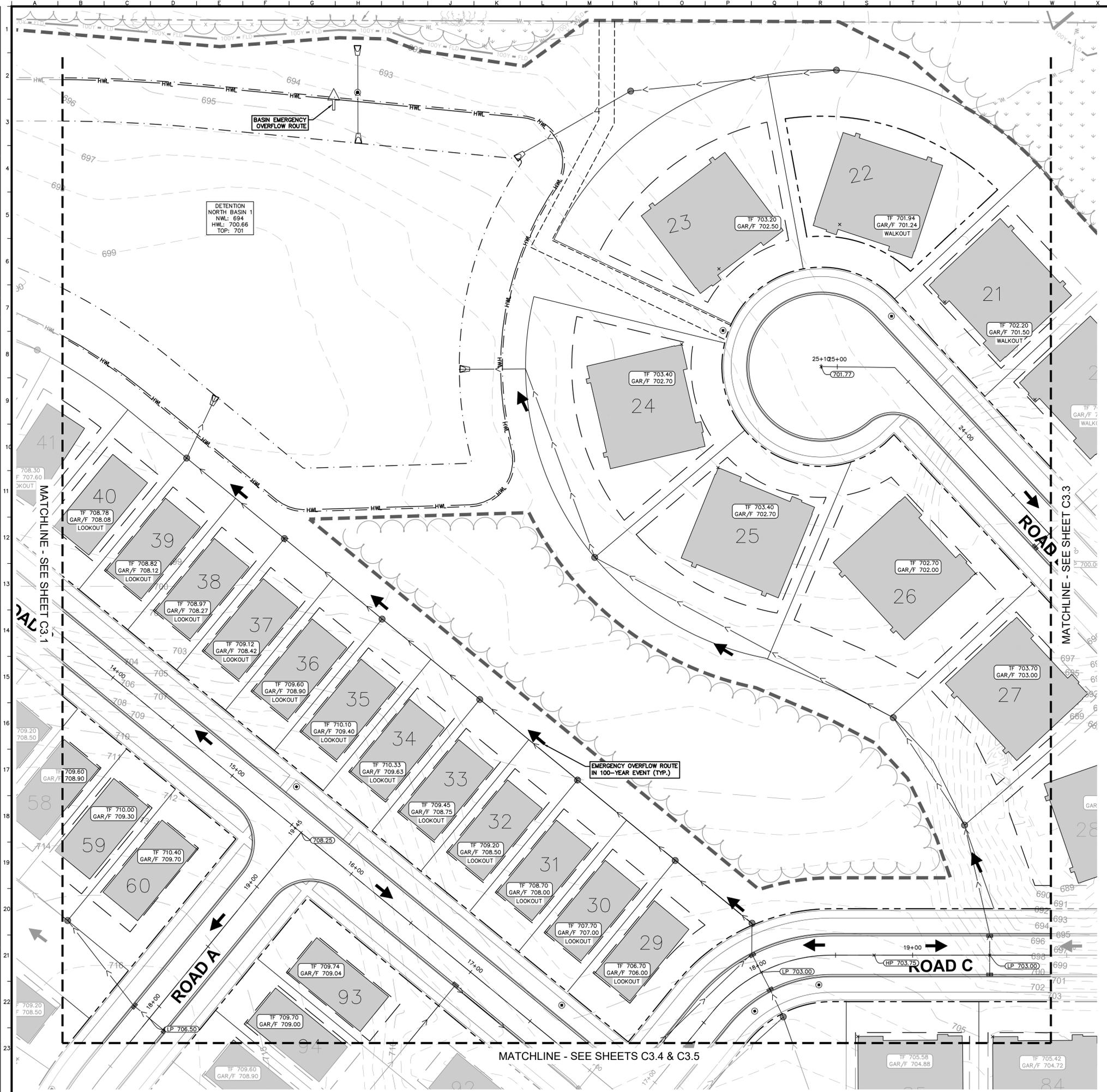
GRADING NOTES

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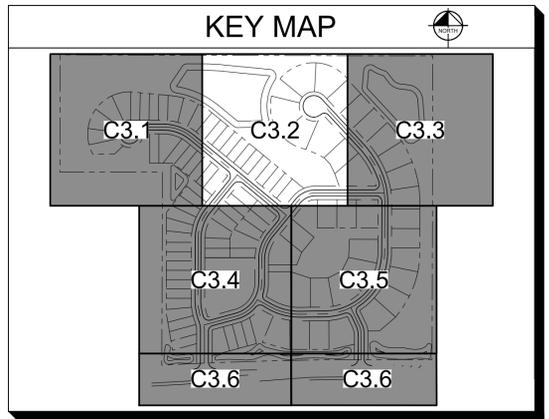
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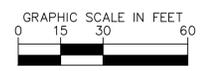
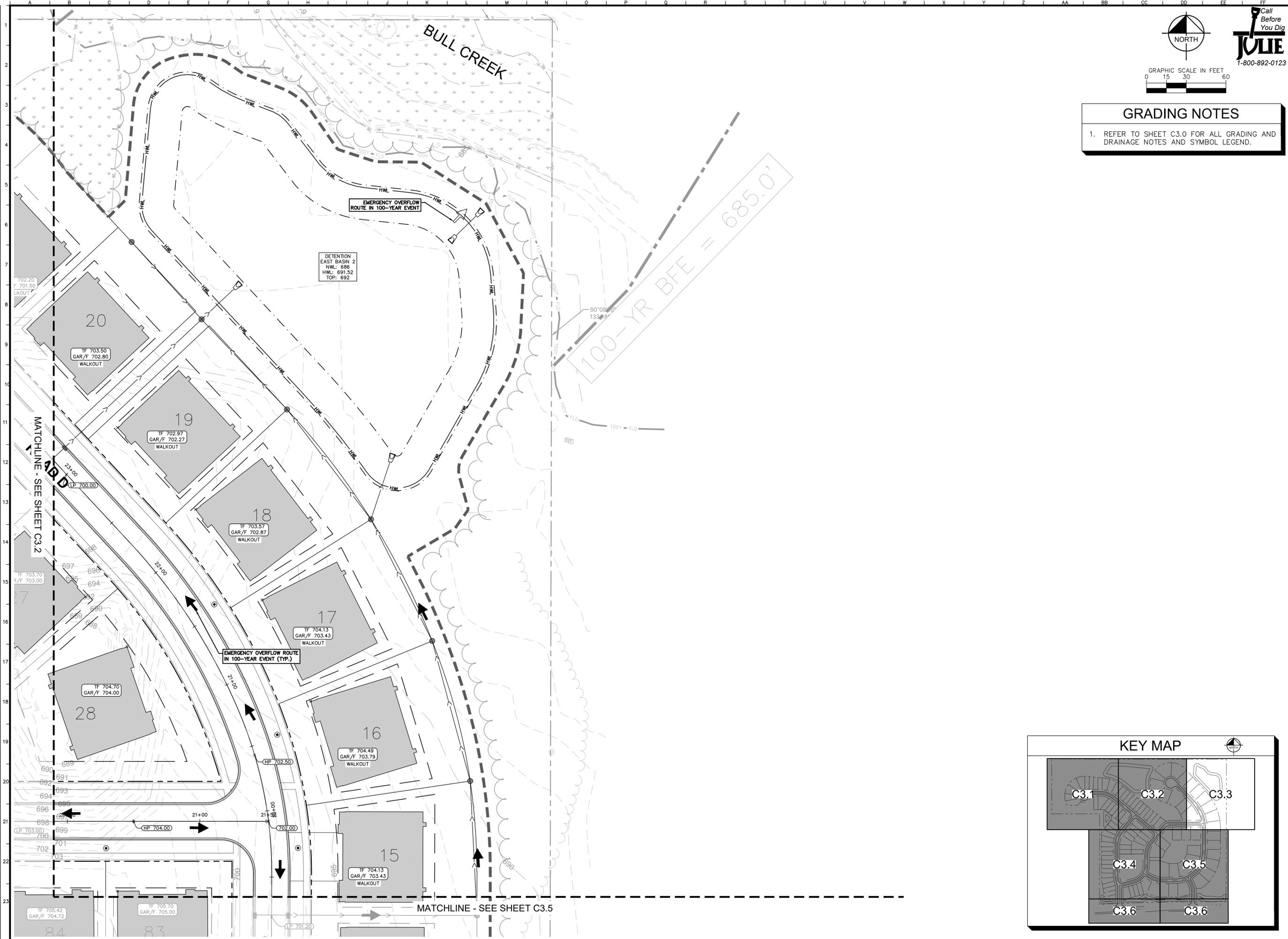
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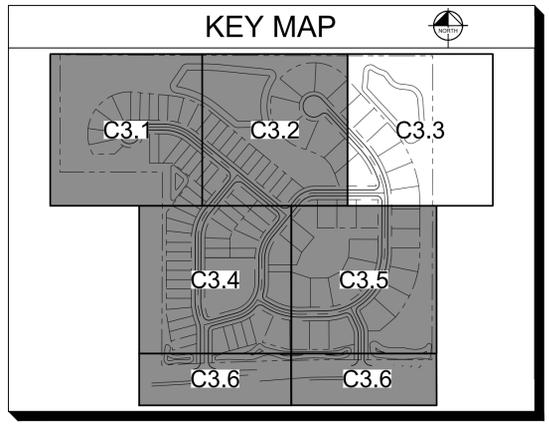
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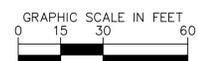
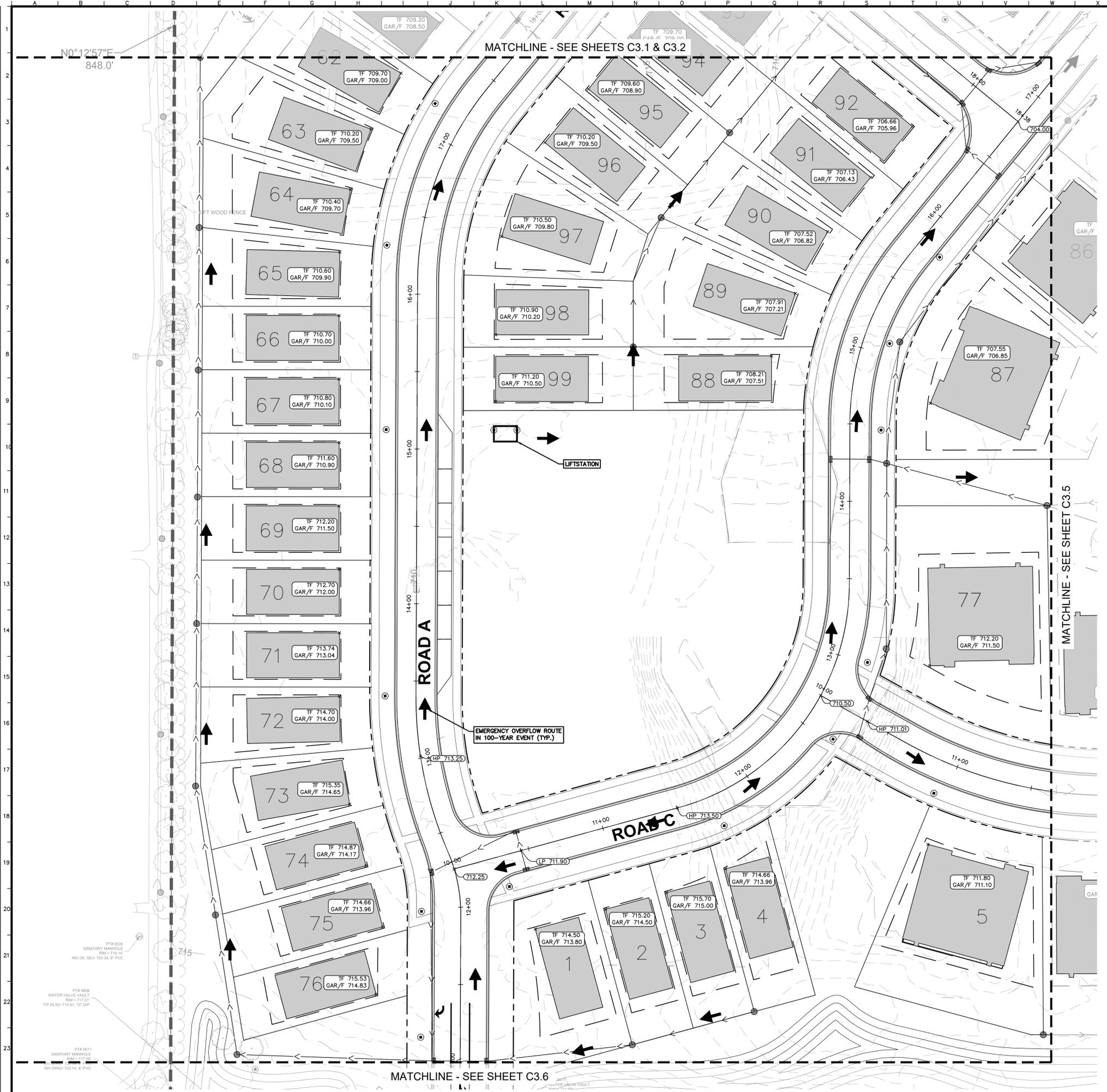
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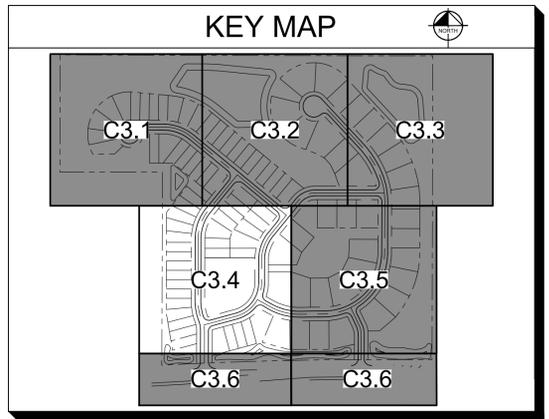
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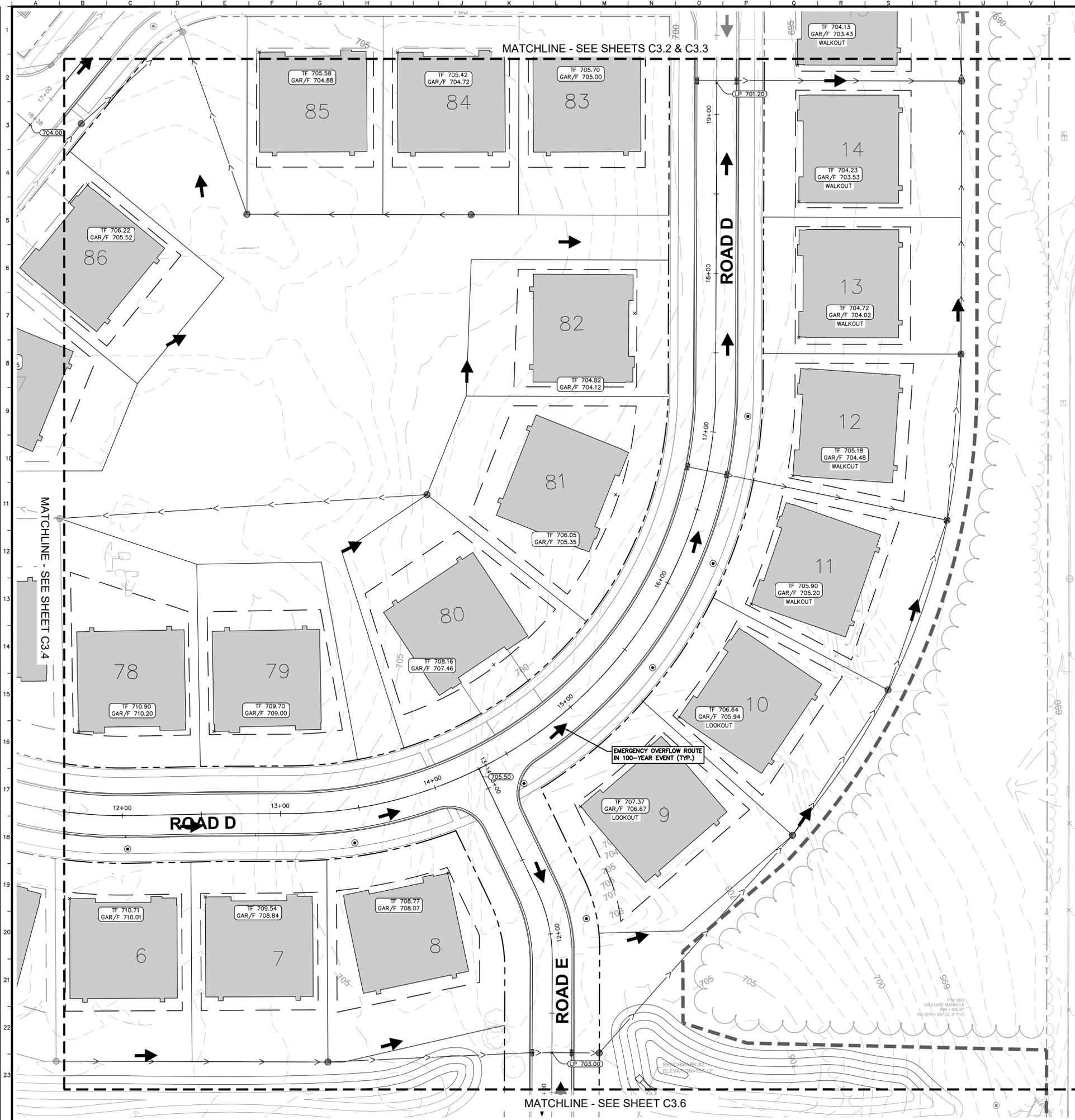
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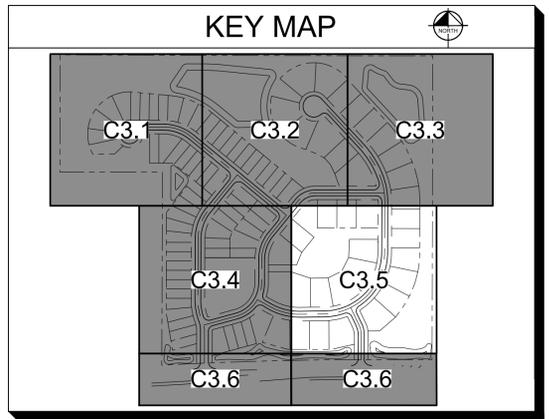
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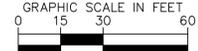
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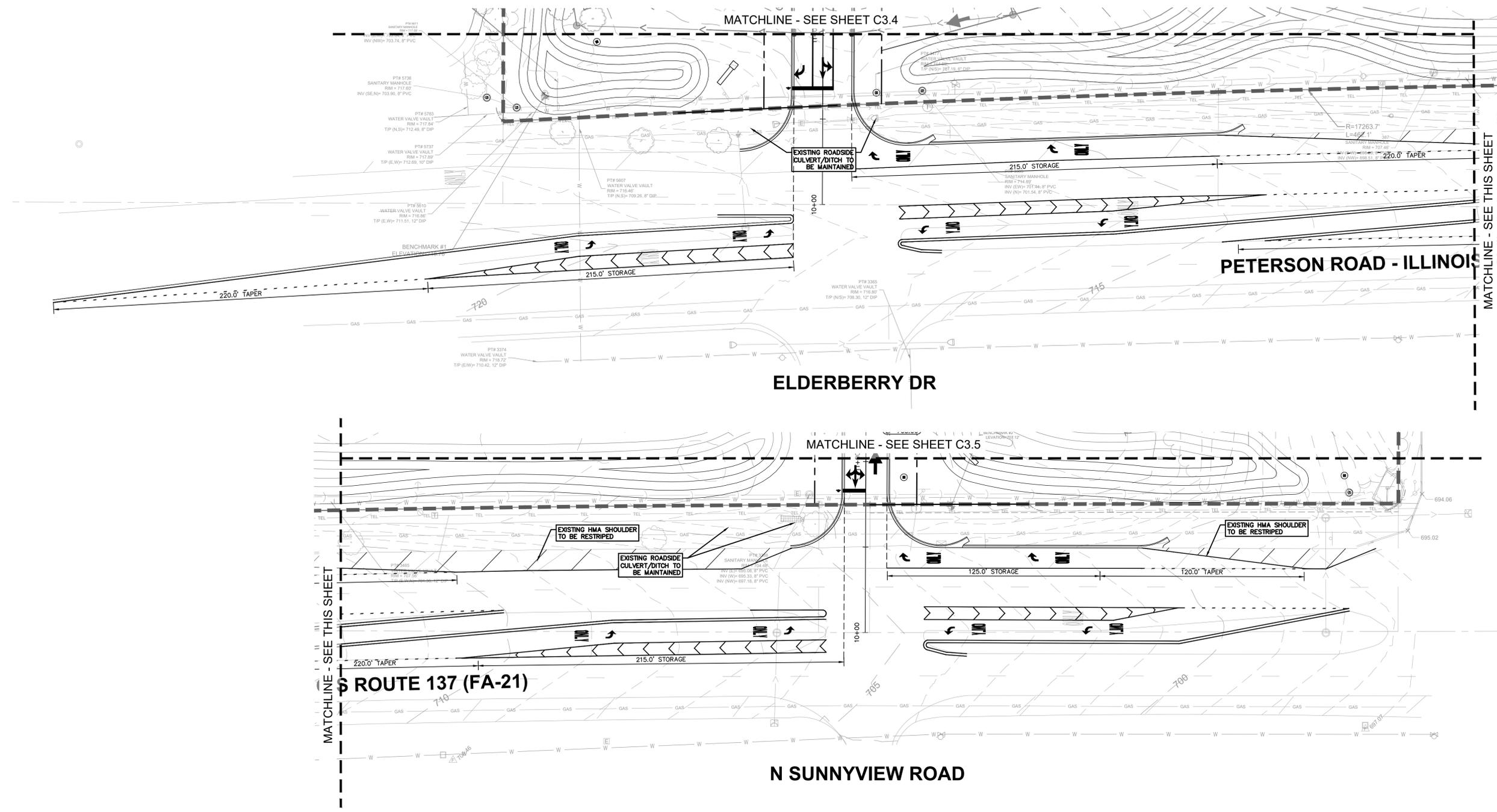
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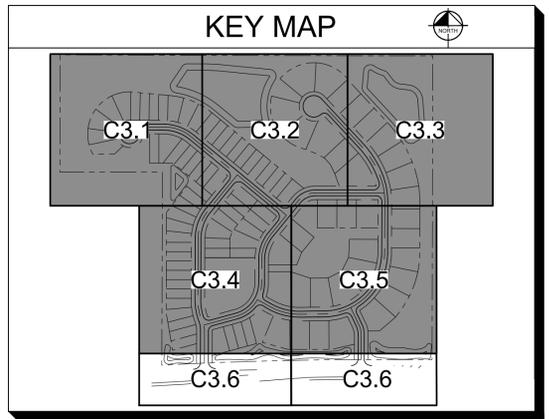
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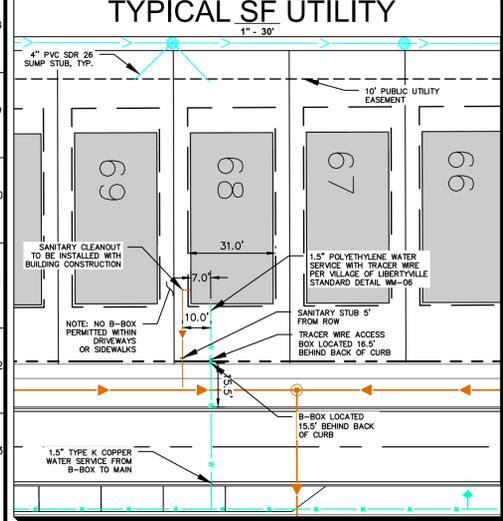
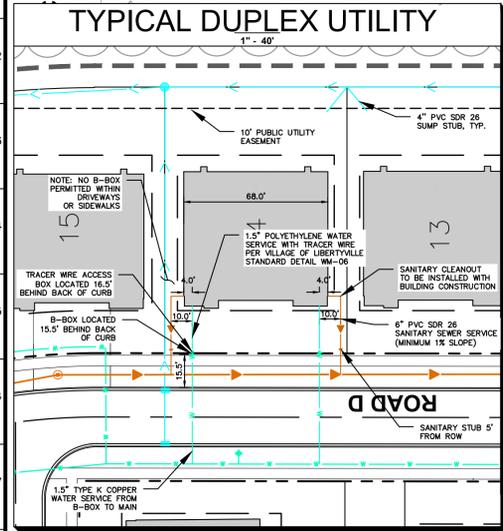
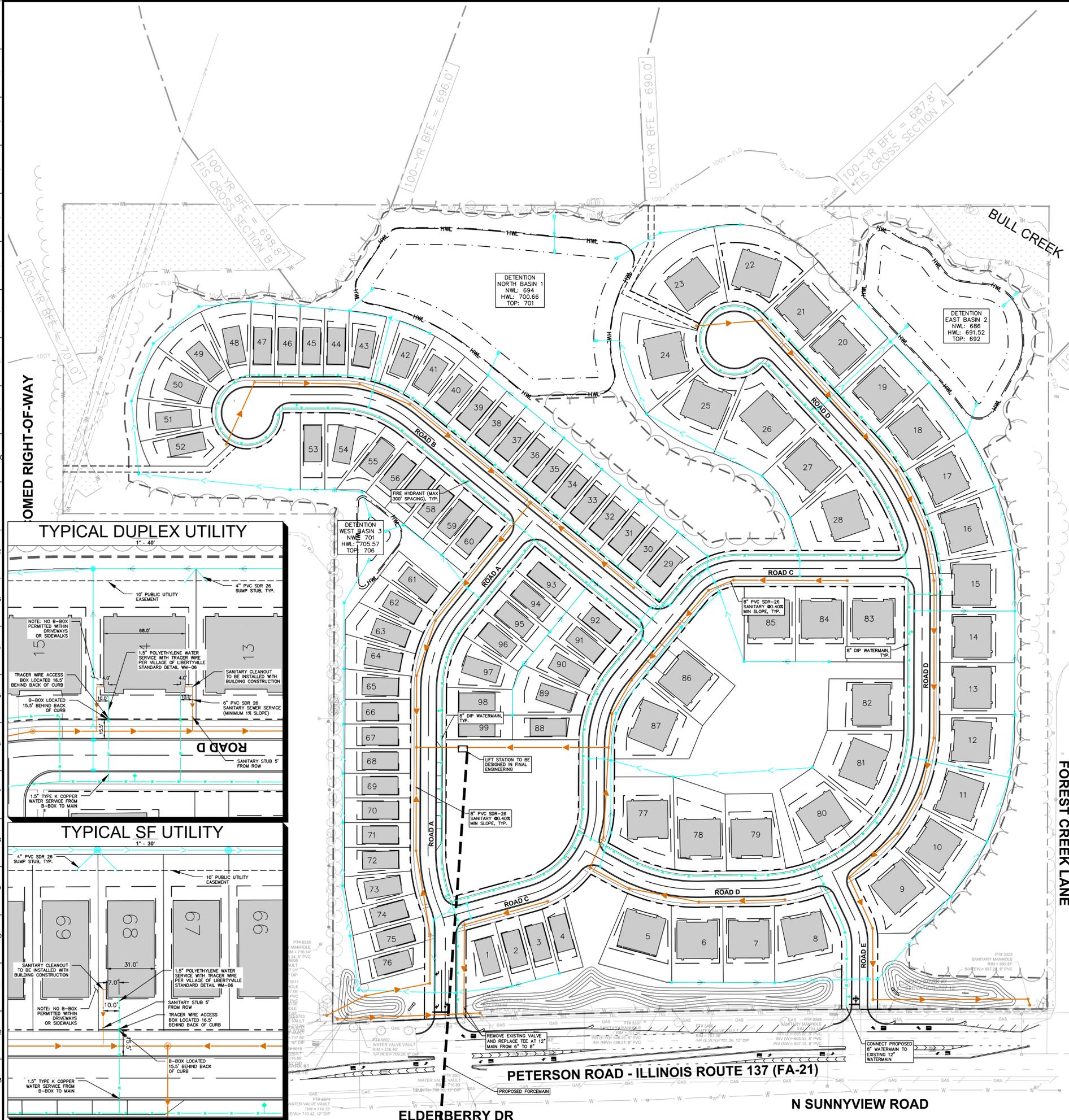
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UTILITY LEGEND

	EX. WATER LINE
	EX. STORM SEWER LINE
	EX. SANITARY SEWER LINE
	EX. HYDRANT
	EX. WATER VALVE
	EX. SANITARY MANHOLE
	EX. STORM MANHOLE/STRUCTURE
	EX. STORM INLET/CATCH BASIN
	EX. OVERHEAD ELECTRIC LINE
	EX. LIGHT POLE
	EX. WETLAND MARKER
	EX. WATER VALVE
	EX. SANITARY CLEANOUT
	PROPOSED UNDERGROUND ELECTRIC LINE
	GAS LINE (BY GAS COMPANY)
	PROPOSED PHONE LINE
	PROPOSED STORM SEWER LINE
	PROPOSED OPEN LID STORM STRUCTURE (PAVEMENT USE NEENAH R-2540) (GRASS USE NEENAH R-4340-B BEEHIVE)
	PROPOSED CLOSED LID STORM STRUCTURE (PAVEMENT USE NEENAH R-1713) (GRASS USE NEENAH R-1786)
	PROPOSED COMBINATION CURB INLET (M3.12 C&G USE NEENAH R-3501-E2) (B6.12 C&G USE NEENAH R-3281-A)
	PROPOSED FLARED END SECTION (ALL REQUIRED GRATING PER IDOT STANDARDS 542301, 542306, 542311)
	PROPOSED SANITARY SEWER LINE
	PROPOSED SANITARY MANHOLE
	PROPOSED STORM/SANITARY CLEANOUT
	PROPOSED WATER LINE
	PROPOSED VALVE VAULT
	PROPOSED VALVE BOX
	PROPOSED FIRE HYDRANT
	PROPOSED TRENCH BACKFILL
	PROPOSED LIGHT POLE
	RIP RAP (SEE DETAILS)

UTILITY NOTES

- ALL WATER LINES $\geq 3"$ SHALL BE DUCTILE IRON PIPE, CLASS 55.
- ALL SANITARY SEWER LINES SHALL BE PVC MEETING ASTM D-3034 SDR 26 EXCEPT FOR SANITARY SEWER THAT CROSSES ABOVE WATER MAIN. THIS PIPE SHALL BE AWWA C900 (UNLESS WATER MAIN CASING IS UTILIZED). PROVIDE 42" MINIMUM COVER.
- CONTRACTOR SHALL COORDINATE ANY DISRUPTIONS TO EXISTING UTILITY SERVICES WITH ADJACENT PROPERTY OWNERS.
- ALL ELECTRIC AND TELEPHONE EXTENSIONS INCLUDING SERVICE LINES SHALL BE CONSTRUCTED TO THE APPROPRIATE UTILITY COMPANY SPECIFICATIONS. ALL UTILITY DISCONNECTIONS SHALL BE COORDINATED WITH THE DESIGNATED UTILITY COMPANIES.
- CONSTRUCTION SHALL NOT START ON ANY PUBLIC UTILITY SYSTEM UNTIL WRITTEN APPROVAL HAS BEEN RECEIVED BY THE ENGINEER FROM THE APPROPRIATE GOVERNING AUTHORITY AND CONTRACTOR HAS BEEN NOTIFIED BY THE ENGINEER.
- CONTRACTOR TO CALL "JULIE" (1-800-892-0123) TO COORDINATE FIELD LOCATIONS OF EXISTING UNDERGROUND UTILITIES BEFORE ORDERING MATERIALS OR COMMENCING CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES IMMEDIATELY.
- PRIOR TO THE CONSTRUCTION OF OR CONNECTION TO ANY STORM DRAIN, SANITARY SEWER, WATER MAIN OR ANY OTHER UTILITIES, THE CONTRACTOR SHALL EXCAVATE, VERIFY AND CALCULATE ALL POINTS OF CONNECTION AND ALL UTILITY CROSSINGS AND INFORM THE ENGINEER AND THE OWNER/DEVELOPER OF ANY CONFLICT OR REQUIRED DEVIATIONS FROM THE PLAN. NOTIFICATION SHALL BE MADE A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION. THE ENGINEER AND ITS CLIENTS SHALL BE HELD HARMLESS IN THE EVENT THAT THE CONTRACTOR FAILS TO MAKE SUCH NOTIFICATION. THE MUNICIPALITY SHALL BE NOTIFIED OF ANY AND ALL CHANGES TO THE DESIGN PLANS.
- CONTRACTOR SHALL COMPLY COMPLETELY WITH THE LATEST STANDARDS OF OSHA DIRECTIVES OR ANY OTHER AGENCY HAVING JURISDICTION FOR EXCAVATION AND TRENCHING PROCEDURES. THE CONTRACTOR SHALL USE SUPPORT SYSTEMS, SLOPING, BENCHING AND OTHER MEANS OF PROTECTION. THIS IS TO INCLUDE, BUT NOT LIMITED TO ACCESS AND EGRESS FROM ALL EXCAVATION AND TRENCHING. CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH PERFORMANCE CRITERIA AS REQUIRED BY OSHA.
- CONTRACTOR TO AVOID DISRUPTION OF ANY ADJACENT TENANT'S TRAFFIC OPERATIONS DURING INSTALLATION OF UTILITIES.
- ALL DIMENSIONS ARE TO CENTERLINE OF PIPE OR CENTER OF MANHOLE UNLESS NOTED OTHERWISE.
- SEE ARCHITECTURAL AND MEP PLANS FOR EXACT UTILITY CONNECTION LOCATIONS AT BUILDING.
- LIGHT POLES SHOWN FOR COORDINATION PURPOSES ONLY AND DO NOT REPRESENT ACTUAL SIZE. SEE SITE LIGHTING PLANS BY OTHERS FOR MORE INFORMATION.
- SEE DETAILS FOR LOCATING STORM STRUCTURES WITHIN THE CURB LINE.
- STORMWATER FACILITIES MUST BE FUNCTIONAL BEFORE BUILDING CONSTRUCTION BEGINS IF REQUIRED BY AUTHORITY HAVING JURISDICTION.



INS	05/19/25	VILLAGE RESUBMITTAL
RNM	03/28/25	VILLAGE RESUBMITTAL
DC	03/07/25	ARC RE-SUBMITTAL
DATE		REVISIONS

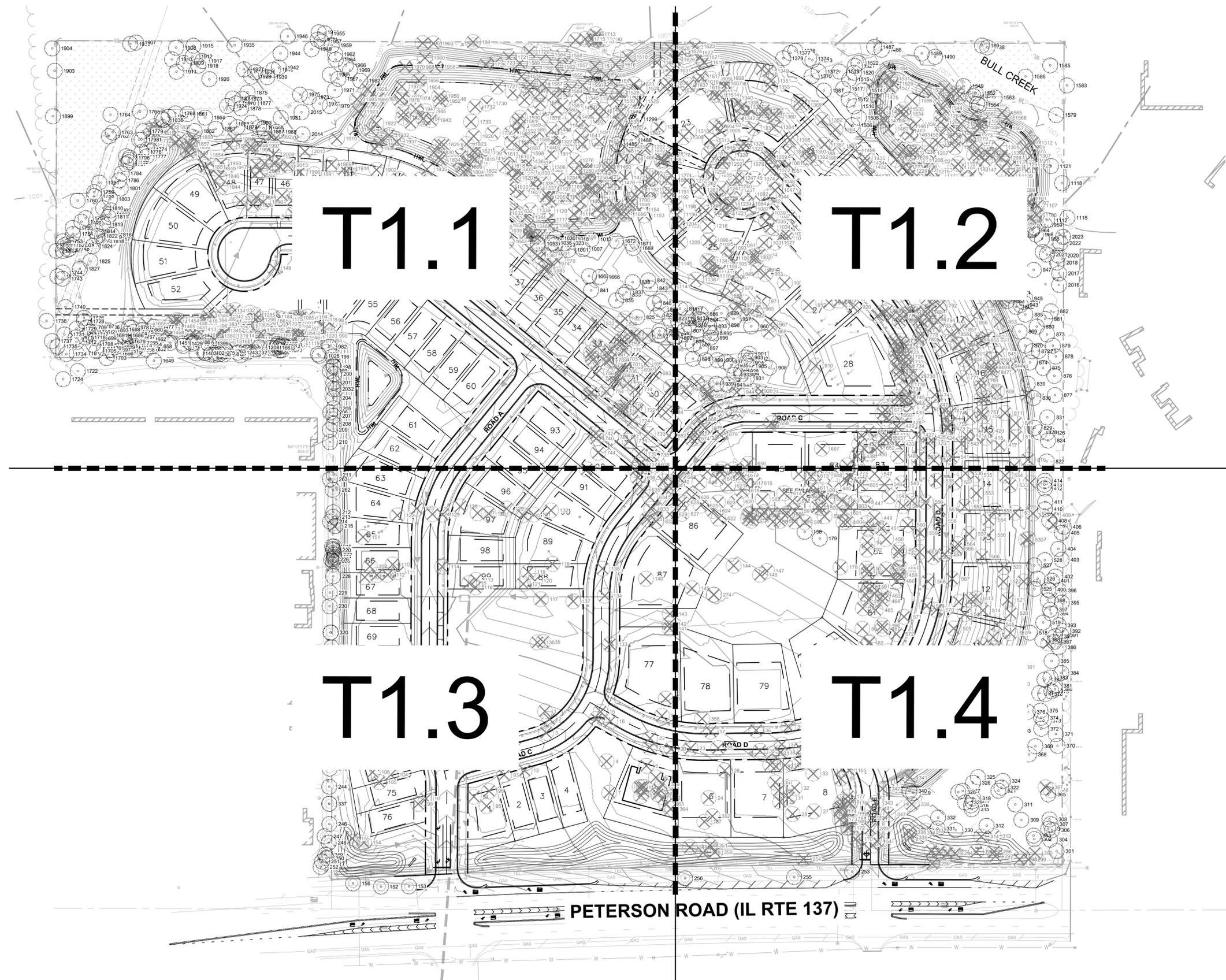
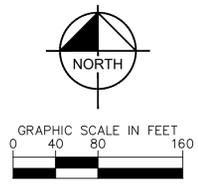
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PULTE HOME COMPANY, LLC
 UTILITY PLAN
 GREENWAY CHASE
 610 PETERSON ROAD
 LIBERTYVILLE, IL 62438

ORIGINAL ISSUE: 01/03/2025
 KHA PROJECT NO. 168247001
 SHEET NUMBER C4.0

Drawing name: K:\GIS\DEV\16827001_Libertyville_IL2_Design\DWG\16827001_TREE_PRESERVATION_PLAN.dwg TREE PRESERVATION PLAN May 16, 2025 11:30am by DanielDrow
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2	RNM	03/28/25	
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PULTE HOME COMPANY, LLC

TREE PRESERVATION PLAN

GREENWAY CHASE
610 PETERSON ROAD
LIBERTYVILLE, IL 60048

ORIGINAL ISSUE:
01/03/2025
KHA PROJECT NO.
168247001
SHEET NUMBER
T1.0

TREE PRESERVATION NOTES

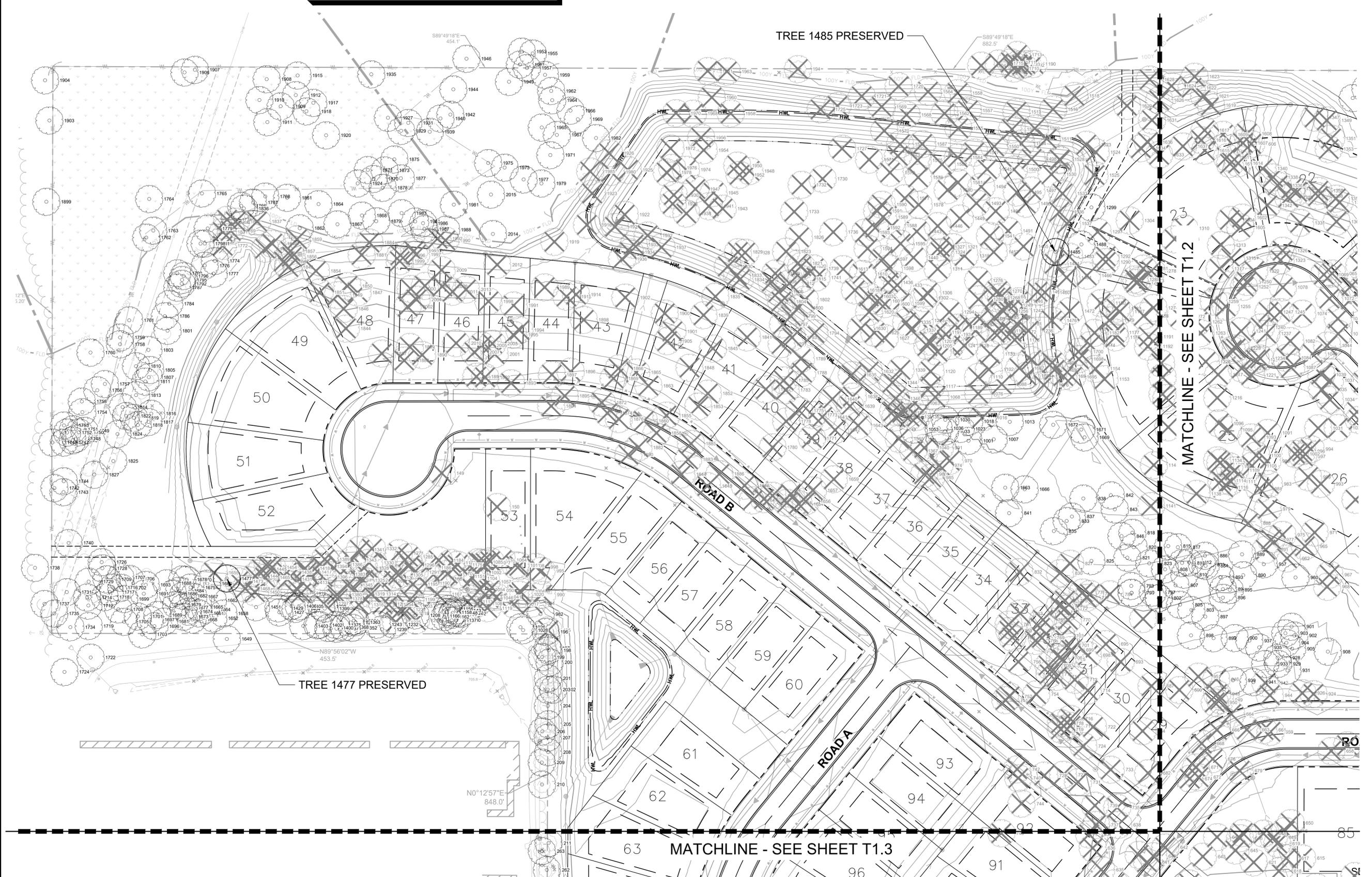
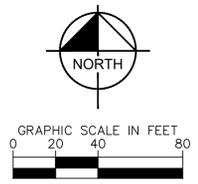
1. ALL TREES ARE TO BE PRESERVED UNLESS OTHERWISE NOTED.
2. FINAL LAYOUT OF TREE PRESERVATION FENCING IS TO BE DETERMINED IN THE FIELD.
3. REFER TO SHEET T1.5 FOR TREE TABULATION DATA.
4. REFER TO SHEET T1.5 FOR VILLAGE OF LIBERTYVILLE STANDARD TREE PROTECTION DETAIL AND REQUIREMENTS.

TREE PRESERVATION LEGEND

- EXISTING TREE TO BE PRESERVED
SEE TREE PROTECTION DETAIL ON SHEET T1.5
ASSOCIATED TREE ID #
- EXISTING TREE TO BE REMOVED
SEE TREE TABULATION DATA ON SHEET T1.5
ASSOCIATED TREE ID #
- TREE PROTECTION FENCE
SEE TREE PROTECTION DETAIL ON SHEET T1.5

TREE PRESERVATION NOTES

- 1 CONTRACTOR TO FURNISH AND INSTALL TEMPORARY TREE PROTECTION FENCE, TYP. SEE DETAIL ON SHEET T1.5
- 2 EXISTING TREE TO BE PRESERVED
- 3 EXISTING WOODLAND TO BE PRESERVED
- 4 EXISTING TREE TO BE REMOVED



Drawing name: \\GIS_DEV\68227001_Design\CAD\PlanSheets\T1.1_PlanSheets\T1.1.dwg
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 Designer: Daniel Coove
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VILLAGE RESUBMITTAL		
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TREE PRESERVATION PLAN		
GREENWAY CHASE 610 PETERSON ROAD LIBERTYVILLE, IL 60048		
ORIGINAL ISSUE: 01/03/2025		
KHA PROJECT NO. 168247001		
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Drawing name: K:\GIS\DEV\16827001_Pulte_Libertyville_IL2_Design\Drawings\T1.3 May 16, 2025 1:31pm by: Daniel Grove
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TREE PRESERVATION NOTES

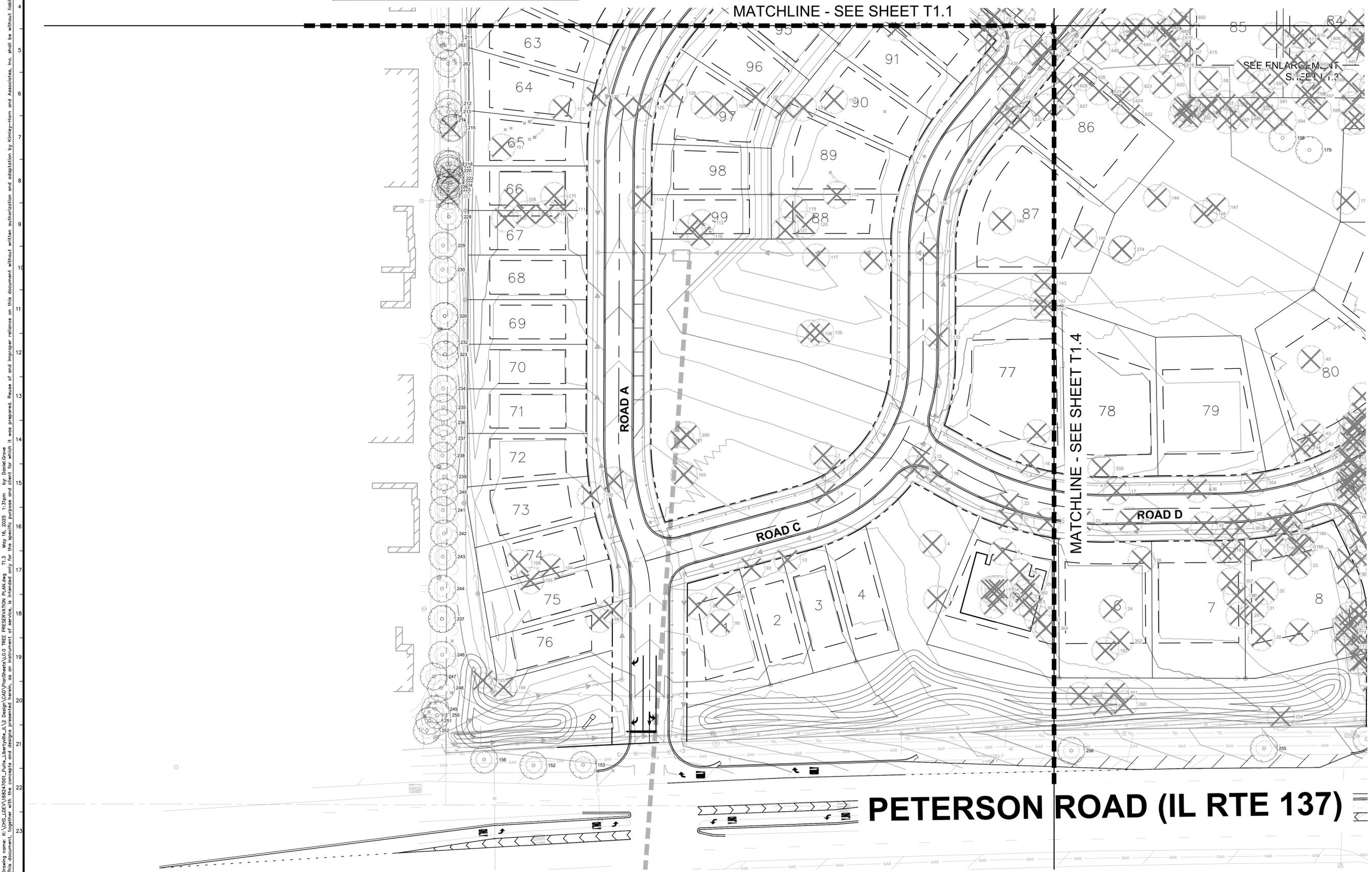
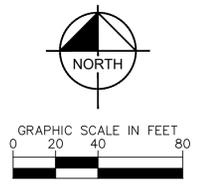
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- REFER TO SHEET T1.5 FOR VILLAGE OF LIBERTYVILLE STANDARD TREE PROTECTION DETAIL AND REQUIREMENTS.

TREE PRESERVATION LEGEND

- EXISTING TREE TO BE PRESERVED
SEE TREE PROTECTION DETAIL ON SHEET T1.5
ASSOCIATED TREE ID #
- EXISTING TREE TO BE REMOVED
SEE TREE TABULATION DATA ON SHEET T1.5
ASSOCIATED TREE ID #
- TREE PROTECTION FENCE
SEE TREE PROTECTION DETAIL ON SHEET T1.5

TREE PRESERVATION NOTES

- CONTRACTOR TO FURNISH AND INSTALL TEMPORARY TREE PROTECTION FENCE, TYP. SEE DETAIL ON SHEET T1.5
- EXISTING TREE TO BE PRESERVED
- EXISTING WOODLAND TO BE PRESERVED
- EXISTING TREE TO BE REMOVED



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TREE PRESERVATION PLAN

GREENWAY CHASE
 610 PETERSON ROAD
 LIBERTYVILLE, IL 60048

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TREE PRESERVATION NOTES

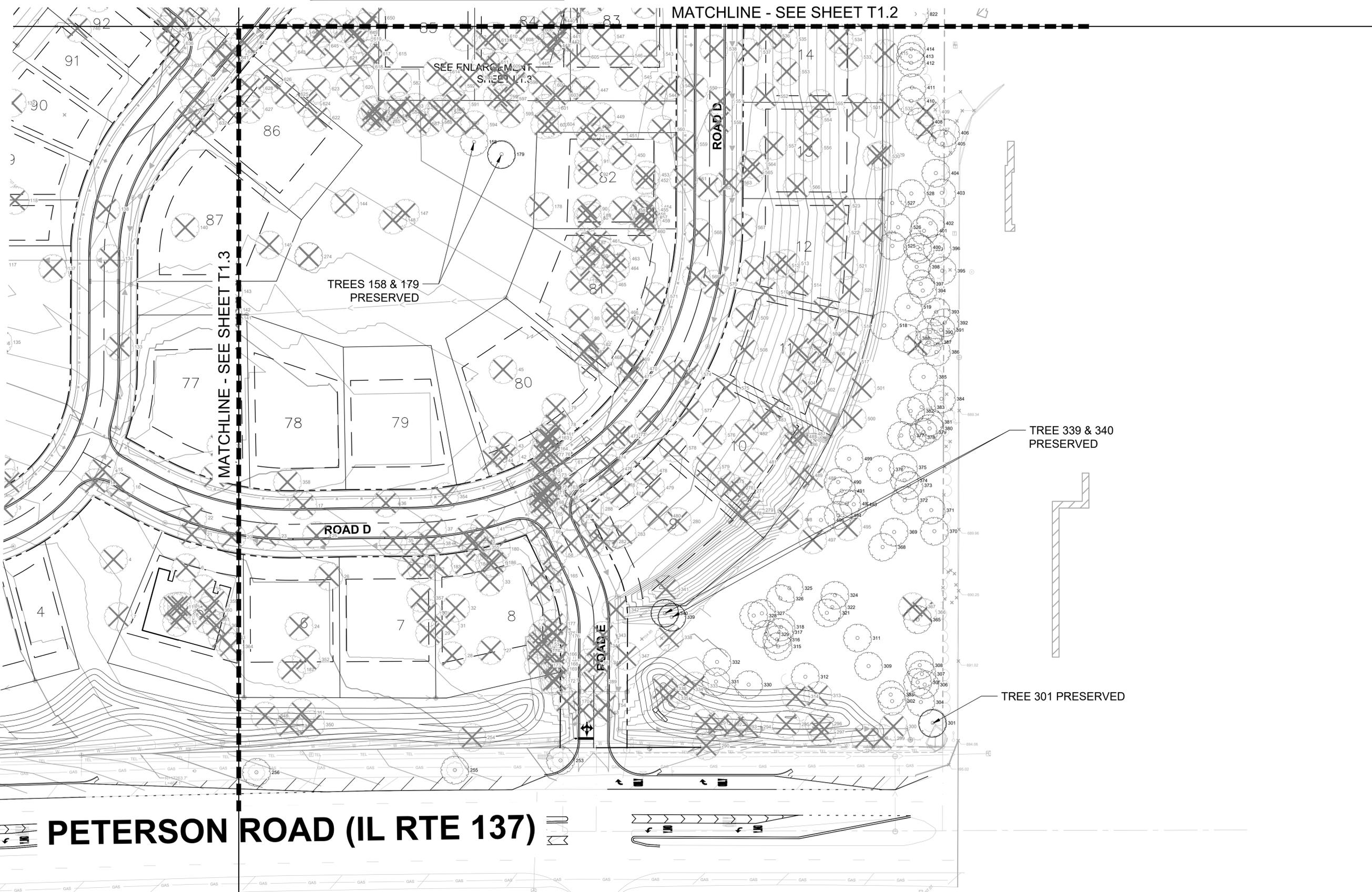
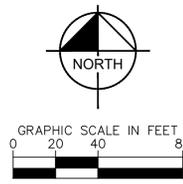
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- EXISTING TREE TO BE PRESERVED
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- EXISTING TREE TO BE REMOVED
SEE TREE TABULATION DATA ON SHEET T1.5
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- TREE PROTECTION FENCE
SEE TREE PROTECTION DETAIL ON SHEET T1.5

TREE PRESERVATION NOTES

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- 2 EXISTING TREE TO BE PRESERVED
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- 4 EXISTING TREE TO BE REMOVED



Drawing name: K:\GIS\DEV\16824001_Pulte_Libertyville_IL2_Design\CD\PlanSheets\16824001_TREE_PRESERVATION_PLAN.dwg T1.4 May 16, 2025 1:31pm by: Daniel Grove
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TREE PRESERVATION PLAN

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 610 PETERSON ROAD
 LIBERTYVILLE, IL 60048

ORIGINAL ISSUE: 01/03/2025
 KHA PROJECT NO. 168247001
 SHEET NUMBER

T1.4

TREE PRESERVATION NOTES

FLORA AND ROOT PROTECTION:

- EXISTING TREES NOT IN DIRECT CONFLICT WITH CONSTRUCTION SHALL BE SAVED AND PROTECTED, UNLESS SPECIFIED TO BE REMOVED. REMOVE TREES ONLY AFTER APPROVAL BY OWNER IN ACCORDANCE WITH LATEST EDITION OF ANSI A300 - TREE CARE OPERATIONS - TREE, SHRUB AND OTHER WOODY PLANT MAINTENANCE - STANDARD PRACTICES. PRUNE TREES ONLY AFTER APPROVAL BY OWNER IN ACCORDANCE WITH LATEST EDITION OF ANSI A300 - TREE CARE OPERATIONS - TREE, SHRUB AND OTHER WOODY PLANT MAINTENANCE - STANDARD PRACTICES WITH ADDITION OF VILLAGE OF LIBERTYVILLE ORDINANCE.

PROTECTION DURING CONSTRUCTION ACTIVITIES:

- TREE PROTECTION FENCE SHALL BE ERRECTED AROUND, MINIMALLY, THE DISTANCE OF DRIPLINE OF THE TREE TO BE PROTECTED. TREE PROTECTION FENCE SHALL BE A MINIMUM OF FOUR FEET WOOD SLAT OR CHAINLINK FENCE ATTACHED TO 6" MINIMUM "T"- STEEL POSTS, DRIVEN AT LEAST 2' INTO GROUND AT 6' INTERVALS OR APPROVED EQUIVALENT.
- TREE PROTECTION SIGNAGE SHALL BE LAMINATED OR OTHERWISE WEATHERPROOF AND PRINTED IN BOLD TEXT SO AS TO BE EASILY READ FROM A DISTANCE OF 20 FEET. WORDING ON SIGNAGE SHALL BE PROVIDED IN BOTH ENGLISH AND SPANISH. SIGNS SHALL BE PLACED EVERY 30' ALONG PROTECTION FENCE.
- NO OTHER CONSTRUCTION ACTIVITY MAY OCCUR ON SITE UNTIL TREE PRESERVATION FENCING HAS BEEN INSTALLED AND APPROVED.
- ALL CONSTRUCTION ACTIVITY WITHIN THE AREAS FENCED OFF AROUND THE TREES SHALL BE PROHIBITED. THIS SHALL INCLUDE, BUT NOT LIMITED TO, THE FOLLOWING ACTIVITIES:
 - PARKING OR DRIVING EQUIPMENT, MACHINERY OR VEHICLES OF ANY TYPE.
 - STORAGE OF ANY CONSTRUCTION MATERIALS, EQUIPMENT, STOCKPILING, EXCAVATION OR FILL, SOIL, GRAVEL, ETC.
 - DUMPING OF ANY CHEMICALS, WASH-OUT MATERIALS FROM CLEANING EQUIPMENT, CONCRETE OR MORTAR REMAINDER, TRASH, GARBAGE, OR DEBRIS OF ANY KIND.
 - TRENCHING, GRADING OR CONSTRUCTION IN THE ROOT AREA.
 - FENCING SHALL REMAIN IN PLACE AND BE CONTINUOUSLY MAINTAINED FOR DURATION OF CONSTRUCTION.
 - ROOT PRUNING SHALL BE PERFORMED WHENEVER GRADES OUTSIDE THE TREE PROTECTION AREA BUT WITHIN THE AN AREA TWICE THE HEIGHT OF THE TREE WILL BE LOWERED.

PROTECTION DURING REMOVAL OF EXISTING MATERIALS WITHIN THE DRIPLINE:

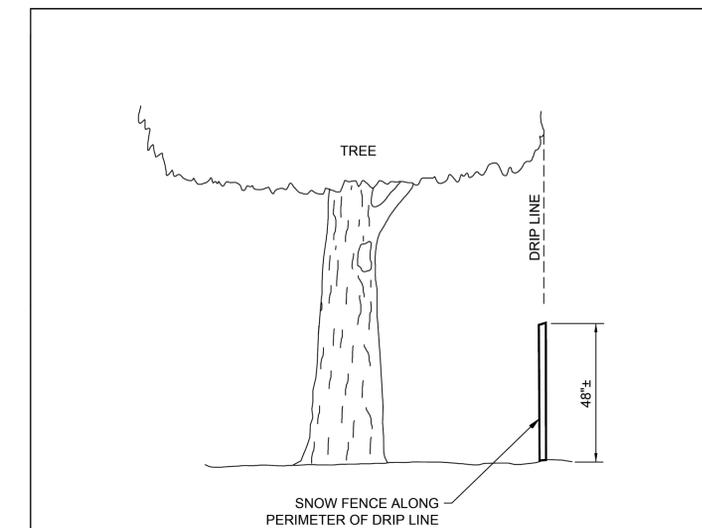
- EXTRA CARE IS TO BE TAKEN DURING THE REMOVAL OF EXISTING MATERIALS WITHIN THE DRIPLINE TO PREVENT BREAKAGE OF ANY ROOTS WITHIN THE DRIPLINE (ROOT ZONE) OF ANY FLORA. VIOLATIONS OF THESE BASIC POLICIES AND PROCEDURES MAY RESULT IN FINES BEING LEVIED AGAINST THE OFFENDING PARTY. ADDITIONALLY, NEGLECT ACTS MAY RESULT IN THE POTENTIAL CATASTROPHIC FAILURE OF THE AFFECTED FLORA LEADING TO INJURY, PROPERTY DAMAGE OR LOSS OF LIFE FOR WHICH THE OFFENDING PARTY SHALL BE HELD RESPONSIBLE.
- DRIPLINE (ROOT ZONE) DIMENSIONS ARE DEFINED BY SIZE CLASSIFICATION OF FLORA.
- NO ROOTS ARE TO BE BROKEN DURING REMOVAL OF EXISTING WALKS, CURBS OR ANY OTHER FACILITIES, UNLESS ROOTS ARE FIRST "PRE-CUT" ON THE TREE SIDE OF THE EXCAVATION. ROOTS ARE TO BE CUT WITH SHARP TOOLS, SUCH AS, CHAIN SAWS, HANDSAWS, LOPPERS, OR OTHER. EQUIPMENT BREAKAGE OF ROOTS DAMAGES MORE OF THE ROOT THAN NECESSARY AND CREATES UNSEEN FRACTURES BEYOND THE SOIL WALL. SEE FIGURES 2A-2B.
- PRE-CUTTING OF ROOTS WILL ONLY BE PERMISSIBLE IN SITUATIONS WHERE IT IS IMPOSSIBLE TO EITHER ELEVATE FINAL PAVED GRADE TO LAY ATOP ROOTS, REDUCE PAVED WIDTH TO ACCEPTABLE SPECIFICATIONS WHILE AVOIDING ROOT INTERFERENCE OR ADHERE TO DIRECTIONAL BORING SPECIFICATIONS.
- ABOVE GROUND PORTIONS OF FLORA ARE TO BE PROTECTED FROM CONTACT WITH ANY EQUIPMENT OR MATERIALS. CONSTRUCTION FENCING SHOULD BE PLACED AT THE PERIMETER OF THE AREA TO BE PROTECTED TO HELP PREVENT UNNECESSARY DAMAGE.
- NO CONCRETE OR OTHER FOREIGN MATERIALS SHALL BE PLACED DIRECTLY AGAINST CUT PORTIONS OF ROOTS OR WITHIN 6" OF CUT PORTIONS.
- THERE SHALL BE FOUR(4) CLASSIFICATIONS OF ROOT SIZES ACCORDING TO THE SIZE OF THE MAIN STEM OF ANY PARTICULAR FLORA.
 - CLASS 1: ROOT DIAMETER = 20-25% OF MAIN STEM DIAMETER
 - CLASS 2: ROOT DIAMETER = 15-20% OF MAIN STEM DIAMETER
 - CLASS 3: ROOT DIAMETER = 10-15% OF MAIN STEM DIAMETER
 - CLASS 4: ROOT DIAMETER = 1-10% OF MAIN STEM DIAMETER
 NO MORE THAN ONE (1) CLASS 1 ROOT MAY BE CUT FROM ANY GIVEN FLORA
 NO MORE THAN TWO (2) CLASS 2 ROOTS MAY BE CUT FROM ANY GIVEN FLORA
 NO MORE THAN FOUR (4) CLASS 3 ROOTS MAY BE CUT FROM ANY GIVEN FLORA
 NO LIMIT FOR CLASS 4 ROOTS
- ROOT REMOVAL IN EXCESS OF THE ABOVE SPECIFICATIONS MAY RESULT IN THE REQUIRED REMOVAL AND REPLACEMENT OF THE AFFECTED FLORA BY THE OFFENDING PARTY.
- NOT MORE THAN ONE SIDE OF ANY FLORA MAY SUFFER CUT ROOTS.
- EXCAVATED SOIL SHALL BE PLACED ON THE SIDE OF THE CUT OPPOSITE THE TREE.
- NO ROOTS GREATER THAN 25% OF THE TRUNK DIAMETER OF FLORA MAY BE CUT OR GROUND OFF AT THE TRUNK OF ANY FLORA.
- NO ROOTS SHALL BE CUT WITHIN 24" OF THE TRUNK OF ANY FLORA OR WITHIN THE STRUCTURAL CRITICAL ROOTING DISTANCE, WHICHEVER IS LARGER.

CLEAN CUTTING AND BACKFILLING ROOTS:

- ALL PRE-CUT AND/OR DAMAGED ROOTS SHALL BE CLEAN CUT WITH THE APPROPRIATE SHARP TOOL PRIOR TO BACK-FILLING OF SOIL.
- ALL CUT ROOTS MUST BE CLEAN CUT PERPENDICULAR TO THE NATURAL DIRECTION OF ROOT GROWTH AT THE POINT WHERE THE CUT IS TO OCCUR.
- ALL CLEAN CUTS SHALL OCCUR ON THE TREE SIDE OF THE ROOT BEYOND A POINT WHERE ALL ROOT TISSUES HAVE BEEN DAMAGED. SEE FIGURES 3A-3B.
- ALL DAMAGED ROOTS SHALL BE BACK-FILLED WITH TOPSOIL WITHIN 60 MINUTES OF BEING CLEAN CUT.
- ALL DAMAGED ROOTS MUST HAVE AT LEAST 6" CLEARANCE FROM ALL PERMANENT CONSTRUCTION MATERIALS EXCEPT TOPSOIL.
- ALL ROOT DAMAGED FLORA MUST BE HEAVILY WATERED WITHIN 24 HOURS OF BACK-FILLING.

GRADE CHANGE SPECIFICATIONS:

- ELEVATING THE FINAL GRADE SHOULD BE CONSIDERED IN ALL SITUATIONS WHERE THE DIAMETER OF THE TRUNK OF VALUABLE FLORA EXCEEDS 10" AND THE LOCATION OF THE WORK TO BE PERFORMED IS WITHIN THE DRIPLINE, AND/OR, THE TRUNK DIAMETER DOES NOT EXCEED THE WIDTH OF THE EXISTING TREE LAWN, AND/OR WHEN DIRECTIONAL BORING IS IMPRACTICAL OR IMPOSSIBLE.
- WHEN FINAL GRADES ARE TO BE ELEVATED ABOVE EXISTING GRADES, EXTRA CARE IS TO BE TAKEN DURING REMOVAL OF EXISTING MATERIALS, I.E., BACKHOES SHOULD "SCRAPE" OR "LIFT" CONCRETE AWAY FROM ROOTS RATHER THAN "SCOOP UP" CONCRETE, OR CONCRETE SHOULD BE BROKEN WITH A JACKHAMMER AND REMOVED MANUALLY, ETC.
- GRADE ELEVATIONS IN EXCESS OF 12" DEPTH SHALL REQUIRE ADHERENCE TO THE ATTACHED TREE WELL SPECIFICATIONS.
- WHEN ELEVATING GRADE LESS THAN 12", NOT MORE THAN 20% OF THE SURFACE AREA WITHIN THE DRIPLINE OF ANY FLORA MAY BE PAVED OR COMPACTED WITH ANY MACHINERY.
- UNDAMAGED ROOTS SHOULD BE COVERED WITH AT LEAST 2" OF SAND OR SOIL BASE PRIOR TO INSTALLATION OF A GRAVEL BASE OR THE POURING OF CONCRETE.
- FORM STAKES SHOULD BE SET AT MAX. 15' INTERVALS FOR ALL FLORA AS REQUIRED BY THE ATTACHED TREE WELL SPECIFICATIONS.
- ELEVATION SHOULD BE HIGH ENOUGH TO PREVENT ROOT CUTTING FOR INSTALLATION OF FORMS.



NOTES:

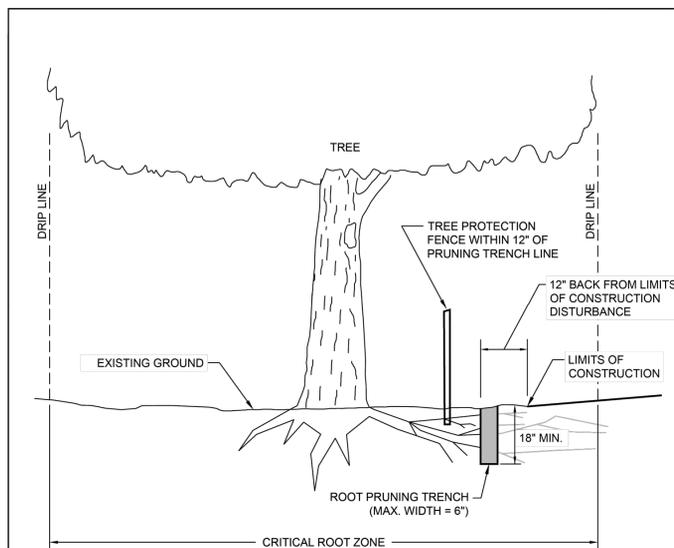
- EXISTING TREES TO BE PROTECTED SHALL HAVE SNOW FENCE INSTALLED AT THE DRIP LINE OF THE TREE TO PREVENT THE STOCKPILING OF EXCAVATED OR CONSTRUCTION MATERIALS UNDER THE TREE, AND PROHIBIT VEHICULAR TRAFFIC OR EXCESSIVE FOOT TRAFFIC WITHIN THE DRIP LINE.
- SNOW FENCE SHALL BE WEBBED HDPE CONSTRUCTION FENCING, COLORED ORANGE AND SUPPORTED WITH STEEL "TEE" POSTS SET AT MAX. 15' INTERVALS. OR SIGNS REQUIRED TO MAINTAIN THE FENCE IN AN UPRIGHT POSITION THROUGHOUT THE TERM OF CONSTRUCTION.
- TREES THAT MAY BE DAMAGED BY CONSTRUCTION OPERATIONS SHALL BE REPAIRED OR REPLACED IN A MANNER ACCEPTABLE TO THE VILLAGE.

TREE PROTECTION FENCING

Libertyville
 LAST REVISED: 02/15/2023
 STANDARD DETAIL # EROS - 04

TREE_NUM	SPECIES	LATIN	DBH	CONDITION	LIBERTYVILLE TREE-GROUP	STATUS
1	HONEYLOCUST	Geditsia triacanthos	9	3	B	TO REMOVE
2	HONEYLOCUST	Geditsia triacanthos	7	3	B	TO REMOVE
3	APPLE-CRABSP	Malus spp	10	4	A	TO REMOVE
4	CRAB-PPN	Quercus palustris	30	4	B	TO REMOVE
5	MAPLE-SILVER	Acer saccharinum	24	3	B	TO REMOVE
6	SPRUCE-BLUE	Picea pungens	17	3	B	TO REMOVE
7	MAPLE-SILVER	Acer saccharinum	26	4	B	TO REMOVE
8	CHERRY-BLACK	Prunus serotina	11	4	C	TO REMOVE
9	MAPLE-SILVER	Acer saccharinum	12	4	B	TO REMOVE
10	EASTERN REDCEDAR	Juniperus virginiana	6	3	D	TO REMOVE
11	MAPLE-SILVER	Acer saccharinum	38	3	B	TO REMOVE
12	MAPLE-SILVER	Acer saccharinum	36	3	B	TO REMOVE
13	SPRUCE-WHITE	Picea glauca	6	3	B	TO REMOVE
14	HONEYLOCUST	Geditsia triacanthos	10	3	B	TO REMOVE
15	HONEYLOCUST	Geditsia triacanthos	10	3	B	TO REMOVE
16	EASTERN REDCEDAR	Juniperus virginiana	4	3	D	TO REMOVE
17	HONEYLOCUST	Geditsia triacanthos	34	3	B	TO REMOVE
18	APPLE-CRABSP	Malus spp	9	4	A	TO REMOVE
19	HONEYLOCUST	Geditsia triacanthos	20	3	B	TO REMOVE
20	HONEYLOCUST	Geditsia triacanthos	19	3	B	TO REMOVE
21	HONEYLOCUST	Geditsia triacanthos	14	3	B	TO REMOVE
22	APPLE-CRABSP	Malus spp	11	4	A	TO REMOVE
23	APPLE-CRABSP	Malus spp	11	4	A	TO REMOVE
24	SPRUCE-BLUE	Picea pungens	23	2	B	TO REMOVE
25	APPLE-CRABSP	Malus spp	12	3	A	TO REMOVE
26	HAWTHORN-SPP	Ostrya spp	8	3	A	TO REMOVE
27	MAPLE-SILVER	Acer saccharinum	23	3	B	TO REMOVE
28	SPRUCE-BLUE	Picea pungens	17	3	B	TO REMOVE
29	SPRUCE-BLUE	Picea pungens	15	3	B	TO REMOVE
30	SPRUCE-BLUE	Picea pungens	14	3	B	TO REMOVE
31	SPRUCE-BLUE	Picea pungens	12	4	B	TO REMOVE
32	MULBERRY-SPP	Morus spp	24	4	D	TO REMOVE
33	CHERRY-BLACK	Prunus serotina	17	4	C	TO REMOVE
34	CHERRY-BLACK	Prunus serotina	11	4	C	TO REMOVE
35	SPRUCE-BLUE	Picea pungens	12	3	B	TO REMOVE
36	COTTONWOOD	Populus deltoides	42	4	C	TO REMOVE
37	CHERRY-BLACK	Prunus serotina	12	4	C	TO REMOVE
38	BOXELDER	Acer negundo	11	4	C	TO REMOVE
39	HAWKBERY	Ostrya occidentalis	21	3	A	TO REMOVE
40	CHERRY-BLACK	Prunus serotina	9	4	C	TO REMOVE
41	CHERRY-BLACK	Prunus serotina	9	4	C	TO REMOVE
42	MAPLE-SILVER	Acer saccharinum	22	3	B	TO REMOVE
43	MAPLE-SILVER	Acer saccharinum	23	3	B	TO REMOVE
44	CHERRY-BLACK	Prunus serotina	23	3	C	TO REMOVE
45	WALNUT-BLACK	Juglans nigra	30	4	B	TO REMOVE

TREE_NUM	SPECIES	LATIN	DBH	CONDITION	LIBERTYVILLE TREE-GROUP	STATUS
46	MAPLE-SILVER	Acer saccharinum	20	4	B	TO REMOVE
49	MAPLE-SILVER	Acer saccharinum	13	3	B	TO REMOVE
50	MAPLE-SILVER	Acer saccharinum	17	4	B	TO REMOVE
51	MAPLE-SILVER	Acer saccharinum	21	3	B	TO REMOVE
52	SPRUCE-BLUE	Picea pungens	13	3	B	TO REMOVE
53	SPRUCE-BLUE	Picea pungens	19	3	B	TO REMOVE
54	SPRUCE-BLUE	Picea pungens	11	3	B	TO REMOVE
55	BOXELDER	Acer negundo	12	4	C	TO REMOVE
56	MAPLE-SILVER	Acer saccharinum	14	5	B	TO REMOVE
57	MAPLE-SILVER	Acer saccharinum	11	4	B	TO REMOVE
58	MAPLE-SILVER	Acer saccharinum	30	5	B	TO REMOVE
59	MAPLE-SILVER	Acer saccharinum	10	5	B	TO REMOVE
60	BOXELDER	Acer negundo	15	4	C	TO REMOVE
61	MAPLE-SILVER	Acer saccharinum	13	3	B	TO REMOVE
62	MAPLE-SILVER	Acer saccharinum	16	3	B	TO REMOVE
63	MAPLE-SILVER	Acer saccharinum	11	3	B	TO REMOVE
64	MAPLE-SILVER	Acer saccharinum	7	4	B	TO REMOVE
65	MAPLE-SILVER	Acer saccharinum	16	3	B	TO REMOVE
66	MAPLE-SILVER	Acer saccharinum	21	3	B	TO REMOVE
67	MAPLE-SILVER	Acer saccharinum	13	3	B	TO REMOVE
68	MAPLE-SILVER	Acer saccharinum	16	3	B	TO REMOVE
69	MAPLE-SILVER	Acer saccharinum	16	3	B	TO REMOVE
70	MAPLE-SILVER	Acer saccharinum	12	3	B	TO REMOVE
71	MAPLE-SILVER	Acer saccharinum	12	3	B	TO REMOVE
72	MAPLE-SILVER	Acer saccharinum	8	3	B	TO REMOVE
73	MAPLE-SILVER	Acer saccharinum	10	4	B	TO REMOVE
74	MAPLE-SILVER	Acer saccharinum	21	3	B	TO REMOVE
75	MAPLE-SILVER	Acer saccharinum	20	3	B	TO REMOVE
76	MAPLE-SILVER	Acer saccharinum	6	4	B	TO REMOVE
77	MAPLE-SILVER	Acer saccharinum	13	4	B	TO REMOVE
78	BOXELDER	Acer negundo	13	4	C	TO REMOVE
79	BOXELDER	Acer negundo	27	4	C	TO REMOVE
80	MULBERRY-SPP	Morus spp	24	4	D	TO REMOVE
81	BOXELDER	Acer negundo	14	4	C	TO REMOVE
82	CHERRY-BLACK	Prunus serotina	25	4	C	TO REMOVE
83	BOXELDER	Acer negundo	12	4	C	TO REMOVE
84	BOXELDER	Acer negundo	13	4	C	TO REMOVE
85	MULBERRY-SPP	Morus spp	20	4	D	TO REMOVE
86	WALNUT-BLACK	Juglans nigra	17	3	B	TO REMOVE
87	CHERRY-BLACK	Prunus serotina	11	4	C	TO REMOVE
88	BOXELDER	Acer negundo	13	4	C	TO REMOVE
89	BOXELDER	Acer negundo	9	4	C	TO REMOVE
90	BOXELDER	Acer negundo	7	4	C	TO REMOVE
91	BOXELDER	Acer negundo	21	4	C	TO REMOVE
92	BOXELDER	Acer negundo	19	4	C	TO REMOVE
93	SPRUCE-BLUE	Picea pungens	2	3	B	TO REMOVE
94	SPRUCE-BLUE	Picea pungens	19	5	B	TO REMOVE
95	SPRUCE-BLUE	Picea pungens	16	5	B	TO REMOVE
96	SPRUCE-BLUE	Picea pungens	11	3	B	TO REMOVE
97	SPRUCE-BLUE	Picea pungens	15	4	B	TO REMOVE
98	SPRUCE-BLUE	Picea pungens	16	4	B	TO REMOVE
99	MAPLE-RED	Acer rubrum	6	3	B	TO REMOVE
100	APPLE-CRABSP	Malus spp	5	4	A	TO REMOVE
101	EASTERN REDCEDAR	Juniperus virginiana	12	2	D	TO REMOVE
102	HONEYLOCUST	Geditsia triacanthos	18	3	B	TO REMOVE
103	MAPLE-JAPANESE	Acer japonicum	2	3	A	TO REMOVE
104	SPRUCE-BLUE	Picea pungens	17	4	B	TO REMOVE
105	SPRUCE-BLUE	Picea pungens	24	4	B	TO REMOVE
106	SPRUCE-BLUE	Picea pungens	16	4	B	TO REMOVE
107	SPRUCE-BLUE	Picea pungens	16	4	B	TO REMOVE
108	SPRUCE-BLUE	Picea pungens	12	4	B	TO REMOVE
109	SPRUCE-BLUE	Picea pungens	16	4	B	TO REMOVE
110	SPRUCE-BLUE	Picea pungens	11	4	B	TO REMOVE
111	SPRUCE-BLUE	Picea pungens	17	4	B	TO REMOVE
112	SPRUCE-BLUE	Picea pungens	19	4	B	TO REMOVE
113	SPRUCE-BLUE	Picea pungens	16	4	B	TO REMOVE
114	SPRUCE-BLUE	Picea pungens	18	3	B	TO REMOVE
115	SPRUCE-BLUE	Picea pungens	17	4	B	TO REMOVE
116	SPRUCE-BLUE	Picea pungens	18	4	B	TO REMOVE
117	SPRUCE-BLUE	Picea pungens	11	4	B	TO REMOVE
118	SPRUCE-BLUE	Picea pungens	16	3	B	TO REMOVE
119	SPRUCE-BLUE	Picea pungens	10	4	B	TO REMOVE
120	SPRUCE-BLUE	Picea pungens	10	4	B	TO REMOVE
121	SPRUCE-BLUE	Picea pungens	10	5	B	TO REMOVE
122	SPRUCE-BLUE	Picea pungens	16	3	B	TO REMOVE
123	SPRUCE-BLUE	Picea pungens	17	3	B	TO REMOVE
124	SPRUCE-BLUE	Picea pungens	15	4	B	TO REMOVE
125	SPRUCE-BLUE	Picea pungens	16	5	B	TO REMOVE
126	SPRUCE-BLUE	Picea pungens	17	3	B	TO REMOVE
127	SPRUCE-BLUE	Picea pungens	18	5	B	TO REMOVE
128	SPRUCE-BLUE	Picea pungens	18	4	B	TO REMOVE
129	SPRUCE-BLUE	Picea pungens	16	3	B	TO REMOVE
130	SPRUCE-BLUE	Picea pungens	17	3	B	TO REMOVE
131	SPRUCE-BLUE	Picea pungens	17	4	B	TO REMOVE
132	SPRUCE-BLUE	Picea pungens	16	3	B	TO REMOVE
133	SPRUCE-WHITE	Picea glauca	2	3	B	TO REMOVE
134	SPRUCE-NORWAY	Picea abies	16	2	B	TO REMOVE
135	SPRUCE-BLUE	Picea pungens	3	3	B	TO REMOVE
136	SPRUCE-NORWAY	Picea abies	2	3	B	TO REMOVE
137	HONEYLOCUST	Geditsia triacanthos	14	3	B	TO REMOVE
138	SPRUCE-BLUE	Picea pungens	16	3	B	TO REMOVE
140	SPRUCE-BLUE	Picea pungens	8	3	B	TO REMOVE
141	SPRUCE-WHITE	Picea glauca	2	4	B	TO REMOVE
142	SPRUCE-WHITE	Picea glauca	2	4	B	TO REMOVE



NOTES:

- EXACT LOCATION OF ROOT PRUNING TO BE AS SPECIFIED BY ENGINEER OR APPOINTED REPRESENTATIVE OF THE VILLAGE OF LIBERTYVILLE PARKS DIVISION.
- ROOT PRUNING TRENCH SHALL BE BACKFILLED IMMEDIATELY FOLLOWING COMPLETION OF PRUNING ACTIVITIES. TRENCH SHALL BE COVERED WITH A MINIMUM OF 3" MULCH.
- ROOT PRUNING ACTIVITIES AND THE PLACEMENT OF PROTECTIVE FENCING SHALL BE COORDINATED SO THAT BOTH SHALL BE COMPLETED AT EACH LOCATION WITHIN A 48-HOUR PERIOD.
- UNDER NO CIRCUMSTANCES SHALL CONSTRUCTION PROCEED PRIOR TO THE COMPLETION OF ROOT PRUNING AND THE PLACEMENT OF PROTECTIVE FENCING, NOR SHALL FENCING BE REMOVED WITHOUT PRIOR AUTHORIZATION FROM THE ENGINEER OR PARKS DIVISION REPRESENTATIVE.

TREE ROOT PRUNING

Libertyville
 LAST REVISED: 02/15/2023
 STANDARD DETAIL # EROS - 05

INS 05/19/25
 RNM 03/28/25
 DG 03/07/25
 DATE

VILLAGE RESUBMITTAL
 VILLAGE RESUBMITTAL
 ARC RE-SUBMITTAL

NO. REVISIONS

Kimley»Horn
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 PEORIA, IL 61604
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SCALE: AS NOTED
 DESIGNED BY: INS
 DRAWN BY: KTRM
 CHECKED BY: RNM

TREE PRESERVATION PLAN

PULTE HOME COMPANY, LLC

GREENWAY CHASE
 610 PETERSON ROAD
 LIBERTYVILLE, IL 60048

ORIGINAL ISSUE: 01/03/2025
 KHA PROJECT NO. 168247001
 SHEET NUMBER

T1.5

Drawing name: I:\GIS\DEV\168247001_Pulte_Libertyville_IL_V Design (CAD) Plots\Sheets\168247001_TREE PRESERVATION PLAN.dwg, T1.5 May 16, 2025, 1:13pm
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Drawing name: \\GIS\DEV\Wb2020_Plan_PRESERVATION_Plan.dwg T.B. May 16, 2025 1:33pm By: Daniel Greene
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TREE NUM	SPECIES	LATIN	DBH	CONDITION	LIBERTYVILLE TREE-GROUP	STATUS
143	SPRUCE-WHITE	Picea glauca	2	4	B	TO REMOVE
144	BOXELDER	Acer negundo	27	4	C	TO REMOVE
145	SPRUCE-NORWAY	Picea abies	3	3	B	TO REMOVE
146	HONEYLOCUST	Gleditsia triacanthos	6	3	B	TO REMOVE
147	SPRUCE-BLUE	Picea pungens	15	5	B	TO REMOVE
148	SPRUCE-BLUE	Picea pungens	15	5	B	TO REMOVE
149	EASTERN REDCEDAR	Juniperus virginiana	14	2	D	TO REMOVE
150	SPRUCE-BLUE	Picea pungens	4	3	B	TO REMOVE
151	SPRUCE-BLUE	Picea pungens	14	3	B	TO REMOVE
152	MAPLE-MIYABE	Acer miyabei 'Morton'	10	3	A	TO SAVE
153	MAPLE-MIYABE	Acer miyabei 'Morton'	4	3	A	TO SAVE
154	SPRUCE-BLUE	Picea pungens	15	4	B	TO REMOVE
155	SPRUCE-BLUE	Picea pungens	16	3	B	TO REMOVE
156	MAPLE-MIYABE	Acer miyabei 'Morton'	7	3	A	TO SAVE
158	SPRUCE-NORWAY	Picea abies	25	2	B	TO SAVE
159	O-HERRY-BLACK	Prunus serotina	17	4	C	TO REMOVE
160	MULBERRY-SP	Morus spp	13	4	D	TO REMOVE
161	BOXELDER	Acer negundo	16	4	C	TO REMOVE
162	MAPLE-SILVER	Acer saccharinum	17	3	B	TO REMOVE
163	BOXELDER	Acer negundo	19	4	C	TO REMOVE
164	MAPLE-SILVER	Acer saccharinum	21	3	B	TO REMOVE
165	WALNUT-BLACK	Juglans nigra	20	3	B	TO REMOVE
166	MAPLE-SILVER	Acer saccharinum	18	3	B	TO REMOVE
167	MAPLE-SILVER	Acer saccharinum	8	3	B	TO REMOVE
168	MAPLE-SILVER	Acer saccharinum	9	3	B	TO REMOVE
169	MAPLE-SILVER	Acer saccharinum	9	3	B	TO REMOVE
170	SPRUCE-BLUE	Picea pungens	14	3	B	TO REMOVE
171	MAPLE-SILVER	Acer saccharinum	20	3	B	TO REMOVE
172	MAPLE-SILVER	Acer saccharinum	12	3	B	TO REMOVE
173	MAPLE-SILVER	Acer saccharinum	18	4	B	TO REMOVE
174	MAPLE-SILVER	Acer saccharinum	11	3	B	TO REMOVE
175	MAPLE-SILVER	Acer saccharinum	14	3	B	TO REMOVE
176	MAPLE-SILVER	Acer saccharinum	8	4	B	TO REMOVE
177	MAPLE-SILVER	Acer saccharinum	14	3	B	TO REMOVE
178	APPLE-CRAB-SP	Malus spp	21	4	A	TO REMOVE
179	SPRUCE-NORWAY	Picea abies	22	2	B	TO SAVE
180	O-HERRY-BLACK	Prunus serotina	12	4	C	TO REMOVE
181	BOXELDER	Acer negundo	11	4	C	TO REMOVE
182	BOXELDER	Acer negundo	21	4	C	TO REMOVE
183	MAPLE-SILVER	Acer saccharinum	24	2	B	TO REMOVE
184	O-HERRY-BLACK	Prunus serotina	13	4	C	TO REMOVE
185	BOXELDER	Acer negundo	14	4	C	TO REMOVE
186	MAPLE-SILVER	Acer saccharinum	38	2	B	TO REMOVE
187	HONEYLOCUST	Gleditsia triacanthos	3	4	B	TO REMOVE
188	OAK-PIN	Quercus palustris	35	3	B	TO SAVE
189	O-HERRY-BLACK	Prunus serotina	21	4	C	TO SAVE
190	COTTONWOOD	Populus deltoides	35	3	C	TO REMOVE
191	COTTONWOOD	Populus deltoides	28	3	C	TO REMOVE
192	COTTONWOOD	Populus deltoides	13	3	C	TO REMOVE
193	COTTONWOOD	Populus deltoides	22	3	C	TO REMOVE
194	O-HERRY-BLACK	Prunus serotina	24	4	C	TO REMOVE
195	O-HERRY-BLACK	Prunus serotina	9	5	C	TO REMOVE
196	BUCKHORN	Rhamnus cathartica	10	4	D	TO SAVE
197	BUCKHORN	Rhamnus cathartica	13	4	D	TO SAVE
198	O-HERRY-BLACK	Prunus serotina	10	3	C	TO SAVE
199	PINE-ALSTRAN	Pinus nigra	8	3	C	TO SAVE
200	O-HERRY-BLACK	Prunus serotina	8	3	C	TO SAVE
201	MULBERRY-SP	Morus spp	8	3	D	TO SAVE
202	BOXELDER	Acer negundo	15	3	C	TO SAVE
203	O-HERRY-BLACK	Prunus serotina	22	4	C	TO SAVE
204	APPLE-CRAB-SP	Malus spp	16	4	A	TO SAVE
205	O-HERRY-BLACK	Prunus serotina	11	4	C	TO SAVE
206	PINE-ALSTRAN	Pinus nigra	11	3	C	TO SAVE
207	BUCKHORN	Rhamnus cathartica	18	4	D	TO SAVE
208	O-HERRY-BLACK	Prunus serotina	26	4	C	TO SAVE
209	PINE-ALSTRAN	Pinus nigra	11	3	C	TO SAVE
210	PINE-ALSTRAN	Pinus nigra	16	2	C	TO SAVE
211	O-HERRY-BLACK	Prunus serotina	32	4	C	TO SAVE
212	O-HERRY-BLACK	Prunus serotina	20	4	C	TO SAVE
213	BOXELDER	Acer negundo	19	3	C	TO SAVE
214	ASH-GREEN	Fraxinus pennsylvanica	12	3	D	TO SAVE
215	BOXELDER	Acer negundo	21	3	C	TO SAVE
216	O-HERRY-BLACK	Prunus serotina	18	5	C	TO REMOVE
217	BOXELDER	Acer negundo	18	3	C	TO SAVE
218	O-HERRY-BLACK	Prunus serotina	23	4	C	TO SAVE
219	O-HERRY-BLACK	Prunus serotina	19	4	C	TO SAVE
220	O-HERRY-BLACK	Prunus serotina	7	4	C	TO SAVE
221	O-HERRY-BLACK	Prunus serotina	12	5	C	TO REMOVE
222	O-HERRY-BLACK	Prunus serotina	17	4	C	TO REMOVE
223	BUCKHORN	Rhamnus cathartica	5	3	D	TO SAVE
224	BUCKHORN	Rhamnus cathartica	7	3	D	TO SAVE
225	BUCKHORN	Rhamnus cathartica	6	3	D	TO SAVE
226	MULBERRY-SP	Morus spp	9	3	D	TO SAVE
227	BUCKHORN	Rhamnus cathartica	16	5	D	TO REMOVE
228	O-HERRY-BLACK	Prunus serotina	35	4	C	TO SAVE
229	PINE-SCOTCH	Pinus sylvestris	14	3	C	TO SAVE
230	PINE-SCOTCH	Pinus sylvestris	14	3	C	TO SAVE
231	PINE-ALSTRAN	Pinus nigra	11	2	C	TO SAVE
232	PINE-ALSTRAN	Pinus nigra	12	2	C	TO SAVE
233	PINE-ALSTRAN	Pinus nigra	11	3	C	TO SAVE
234	PINE-ALSTRAN	Pinus nigra	12	3	C	TO SAVE
235	PINE-ALSTRAN	Pinus nigra	11	2	C	TO SAVE
236	PINE-ALSTRAN	Pinus nigra	14	2	C	TO SAVE
237	SPRUCE-BLUE	Picea pungens	2	3	D	TO SAVE
238	SPRUCE-BLUE	Picea pungens	12	2	B	TO SAVE
239	SPRUCE-BLUE	Picea pungens	10	2	B	TO SAVE
240	SPRUCE-BLUE	Picea pungens	10	2	B	TO SAVE
241	PINE-ALSTRAN	Pinus nigra	16	2	C	TO SAVE
242	SPRUCE-BLUE	Picea pungens	9	2	B	TO SAVE
243	SPRUCE-BLUE	Picea pungens	6	3	B	TO SAVE
244	PINE-ALSTRAN	Pinus nigra	15	2	C	TO SAVE
245	PINE-SCOTCH	Pinus sylvestris	13	2	C	TO SAVE
246	SPRUCE-NORWAY	Picea abies	15	2	B	TO SAVE
247	SPRUCE-NORWAY	Picea abies	18	3	B	TO SAVE
248	SPRUCE-NORWAY	Picea abies	12	3	B	TO SAVE
249	ARBORVITAE	Thuja occidentalis	2	3	C	TO SAVE
250	ARBORVITAE	Thuja occidentalis	2	3	C	TO SAVE
251	PINE-ALSTRAN	Pinus nigra	10	3	C	TO SAVE
252	SPRUCE-BLUE	Picea pungens	12	3	B	TO SAVE
253	EASTERN REDCEDAR	Juniperus virginiana	11	3	D	TO REMOVE
254	APPLE-CRAB-SP	Malus spp	13	3	A	TO REMOVE
255	MAPLE-MIYABE	Acer miyabei 'Morton'	4	4	A	TO SAVE
256	MAPLE-MIYABE	Acer miyabei 'Morton'	5	4	A	TO SAVE
257	AMERICAN REDBUD	Cercis canadensis	2	3	A	TO SAVE
263	SPRUCE-NORWAY	Picea abies	1	3	B	TO REMOVE
264	HONEYLOCUST	Gleditsia triacanthos	4	3	B	TO REMOVE
266	ARBORVITAE	Thuja occidentalis	6	3	C	TO REMOVE
267	ARBORVITAE	Thuja occidentalis	6	3	C	TO REMOVE
268	ARBORVITAE	Thuja occidentalis	6	3	C	TO REMOVE
269	ARBORVITAE	Thuja occidentalis	2	3	C	TO REMOVE
270	ARBORVITAE	Thuja occidentalis	2	3	C	TO REMOVE
271	AMERICAN REDBUD	Cercis canadensis	2	4	A	TO REMOVE

TREE NUM	SPECIES	LATIN	DBH	CONDITION	LIBERTYVILLE TREE-GROUP	STATUS
272	BOXELDER	Acer negundo	14	4	C	TO REMOVE
273	HACBERRY	Ostrya occidentalis	19	2	A	TO REMOVE
274	BOXELDER	Acer negundo	14	4	C	TO REMOVE
275	PINE-WHITE	Pinus strobus	13	3	B	TO REMOVE
276	PINE-WHITE	Pinus strobus	16	3	B	TO REMOVE
277	PINE-WHITE	Pinus strobus	19	3	B	TO REMOVE
278	ARBORVITAE	Thuja occidentalis	12	4	C	TO REMOVE
279	ARBORVITAE	Thuja occidentalis	10	3	C	TO REMOVE
280	MAPLE-SILVER	Acer saccharinum	40	3	B	TO REMOVE
281	O-HERRY-BLACK	Prunus serotina	17	4	C	TO REMOVE
282	BOXELDER	Acer negundo	23	4	C	TO REMOVE
283	BOXELDER	Acer negundo	17	4	C	TO REMOVE
284	ELM-AMERICAN	Ulmus americana	9	4	C	TO REMOVE
285	O-HERRY-BLACK	Prunus serotina	15	4	C	TO REMOVE
286	BOXELDER	Acer negundo	10	4	C	TO REMOVE
287	MAPLE-SILVER	Acer saccharinum	12	3	B	TO REMOVE
288	BOXELDER	Acer negundo	16	4	C	TO REMOVE
289	BOXELDER	Acer negundo	25	4	C	TO REMOVE
290	MULBERRY-SP	Morus spp	43	4	D	TO REMOVE
291	SPRUCE-BLUE	Picea pungens	15	3	B	TO REMOVE
292	SPRUCE-BLUE	Picea pungens	12	4	B	TO REMOVE
293	SPRUCE-BLUE	Picea pungens	11	3	B	TO REMOVE
294	SPRUCE-BLUE	Picea pungens	13	3	B	TO REMOVE
295	SPRUCE-BLUE	Picea pungens	14	3	B	TO REMOVE
296	EASTERN REDCEDAR	Juniperus virginiana	7	3	D	TO REMOVE
297	MULBERRY-SP	Morus spp	11	3	D	TO REMOVE
298	MULBERRY-SP	Morus spp	15	4	D	TO REMOVE
299	MULBERRY-SP	Morus spp	25	4	D	TO REMOVE
300	MULBERRY-SP	Morus spp	14	4	D	TO REMOVE
301	O-HERRY-BLACK	Prunus serotina	13	3	C	TO SAVE
302	MAPLE-SILVER	Acer saccharinum	29	2	B	TO SAVE
303	MAPLE-SILVER	Acer saccharinum	25	2	B	TO SAVE
304	MAPLE-SILVER	Acer saccharinum	20	2	B	TO SAVE
305	ELM-AMERICAN	Ulmus americana	22	2	C	TO SAVE
306	MAPLE-SILVER	Acer saccharinum	11	3	B	TO SAVE
307	O-HERRY-BLACK	Prunus serotina	16	4	C	TO SAVE
308	O-HERRY-BLACK	Prunus serotina	8	4	C	TO SAVE
309	O-HERRY-BLACK	Prunus serotina	17	4	C	TO SAVE
311	O-HERRY-BLACK	Prunus serotina	26	4	C	TO SAVE
312	MULBERRY-SP	Morus spp	11	4	D	TO SAVE
313	WALNUT-BLACK	Juglans nigra	14	3	B	TO REMOVE
314	HACBERRY	Ostrya occidentalis	3	3	A	TO REMOVE
315	MULBERRY-SP	Morus spp	15	4	D	TO REMOVE
316	EASTERN REDCEDAR	Juniperus virginiana	13	3	D	TO SAVE
317	EASTERN REDCEDAR	Juniperus virginiana	11	3	D	TO SAVE
318	EASTERN REDCEDAR	Juniperus virginiana	8	3	D	TO SAVE
319	EASTERN REDCEDAR	Juniperus virginiana	1	3	D	TO REMOVE
320	EASTERN REDCEDAR	Juniperus virginiana	1	3	D	TO SAVE
321	MAPLE-SILVER	Acer saccharinum	18	3	B	TO REMOVE
322	O-HERRY-BLACK	Prunus serotina	10	4	C	TO SAVE
323	SPRUCE-NORWAY	Picea abies	1	3	B	TO SAVE
324	BOXELDER	Acer negundo	16	4	C	TO REMOVE
325	WALNUT-BLACK	Juglans nigra	7	3	B	TO SAVE
326	WALNUT-BLACK	Juglans nigra	11	3	B	TO SAVE
327	MULBERRY-SP	Morus spp	10	4	D	TO REMOVE
328	MULBERRY-SP	Morus spp	9	4	D	TO REMOVE
329	WALNUT-BLACK	Juglans nigra	8	3	B	TO REMOVE
330	O-HERRY-BLACK	Prunus serotina	25	4	C	TO SAVE
331	MULBERRY-SP	Morus spp	12	3	D	TO SAVE
332	O-HERRY-BLACK	Prunus serotina	37	4	C	TO SAVE
333	O-HERRY-BLACK	Prunus serotina	18	4	C	TO REMOVE
334	O-HERRY-BLACK	Prunus serotina	15	4	C	TO REMOVE
335	O-HERRY-BLACK	Prunus serotina	23	4	C	TO REMOVE
336	O-HERRY-BLACK	Prunus serotina	17	4	C	TO REMOVE
337	EASTERN REDCEDAR	Juniperus virginiana	1	3	D	TO SAVE
338	O-HERRY-BLACK	Prunus serotina	22	4	C	TO REMOVE
339	MAPLE-SILVER	Acer saccharinum	34	2	B	TO SAVE
340	MAPLE-SILVER	Acer saccharinum	39	2	B	TO SAVE
341	MAPLE-SILVER	Acer saccharinum	33	2	B	TO REMOVE
342	BOXELDER	Acer negundo	25	4	C	TO REMOVE
343	MAPLE-SILVER	Acer saccharinum	46	1	B	TO REMOVE
344	BOXELDER	Acer negundo	16	4	C	TO REMOVE
345	O-HERRY-BLACK	Prunus serotina	23	4	C	TO REMOVE
347	HACBERRY	Ostrya occidentalis	9	3	A	TO REMOVE
348	ARBORVITAE	Thuja occidentalis	21	3	C	TO REMOVE
349	ARBORVITAE	Thuja occidentalis	14	3	C	TO REMOVE
350	ARBORVITAE	Thuja occidentalis	11	3	C	TO REMOVE
351	ARBORVITAE	Thuja occidentalis	12	3	C	TO REMOVE
352	BRO-WHITE	Betula papyrifera	15	3	C	TO REMOVE
354	O-HERRY-BLACK	Prunus serotina	21	4	C	TO REMOVE
355	O-HERRY-BLACK	Prunus serotina	14	4	C	TO REMOVE
356	LILAC-TREE	Syringa spp	13	3	A	TO REMOVE
357	SPRUCE-BLUE	Picea pungens	14	3	B	TO REMOVE
358	APPLE-CRAB-SP	Malus spp	7	4	A	TO REMOVE
359	ARBORVITAE	Thuja occidentalis	11	3	C	TO REMOVE
360	ARBORVITAE	Thuja occidentalis	9	3	C	TO REMOVE
361	ARBORVITAE	Thuja occidentalis	11	3	C	TO REMOVE
362	ARBORVITAE	Thuja occidentalis	14	3	C	TO REMOVE
363	ARBORVITAE	Thuja occidentalis	14	3	C	TO REMOVE
364	ARBORVITAE	Thuja occidentalis	11	3	C	TO REMOVE
365	O-HERRY-BLACK	Prunus serotina	10	4	C	TO REMOVE
366	ASH-WHITE	Fraxinus americana	16	5	D	TO REMOVE
367	ELM-AMERICAN	Ulmus americana	16	5	C	TO REMOVE

Drawing name: X:\GIS\DEV\Map2020\000_Plan\Preservation_Plan.dwg 177 May 16, 2025 1:33pm By: Daniel Greene
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TREE_NUM	SPECIES	LATIN	DBH	CONDITION	LIBERTYVILLE TREE-GROUP	STATUS
639	WALNUT-BLACK	Juglans nigra	17	2	B	TO REMOVE
640	CHERRY-BLACK	Prunus serotina	9	4	C	TO REMOVE
641	CHERRY-BLACK	Prunus serotina	10	4	C	TO REMOVE
642	CHERRY-BLACK	Prunus serotina	10	5	C	TO REMOVE
643	BOXELDER	Acer negundo	6	3	C	TO REMOVE
644	CHERRY-BLACK	Prunus serotina	6	3	C	TO REMOVE
645	CHERRY-BLACK	Prunus serotina	8	4	C	TO REMOVE
646	CHERRY-BLACK	Prunus serotina	10	2	C	TO REMOVE
647	BOXELDER	Acer negundo	7	3	C	TO REMOVE
648	BOXELDER	Acer negundo	20	3	C	TO REMOVE
649	BOXELDER	Acer negundo	8	3	C	TO REMOVE
650	BOXELDER	Acer negundo	15	4	C	TO REMOVE
651	ELM-AMERICAN	Ulmus americana	7	3	C	TO REMOVE
652	CHERRY-BLACK	Prunus serotina	18	5	C	TO REMOVE
653	ELM-AMERICAN	Ulmus americana	6	3	C	TO REMOVE
654	WALNUT-BLACK	Juglans nigra	15	2	B	TO REMOVE
655	WALNUT-BLACK	Juglans nigra	16	2	B	TO REMOVE
656	ELM-SPP	Ulmus spp	8	3	B	TO REMOVE
657	CHERRY-BLACK	Prunus serotina	15	5	C	TO REMOVE
658	ASH-GREEN	Fraxinus pennsylvanica	11	3	D	TO REMOVE
659	CHERRY-BLACK	Prunus serotina	16	3	C	TO REMOVE
660	MAPLE-SUGAR	Acer saccharum	22	2	B	TO REMOVE
661	MAPLE-SILVER	Acer saccharinum	44	2	B	TO REMOVE
662	BOXELDER	Acer negundo	9	3	C	TO REMOVE
663	CHERRY-BLACK	Prunus serotina	9	3	C	TO REMOVE
664	CHERRY-BLACK	Prunus serotina	10	3	C	TO REMOVE
665	CHERRY-BLACK	Prunus serotina	11	4	C	TO REMOVE
666	CHERRY-BLACK	Prunus serotina	10	3	C	TO REMOVE
667	CHERRY-BLACK	Prunus serotina	15	2	C	TO REMOVE
668	CHERRY-BLACK	Prunus serotina	14	3	C	TO REMOVE
669	CHERRY-BLACK	Prunus serotina	12	3	C	TO REMOVE
670	CHERRY-BLACK	Prunus serotina	12	4	C	TO REMOVE
671	CHERRY-BLACK	Prunus serotina	10	3	C	TO REMOVE
672	BOXELDER	Acer negundo	11	4	C	TO REMOVE
673	WALNUT-BLACK	Juglans nigra	13	4	B	TO REMOVE
674	MAPLE-SUGAR	Acer saccharum	24	2	B	TO REMOVE
675	CHERRY-BLACK	Prunus serotina	17	3	C	TO REMOVE
676	ELM-SIBERIAN	Ulmus pumila	16	4	D	TO REMOVE
677	CHERRY-BLACK	Prunus serotina	14	3	C	TO REMOVE
678	CHERRY-BLACK	Prunus serotina	14	4	C	TO REMOVE
679	MAPLE-SUGAR	Acer saccharum	21	1	B	TO REMOVE
680	BOXELDER	Acer negundo	21	3	C	TO REMOVE
681	MULBERRY-SPP	Morus spp	16	3	D	TO REMOVE
682	BOXELDER	Acer negundo	11	4	C	TO REMOVE
683	BOXELDER	Acer negundo	15	4	C	TO REMOVE
684	WALNUT-BLACK	Juglans nigra	26	3	B	TO REMOVE
685	MULBERRY-SPP	Morus spp	9	3	D	TO REMOVE
686	COTTONWOOD	Populus deltoides	16	2	C	TO REMOVE
687	WALNUT-BLACK	Juglans nigra	24	3	B	TO REMOVE
688	MULBERRY-SPP	Morus spp	7	4	D	TO REMOVE
689	COTTONWOOD	Populus deltoides	12	3	C	TO REMOVE
690	BOXELDER	Acer negundo	22	4	C	TO REMOVE
691	COTTONWOOD	Populus deltoides	30	3	C	TO REMOVE
692	COTTONWOOD	Populus deltoides	12	2	C	TO REMOVE
693	ELM-SIBERIAN	Ulmus pumila	26	4	D	TO REMOVE
694	COTTONWOOD	Populus deltoides	15	3	C	TO REMOVE
695	BOXELDER	Acer negundo	16	3	C	TO REMOVE
696	BOXELDER	Acer negundo	10	3	C	TO REMOVE
697	MULBERRY-SPP	Morus spp	9	3	D	TO REMOVE
698	BOXELDER	Acer negundo	15	4	C	TO REMOVE
699	BOXELDER	Acer negundo	11	4	C	TO REMOVE
700	MULBERRY-SPP	Morus spp	6	3	D	TO REMOVE
701	MAPLE-SILVER	Acer saccharinum	11	3	B	TO REMOVE
702	MULBERRY-SPP	Morus spp	6	3	D	TO REMOVE
703	BOXELDER	Acer negundo	14	4	C	TO REMOVE
704	CHERRY-BLACK	Prunus serotina	18	4	C	TO REMOVE
705	CHERRY-BLACK	Prunus serotina	14	4	C	TO REMOVE
706	MAPLE-SUGAR	Acer saccharum	8	3	B	TO REMOVE
707	CHERRY-BLACK	Prunus serotina	16	4	C	TO REMOVE
708	BOXELDER	Acer negundo	11	4	C	TO REMOVE
709	BOXELDER	Acer negundo	18	4	C	TO REMOVE
710	CHERRY-BLACK	Prunus serotina	12	4	C	TO REMOVE
711	BOXELDER	Acer negundo	13	4	C	TO REMOVE
712	CHERRY-BLACK	Prunus serotina	13	3	C	TO REMOVE
713	CHERRY-BLACK	Prunus serotina	10	3	C	TO REMOVE
714	WALNUT-BLACK	Juglans nigra	6	4	B	TO REMOVE
715	BOXELDER	Acer negundo	6	4	C	TO REMOVE
716	MAPLE-SUGAR	Acer saccharum	14	2	B	TO REMOVE
717	CHERRY-BLACK	Prunus serotina	15	3	C	TO REMOVE
718	CHERRY-BLACK	Prunus serotina	9	4	C	TO REMOVE
719	CHERRY-BLACK	Prunus serotina	10	4	C	TO REMOVE
720	MULBERRY-SPP	Morus spp	17	4	D	TO REMOVE
721	CHERRY-BLACK	Prunus serotina	15	3	C	TO REMOVE
722	CHERRY-BLACK	Prunus serotina	12	4	C	TO REMOVE
723	CHERRY-BLACK	Prunus serotina	11	3	C	TO REMOVE
724	BOXELDER	Acer negundo	12	4	C	TO REMOVE
725	CHERRY-BLACK	Prunus serotina	16	5	C	TO REMOVE
726	MULBERRY-SPP	Morus spp	16	3	D	TO REMOVE
727	CATALPA	Catalpa speciosa	21	3	B	TO REMOVE
728	WALNUT-BLACK	Juglans nigra	13	3	B	TO REMOVE
729	MAPLE-SILVER	Acer saccharinum	18	3	B	TO REMOVE
730	BOXELDER	Acer negundo	13	3	C	TO REMOVE
731	WALNUT-BLACK	Juglans nigra	13	3	B	TO REMOVE
732	CHERRY-BLACK	Prunus serotina	17	3	C	TO REMOVE
733	WALNUT-BLACK	Juglans nigra	16	3	B	TO REMOVE
734	CHERRY-BLACK	Prunus serotina	16	3	C	TO REMOVE
735	WALNUT-BLACK	Juglans nigra	21	2	B	TO REMOVE
736	CHERRY-BLACK	Prunus serotina	31	4	C	TO REMOVE
737	WALNUT-BLACK	Juglans nigra	18	3	B	TO REMOVE
738	CHERRY-BLACK	Prunus serotina	6	3	C	TO REMOVE
739	WALNUT-BLACK	Juglans nigra	13	3	B	TO REMOVE
740	CHERRY-BLACK	Prunus serotina	15	4	C	TO REMOVE
741	OKA-WHITE	Quercus alba	16	4	A	TO REMOVE
742	CHERRY-BLACK	Prunus serotina	22	4	C	TO REMOVE
743	BOXELDER	Acer negundo	6	4	C	TO REMOVE
744	CHERRY-BLACK	Prunus serotina	26	4	C	TO REMOVE
745	BOXELDER	Acer negundo	8	4	C	TO REMOVE
746	MAPLE-SILVER	Acer saccharinum	36	3	B	TO REMOVE
747	WALNUT-BLACK	Juglans nigra	19	2	B	TO REMOVE
748	CHERRY-BLACK	Prunus serotina	13	4	C	TO REMOVE
749	CHERRY-BLACK	Prunus serotina	13	4	C	TO REMOVE
750	CHERRY-BLACK	Prunus serotina	13	4	C	TO REMOVE
751	BOXELDER	Acer negundo	12	4	C	TO REMOVE
752	CHERRY-BLACK	Prunus serotina	17	4	C	TO REMOVE
753	MULBERRY-SPP	Morus spp	6	3	D	TO REMOVE
754	BOXELDER	Acer negundo	13	4	C	TO REMOVE
755	CHERRY-BLACK	Prunus serotina	25	3	C	TO REMOVE
756	MAPLE-SILVER	Acer saccharinum	33	2	B	TO REMOVE
757	CHERRY-BLACK	Prunus serotina	6	3	C	TO REMOVE
758	BOXELDER	Acer negundo	17	4	C	TO REMOVE
759	BOXELDER	Acer negundo	18	4	C	TO REMOVE
760	COTTONWOOD	Populus deltoides	25	3	C	TO REMOVE

TREE_NUM	SPECIES	LATIN	DBH	CONDITION	LIBERTYVILLE TREE-GROUP	STATUS
761	BOXELDER	Acer negundo	22	4	C	TO REMOVE
762	CHERRY-BLACK	Prunus serotina	12	4	C	TO REMOVE
763	BOXELDER	Acer negundo	25	4	C	TO REMOVE
764	CHERRY-BLACK	Prunus serotina	21	3	C	TO REMOVE
765	BOXELDER	Acer negundo	16	4	C	TO REMOVE
766	CHERRY-BLACK	Prunus serotina	6	3	C	TO REMOVE
767	CHERRY-BLACK	Prunus serotina	15	3	C	TO REMOVE
768	BOXELDER	Acer negundo	14	4	C	TO REMOVE
769	BOXELDER	Acer negundo	10	4	C	TO REMOVE
770	BOXELDER	Acer negundo	11	4	C	TO REMOVE
771	MULBERRY-SPP	Morus spp	16	4	D	TO REMOVE
772	BOXELDER	Acer negundo	15	4	C	TO REMOVE
773	BOXELDER	Acer negundo	15	4	C	TO REMOVE
774	MAPLE-SILVER	Acer saccharinum	23	1	B	TO REMOVE
775	BOXELDER	Acer negundo	17	4	C	TO REMOVE
776	CHERRY-BLACK	Prunus serotina	16	2	C	TO REMOVE
777	BOXELDER	Acer negundo	13	4	C	TO REMOVE
778	BOXELDER	Acer negundo	12	3	C	TO REMOVE
779	BOXELDER	Acer negundo	14	4	C	TO REMOVE
780	CHERRY-BLACK	Prunus serotina	10	3	C	TO REMOVE
781	BOXELDER	Acer negundo	23	4	C	TO REMOVE
782	CHERRY-BLACK	Prunus serotina	15	4	C	TO REMOVE
783	CHERRY-BLACK	Prunus serotina	16	3	C	TO REMOVE
784	CHERRY-BLACK	Prunus serotina	20	2	C	TO REMOVE
785	CHERRY-BLACK	Prunus serotina	9	3	C	TO REMOVE
786	CHERRY-BLACK	Prunus serotina	14	4	C	TO REMOVE
787	CHERRY-BLACK	Prunus serotina	20	4	C	TO REMOVE
788	CHERRY-BLACK	Prunus serotina	10	4	C	TO REMOVE
789	BOXELDER	Acer negundo	13	4	C	TO REMOVE
790	CHERRY-BLACK	Prunus serotina	16	3	C	TO REMOVE
791	BOXELDER	Acer negundo	11	4	C	TO REMOVE
792	CHERRY-BLACK	Prunus serotina	17	4	C	TO REMOVE
793	BOXELDER	Acer negundo	10	4	C	TO REMOVE
794	CHERRY-BLACK	Prunus serotina	24	4	C	TO REMOVE
795	BOXELDER	Acer negundo	22	4	C	TO REMOVE
796	CHERRY-BLACK	Prunus serotina	8	4	C	TO REMOVE
797	BOXELDER	Acer negundo	10	4	C	TO REMOVE
798	CHERRY-BLACK	Prunus serotina	9	4	C	TO REMOVE
799	BOXELDER	Acer negundo	9	4	C	TO REMOVE
800	ELM-SIBERIAN	Ulmus pumila	18	3	D	TO REMOVE
801	CHERRY-BLACK	Prunus serotina	11	4	C	TO REMOVE
802	BOXELDER	Acer negundo	8	4	C	TO REMOVE
803	BOXELDER	Acer negundo	11	4	C	TO REMOVE
804	BOXELDER	Acer negundo	14	4	C	TO REMOVE
805	BOXELDER	Acer negundo	12	4	C	TO REMOVE
806	CHERRY-BLACK	Prunus serotina	8	4	C	TO REMOVE
807	CHERRY-BLACK	Prunus serotina	8	4	C	TO REMOVE
808	BOXELDER	Acer negundo	16	4	C	TO REMOVE
809	APPLE-CRAB-SPP	Malus spp	7	4	A	TO REMOVE
810	BOXELDER	Acer negundo	11	4	C	TO REMOVE
811	CHERRY-BLACK	Prunus serotina	6	3	C	TO REMOVE
812	BOXELDER	Acer negundo	18	4	C	TO REMOVE
813	BOXELDER	Acer negundo	13	4	C	TO REMOVE
814	MAPLE-SILVER	Acer saccharinum	40	3	B	TO REMOVE
815	BOXELDER	Acer negundo	16	3	C	TO REMOVE
816	CHERRY-BLACK	Prunus serotina	10	3	C	TO REMOVE
817	BOXELDER	Acer negundo	18	4	C	TO REMOVE
818	BOXELDER	Acer negundo	27	4	C	TO REMOVE
819	CHERRY-BLACK	Prunus serotina	23	4	C	TO REMOVE
820	CHERRY-BLACK	Prunus serotina	11	4	C	TO REMOVE
821	BOXELDER	Acer negundo	11	4	C	TO REMOVE
822	BOXELDER	Acer negundo	15	3	C	TO REMOVE
823	BOXELDER	Acer negundo	11	4	C	TO REMOVE
824	CHERRY-BLACK	Prunus serotina	12	3	C	TO REMOVE
825	CHERRY-BLACK	Prunus serotina	16	4	C	TO REMOVE
826	CHERRY-BLACK	Prunus serotina	16	3	C	TO REMOVE
827	BOXELDER	Acer negundo	23	4	C	TO REMOVE
828	CHERRY-BLACK	Prunus serotina	6	3	C	TO REMOVE
829	APPLE-CRAB-SPP	Malus spp	7	3	A	TO REMOVE
830	CHERRY-BLACK	Prunus serotina	12	4	C	TO REMOVE
831	CHERRY-BLACK	Prunus serotina	19	3	C	TO REMOVE
832	MULBERRY-SPP	Morus spp	12	3	D	TO REMOVE
833	CHERRY-BLACK	Prunus serotina	7	3	C	TO REMOVE
834	HACKBERRY	Ostrya occidentalis	6	5	A	TO REMOVE
835	CHERRY-BLACK	Prunus serotina	8	3	C	TO REMOVE
836	BOXELDER	Acer negundo	8	3	C	TO REMOVE
837	CHERRY-BLACK	Prunus serotina	7	3	C	TO REMOVE
838	MULBERRY-SPP	Morus spp	11	3	D	TO REMOVE
839	MAPLE-SILVER	Acer saccharinum	11	2	B	TO REMOVE
840	CHERRY-BLACK	Prunus serotina	16	4	C	TO REMOVE
841	BOXELDER	Acer negundo	42	3	C	TO REMOVE
842	CHERRY-BLACK	Prunus serotina	24	4	C	TO REMOVE
843	CHERRY-BLACK	Prunus serotina	23	4	C	TO REMOVE
844	CHERRY-BLACK	Prunus serotina	10	2	C	TO REMOVE
845	BOXELDER	Acer negundo	8	3	C	TO REMOVE
846	CHERRY-BLACK	Prunus serotina	10	4	C	TO REMOVE
847	CHERRY-BLACK	Prunus serotina	9	3	C	TO REMOVE
848	CHERRY-BLACK	Prunus serotina	6	4	C	TO REMOVE
849	BOXELDER	Acer negundo	11	4	C	TO REMOVE
850	CHERRY-BLACK	Prunus serotina	9	4	C	TO REMOVE
851	CHERRY-BLACK	Prunus serotina	10	4	C	TO REMOVE
852	CHERRY-BLACK	Prunus serotina	10	4	C	TO REMOVE
853	BOXELDER	Acer negundo	10	3	C	TO REMOVE
854	CHERRY-BLACK	Prunus serotina				

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TREE_NUM	SPECIES	LATIN	DBH	CONDITION	LIBERTYVILLE TREE-GROUP	STATUS
1126	CHERRY-BLACK	Prunus serotina	8	3	C	TO REMOVE
1127	BOXELDER	Acer negundo	10	4	C	TO REMOVE
1128	BOXELDER	Acer negundo	13	4	C	TO REMOVE
1129	CHERRY-BLACK	Prunus serotina	9	4	C	TO REMOVE
1130	MULBERRY-SP	Morus spp	6	3	D	TO REMOVE
1131	CHERRY-BLACK	Prunus serotina	8	4	C	TO REMOVE
1132	BOXELDER	Acer negundo	19	4	C	TO REMOVE
1133	CHERRY-BLACK	Prunus serotina	7	4	C	TO REMOVE
1134	CHERRY-BLACK	Prunus serotina	12	4	C	TO REMOVE
1135	COTTONWOOD	Populus deltoides	11	4	C	TO REMOVE
1136	ELM-AMERICAN	Ulmus americana	14	2	C	TO REMOVE
1137	MAPLE-SILVER	Acer saccharinum	13	3	B	TO REMOVE
1138	BOXELDER	Acer negundo	21	4	C	TO REMOVE
1139	CHERRY-BLACK	Prunus serotina	11	2	C	TO REMOVE
1140	CHERRY-BLACK	Prunus serotina	8	4	C	TO REMOVE
1141	CHERRY-BLACK	Prunus serotina	16	3	C	TO REMOVE
1142	CHERRY-BLACK	Prunus serotina	10	4	C	TO REMOVE
1143	CHERRY-BLACK	Prunus serotina	6	4	C	TO REMOVE
1144	CHERRY-BLACK	Prunus serotina	6	4	C	TO REMOVE
1145	WALNUT-BLACK	Juglans nigra	10	3	B	TO REMOVE
1146	CHERRY-BLACK	Prunus serotina	10	3	C	TO REMOVE
1147	ELM-AMERICAN	Ulmus americana	6	3	C	TO REMOVE
1148	CHERRY-BLACK	Prunus serotina	11	3	C	TO REMOVE
1149	COTTONWOOD	Populus deltoides	10	3	C	TO REMOVE
1150	COTTONWOOD	Populus deltoides	16	2	C	TO REMOVE
1151	CHERRY-BLACK	Prunus serotina	6	4	C	TO REMOVE
1152	BOXELDER	Acer negundo	10	3	C	TO REMOVE
1153	BOXELDER	Acer negundo	16	4	C	TO REMOVE
1154	CHERRY-BLACK	Prunus serotina	14	3	C	TO REMOVE
1155	COTTONWOOD	Populus deltoides	21	2	C	TO REMOVE
1156	BOXELDER	Acer negundo	17	3	C	TO REMOVE
1157	EASTERN REDCEDAR	Juniperus virginiana	6	3	D	TO REMOVE
1158	CHERRY-BLACK	Prunus serotina	11	3	C	TO REMOVE
1159	APPLE-CRAB-SP	Malus spp	11	5	A	TO REMOVE
1160	MAPLE-SILVER	Acer saccharinum	20	1	B	TO REMOVE
1161	COTTONWOOD	Populus deltoides	7	3	C	TO REMOVE
1162	CHERRY-BLACK	Prunus serotina	9	3	C	TO REMOVE
1163	ELM-AMERICAN	Ulmus americana	9	4	C	TO REMOVE
1164	COTTONWOOD	Populus deltoides	8	4	C	TO REMOVE
1165	COTTONWOOD	Populus deltoides	8	3	C	TO REMOVE
1166	MAPLE-SILVER	Acer saccharinum	17	3	B	TO REMOVE
1167	CHERRY-BLACK	Prunus serotina	9	3	C	TO REMOVE
1168	CHERRY-BLACK	Prunus serotina	9	3	C	TO REMOVE
1169	COTTONWOOD	Populus deltoides	7	3	C	TO REMOVE
1170	MULBERRY-SP	Morus spp	17	4	D	TO REMOVE
1171	COTTONWOOD	Populus deltoides	13	3	C	TO REMOVE
1172	BOXELDER	Acer negundo	8	4	C	TO REMOVE
1173	CHERRY-BLACK	Prunus serotina	9	3	C	TO REMOVE
1174	MAPLE-SILVER	Acer saccharinum	14	3	B	TO REMOVE
1175	CHERRY-BLACK	Prunus serotina	11	3	C	TO REMOVE
1176	CHERRY-BLACK	Prunus serotina	11	3	C	TO REMOVE
1177	CHERRY-BLACK	Prunus serotina	10	4	C	TO REMOVE
1178	CHERRY-BLACK	Prunus serotina	22	4	C	TO REMOVE
1179	CHERRY-BLACK	Prunus serotina	16	4	C	TO REMOVE
1180	MAPLE-SILVER	Acer saccharinum	30	2	B	TO REMOVE
1181	CHERRY-BLACK	Prunus serotina	15	4	C	TO REMOVE
1182	MAPLE-SILVER	Acer saccharinum	15	2	C	TO REMOVE
1183	CHERRY-BLACK	Prunus serotina	7	5	C	TO REMOVE
1184	MAPLE-SILVER	Acer saccharinum	17	3	B	TO REMOVE
1185	MAPLE-SILVER	Acer saccharinum	16	2	B	TO REMOVE
1186	CHERRY-BLACK	Prunus serotina	8	3	C	TO REMOVE
1187	CHERRY-BLACK	Prunus serotina	15	3	C	TO REMOVE
1188	MAPLE-SILVER	Acer saccharinum	24	3	B	TO REMOVE
1189	CHERRY-BLACK	Prunus serotina	12	4	C	TO REMOVE
1190	MAPLE-SILVER	Acer saccharinum	28	3	B	TO REMOVE
1191	BOXELDER	Acer negundo	10	4	C	TO REMOVE
1192	BOXELDER	Acer negundo	12	4	C	TO REMOVE
1193	BOXELDER	Acer negundo	21	4	C	TO REMOVE
1194	CHERRY-BLACK	Prunus serotina	13	3	C	TO REMOVE
1195	CHERRY-BLACK	Prunus serotina	6	3	C	TO REMOVE
1196	BOXELDER	Acer negundo	14	4	C	TO REMOVE
1197	BOXELDER	Acer negundo	6	3	C	TO REMOVE
1198	WALNUT-BLACK	Juglans nigra	13	3	B	TO REMOVE
1199	MAPLE-SILVER	Acer saccharinum	21	3	B	TO REMOVE
1200	COTTONWOOD	Populus deltoides	11	3	C	TO REMOVE
1201	CHERRY-BLACK	Prunus serotina	15	2	C	TO REMOVE
1202	COTTONWOOD	Populus deltoides	10	3	C	TO REMOVE
1203	CHERRY-BLACK	Prunus serotina	11	5	C	TO REMOVE
1204	BOXELDER	Acer negundo	10	4	C	TO REMOVE
1205	MAPLE-SILVER	Acer saccharinum	18	3	B	TO REMOVE
1206	COTTONWOOD	Populus deltoides	9	4	C	TO REMOVE
1207	COTTONWOOD	Populus deltoides	4	4	C	TO REMOVE
1208	CHERRY-BLACK	Prunus serotina	15	3	C	TO REMOVE
1209	BOXELDER	Acer negundo	23	4	C	TO REMOVE
1210	COTTONWOOD	Populus deltoides	10	4	C	TO REMOVE
1211	COTTONWOOD	Populus deltoides	12	4	C	TO REMOVE
1212	BOXELDER	Acer negundo	17	5	C	TO REMOVE
1213	CHERRY-BLACK	Prunus serotina	12	3	C	TO REMOVE
1214	COTTONWOOD	Populus deltoides	11	4	C	TO REMOVE
1215	COTTONWOOD	Populus deltoides	6	4	C	TO REMOVE
1216	BOXELDER	Acer negundo	22	3	C	TO REMOVE
1217	BOXELDER	Acer negundo	10	4	C	TO REMOVE
1218	BOXELDER	Acer negundo	9	5	C	TO REMOVE
1219	BOXELDER	Acer negundo	18	3	C	TO REMOVE
1220	COTTONWOOD	Populus deltoides	5	4	C	TO REMOVE
1221	BOXELDER	Acer negundo	11	4	C	TO REMOVE
1222	MAPLE-SILVER	Acer saccharinum	6	5	B	TO REMOVE
1223	ELM-AMERICAN	Ulmus americana	12	4	C	TO REMOVE
1224	COTTONWOOD	Populus deltoides	12	3	C	TO REMOVE
1225	CHERRY-BLACK	Prunus serotina	10	5	C	TO REMOVE
1226	MAPLE-SILVER	Acer saccharinum	25	2	B	TO REMOVE
1227	BOXELDER	Acer negundo	18	3	C	TO REMOVE
1228	BOXELDER	Acer negundo	11	4	C	TO REMOVE
1229	BOXELDER	Acer negundo	11	4	C	TO REMOVE
1230	COTTONWOOD	Populus deltoides	16	3	C	TO REMOVE
1231	BOXELDER	Acer negundo	12	4	C	TO REMOVE
1232	HAWTHORN-COCKSPUR	Crataegus crusgalli	4	3	A	TO REMOVE
1233	COTTONWOOD	Populus deltoides	14	4	C	TO REMOVE
1234	BOXELDER	Acer negundo	17	4	C	TO REMOVE
1235	ELM-AMERICAN	Ulmus americana	10	2	C	TO REMOVE
1236	CHERRY-BLACK	Prunus serotina	16	3	C	TO REMOVE
1237	BOXELDER	Acer negundo	13	3	C	TO REMOVE
1238	COTTONWOOD	Populus deltoides	9	3	C	TO REMOVE
1239	MULBERRY-SP	Morus spp	7	3	D	TO REMOVE
1240	BOXELDER	Acer negundo	14	4	C	TO REMOVE
1241	BOXELDER	Acer negundo	15	4	C	TO REMOVE
1242	MAPLE-SILVER	Acer saccharinum	13	2	B	TO REMOVE
1243	CHERRY-BLACK	Prunus serotina	11	4	C	TO REMOVE
1244	COTTONWOOD	Populus deltoides	6	3	C	TO REMOVE
1245	BOXELDER	Acer negundo	17	4	C	TO REMOVE
1246	MAPLE-SILVER	Acer saccharinum	16	3	B	TO REMOVE
1247	BOXELDER	Acer negundo	12	4	C	TO REMOVE

TREE_NUM	SPECIES	LATIN	DBH	CONDITION	LIBERTYVILLE TREE-GROUP	STATUS
1248	MAPLE-SILVER	Acer saccharinum	20	1	B	TO REMOVE
1249	CHERRY-BLACK	Prunus serotina	18	4	C	TO REMOVE
1250	BOXELDER	Acer negundo	16	3	C	TO REMOVE
1251	MAPLE-SILVER	Acer saccharinum	19	3	B	TO REMOVE
1252	BOXELDER	Acer negundo	11	4	C	TO REMOVE
1253	COTTONWOOD	Populus deltoides	10	4	C	TO REMOVE
1254	CHERRY-BLACK	Prunus serotina	6	5	C	TO REMOVE
1255	CHERRY-BLACK	Prunus serotina	12	4	C	TO REMOVE
1256	MAPLE-SILVER	Acer saccharinum	21	2	B	TO REMOVE
1257	COTTONWOOD	Populus deltoides	10	3	C	TO REMOVE
1258	COTTONWOOD	Populus deltoides	11	3	C	TO REMOVE
1259	BOXELDER	Acer negundo	13	4	C	TO REMOVE
1260	MAPLE-SILVER	Acer saccharinum	22	2	B	TO REMOVE
1261	COTTONWOOD	Populus deltoides	6	4	C	TO REMOVE
1262	MAPLE-SILVER	Acer saccharinum	19	4	B	TO REMOVE
1263	BOXELDER	Acer negundo	9	4	C	TO REMOVE
1264	COTTONWOOD	Populus deltoides	10	3	C	TO REMOVE
1265	MAPLE-SILVER	Acer saccharinum	22	3	B	TO REMOVE
1266	COTTONWOOD	Populus deltoides	9	3	C	TO REMOVE
1267	COTTONWOOD	Populus deltoides	18	4	C	TO REMOVE
1268	CHERRY-BLACK	Prunus serotina	20	3	C	TO REMOVE
1269	COTTONWOOD	Populus deltoides	11	3	C	TO REMOVE
1270	BOXELDER	Acer negundo	8	4	C	TO REMOVE
1271	CHERRY-BLACK	Prunus serotina	13	3	C	TO REMOVE
1272	CHERRY-BLACK	Prunus serotina	12	4	C	TO REMOVE
1273	CHERRY-BLACK	Prunus serotina	6	4	C	TO REMOVE
1274	MAPLE-SILVER	Acer saccharinum	24	2	B	TO REMOVE
1275	CHERRY-BLACK	Prunus serotina	16	3	C	TO REMOVE
1276	MAPLE-SILVER	Acer saccharinum	21	4	B	TO REMOVE
1277	CHERRY-BLACK	Prunus serotina	10	4	C	TO REMOVE
1278	BOXELDER	Acer negundo	10	4	C	TO REMOVE
1279	COTTONWOOD	Populus deltoides	12	3	C	TO REMOVE
1280	CHERRY-BLACK	Prunus serotina	8	3	C	TO REMOVE
1281	CHERRY-BLACK	Prunus serotina	12	4	C	TO REMOVE
1282	COTTONWOOD	Populus deltoides	9	3	C	TO REMOVE
1283	CHERRY-BLACK	Prunus serotina	14	4	C	TO REMOVE
1284	COTTONWOOD	Populus deltoides	10	3	C	TO REMOVE
1285	BOXELDER	Acer negundo	18	4	C	TO REMOVE
1286	CHERRY-BLACK	Prunus serotina	12	4	C	TO REMOVE
1287	COTTONWOOD	Populus deltoides	8	3	C	TO REMOVE
1288	BOXELDER	Acer negundo	8	4	C	TO REMOVE
1289	COTTONWOOD	Populus deltoides	10	3	C	TO REMOVE
1290	CHERRY-BLACK	Prunus serotina	22	4	C	TO REMOVE
1291	MAPLE-SILVER	Acer saccharinum	10	5	B	TO REMOVE
1292	BOXELDER	Acer negundo	7	4	C	TO REMOVE
1293	CHERRY-BLACK	Prunus serotina	6	3	C	TO REMOVE
1294	MAPLE-SILVER	Acer saccharinum	41	3	B	TO REMOVE
1295	COTTONWOOD	Populus deltoides	15	4	C	TO REMOVE
1296	MAPLE-SILVER	Acer saccharinum	13	3	B	TO REMOVE
1297	MAPLE-SILVER	Acer saccharinum	17	3	B	TO REMOVE
1298	ELM-AMERICAN	Ulmus americana	6	3	C	TO REMOVE
1299	MAPLE-SILVER	Acer saccharinum	25	2	B	TO REMOVE
1300	CHERRY-BLACK	Prunus serotina	10	3	C	TO REMOVE
1301	COTTONWOOD	Populus deltoides	9	3	C	TO REMOVE
1302	MAPLE-SILVER	Acer saccharinum	18	4	B	TO REMOVE
1303	BOXELDER	Acer negundo	11	4	C	TO REMOVE
1304	MAPLE-SILVER	Acer saccharinum	25	4	C	TO REMOVE
1305	CHERRY-BLACK	Prunus serotina	7	4	C	TO REMOVE
1306	MAPLE-SILVER	Acer saccharinum	11	4	B	TO REMOVE
1307	COTTONWOOD	Populus deltoides	11	2	C	TO REMOVE
1308	BOXELDER	Acer negundo	11	4	C	TO REMOVE
1309	COTTONWOOD	Populus deltoides	6	3	C	TO REMOVE
1310	BOXELDER	Acer negundo	17	4	C	TO REMOVE
1311	CHERRY-BLACK	Prunus serotina	14	3	C	TO REMOVE
1312	COTTONWOOD	Populus deltoides	10	2	C	TO REMOVE
1313	BOXELDER	Acer negundo	19	4	C	TO REMOVE
1314	CHERRY-BLACK	Prunus serotina	16	3	C	TO REMOVE
1315	BOXELDER	Acer negundo	17	3	C	TO REMOVE
1316	CHERRY-BLACK	Prunus serotina	8	4	C	TO REMOVE
1317	BOXELDER	Acer negundo	12	4	C	TO REMOVE
1318	MAPLE-SILVER	Acer saccharinum	32	3	B	TO REMOVE
1319	CHERRY-BLACK	Prunus serotina	15	3	C	TO REMOVE
1320	CHERRY-BLACK	Prunus serotina	20	3	C	TO REMOVE
1321	CHERRY-BLACK	Prunus serotina	9	3	C	TO REMOVE
1322	MAPLE-SILVER	Acer saccharinum	12	3	C	TO REMOVE
1323	BOXELDER	Acer negundo	14	4	C	TO REMOVE
1324	MAPLE-SILVER	Acer saccharinum	42	2	B	TO REMOVE
1325	MAPLE-SILVER	Acer saccharinum	8	3	B	TO REMOVE
1326	BOXELDER	Acer negundo	21	4	C	TO REMOVE
1327	CHERRY-BLACK	Prunus serotina	17	3	C	TO REMOVE
1328	CHERRY-BLACK	Prunus serotina	10	3	C	TO REMOVE
1329	MAPLE-SILVER	Acer saccharinum	21	2	B	TO REMOVE
1330	CHERRY-BLACK	Prunus serotina	6	4	C	TO REMOVE
1331	MAPLE-SILVER	Acer saccharinum	22	2	B	TO REMOVE
1332	MAPLE-SILVER	Acer saccharinum	25	3	B	TO REMOVE
1333	MAPLE-SILVER	Acer saccharinum	9	3	B	TO REMOVE
1334	MAPLE-SILVER	Acer saccharinum	22	2	B	TO REMOVE
1335	MAPLE-SILVER	Acer saccharinum	22	3	B	TO REMOVE
1336	MAPLE-SILVER	Acer saccharinum	32	2	B	TO REMOVE
1337	CHERRY-BLACK	Prunus serotina	17	3	C	TO REMOVE
1338	BOXELDER	Acer negundo	15	4		

Drawing name: \\GIS_LDEV\B2024000_Plan\Drawings\T1.9_PRESERVATION_Plan.dwg T1.9 May 16, 2024 1:32pm By: David Gove
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TREE_NUM	SPECIES	LATIN	DBH	CONDITION	LIBERTYVILLE TREE-GROUP	STATUS
1614	MAPLE SILVER	Acer saccharinum	18	3	B	TO REMOVE
1615	BOXELDER	Acer negundo	19	4	C	TO REMOVE
1616	BOXELDER	Acer negundo	23	4	C	TO REMOVE
1617	BOXELDER	Acer negundo	10	4	C	TO REMOVE
1618	CHERRY-BLACK	Prunus serotina	10	3	C	TO REMOVE
1619	BOXELDER	Acer negundo	14	4	C	TO REMOVE
1620	CHERRY-BLACK	Prunus serotina	6	4	C	TO REMOVE
1621	BOXELDER	Acer negundo	12	4	C	TO REMOVE
1622	BOXELDER	Acer negundo	21	4	C	TO REMOVE
1623	CHERRY-BLACK	Prunus serotina	12	4	C	TO REMOVE
1624	CHERRY-BLACK	Prunus serotina	14	4	C	TO REMOVE
1625	MAPLE SILVER	Acer saccharinum	24	2	B	TO REMOVE
1626	CHERRY-BLACK	Prunus serotina	10	4	C	TO REMOVE
1627	CHERRY-BLACK	Prunus serotina	6	4	C	TO REMOVE
1628	MULBERRY-SPP	Morus spp	10	4	D	TO REMOVE
1629	BOXELDER	Acer negundo	23	3	C	TO REMOVE
1630	BOXELDER	Acer negundo	8	4	C	TO REMOVE
1631	BOXELDER	Acer negundo	13	4	C	TO REMOVE
1632	MAPLE SILVER	Acer saccharinum	17	3	B	TO REMOVE
1633	MAPLE SILVER	Acer saccharinum	28	2	B	TO REMOVE
1634	CHERRY-BLACK	Prunus serotina	14	4	C	TO REMOVE
1635	BOXELDER	Acer negundo	10	4	C	TO REMOVE
1636	CHERRY-BLACK	Prunus serotina	10	4	C	TO REMOVE
1637	BOXELDER	Acer negundo	14	4	C	TO REMOVE
1638	MAPLE SILVER	Acer saccharinum	19	4	B	TO REMOVE
1639	MAPLE SILVER	Acer saccharinum	15	3	B	TO REMOVE
1640	MAPLE SILVER	Acer saccharinum	20	3	B	TO REMOVE
1641	MAPLE SILVER	Acer saccharinum	31	2	B	TO REMOVE
1642	BOXELDER	Acer negundo	14	3	C	TO REMOVE
1643	MAPLE SILVER	Acer saccharinum	7	3	B	TO REMOVE
1644	BOXELDER	Acer negundo	17	4	C	TO REMOVE
1645	BOXELDER	Acer negundo	7	4	C	TO REMOVE
1646	BOXELDER	Acer negundo	17	4	C	TO REMOVE
1647	BOXELDER	Acer negundo	19	4	C	TO REMOVE
1648	BOXELDER	Acer negundo	10	4	C	TO REMOVE
1649	WALNUT-BLACK	Juglans nigra	6	3	B	TO SAVE
1650	BOXELDER	Acer negundo	6	4	C	TO REMOVE
1651	BOXELDER	Acer negundo	16	4	C	TO REMOVE
1652	MAPLE SILVER	Acer saccharinum	12	3	B	TO REMOVE
1653	BOXELDER	Acer negundo	16	4	C	TO REMOVE
1654	BOXELDER	Acer negundo	11	4	C	TO REMOVE
1655	MAPLE SILVER	Acer saccharinum	22	3	B	TO REMOVE
1656	BOXELDER	Acer negundo	15	4	C	TO REMOVE
1657	BOXELDER	Acer negundo	7	4	C	TO REMOVE
1658	BOXELDER	Acer negundo	9	4	C	TO SAVE
1659	MULBERRY-SPP	Morus spp	31	4	D	TO REMOVE
1660	MAPLE SILVER	Acer saccharinum	16	3	B	TO SAVE
1661	CHERRY-BLACK	Prunus serotina	11	3	C	TO SAVE
1662	MAPLE SILVER	Acer saccharinum	13	3	B	TO SAVE
1663	BOXELDER	Acer negundo	30	4	C	TO SAVE
1664	MAPLE SILVER	Acer saccharinum	8	3	B	TO SAVE
1665	MAPLE SILVER	Acer saccharinum	8	3	B	TO SAVE
1666	WALNUT-BLACK	Juglans nigra	17	3	B	TO SAVE
1667	MAPLE SILVER	Acer saccharinum	20	3	B	TO SAVE
1668	MAPLE SILVER	Acer saccharinum	12	3	B	TO SAVE
1669	BOXELDER	Acer negundo	8	4	C	TO SAVE
1670	MAPLE SILVER	Acer saccharinum	10	3	B	TO SAVE
1671	BOXELDER	Acer negundo	19	4	C	TO SAVE
1672	HOCKBERRY	Ostrya occidentalis	15	3	A	TO SAVE
1673	MAPLE SILVER	Acer saccharinum	23	3	B	TO SAVE
1674	MAPLE SILVER	Acer saccharinum	13	3	B	TO SAVE
1675	MAPLE SILVER	Acer saccharinum	10	3	B	TO SAVE
1676	BOXELDER	Acer negundo	15	4	C	TO REMOVE
1677	CHERRY-BLACK	Prunus serotina	16	3	C	TO SAVE
1678	CHERRY-BLACK	Prunus serotina	6	4	C	TO SAVE
1679	MAPLE SILVER	Acer saccharinum	6	4	B	TO SAVE
1680	BOXELDER	Acer negundo	13	4	C	TO REMOVE
1681	MAPLE SILVER	Acer saccharinum	13	3	B	TO SAVE
1682	MAPLE SILVER	Acer saccharinum	10	3	B	TO SAVE
1683	MAPLE SILVER	Acer saccharinum	9	4	B	TO REMOVE
1684	MAPLE SILVER	Acer saccharinum	17	2	B	TO SAVE
1685	MAPLE SILVER	Acer saccharinum	21	3	B	TO REMOVE
1686	MAPLE SILVER	Acer saccharinum	6	4	B	TO SAVE
1687	BOXELDER	Acer negundo	13	4	C	TO REMOVE
1688	MAPLE SILVER	Acer saccharinum	24	3	B	TO SAVE
1689	MAPLE SILVER	Acer saccharinum	8	3	B	TO SAVE
1690	CHERRY-BLACK	Prunus serotina	16	3	C	TO REMOVE
1691	CHERRY-BLACK	Prunus serotina	12	3	C	TO SAVE
1692	CHERRY-BLACK	Prunus serotina	12	3	C	TO SAVE
1693	MAPLE SILVER	Acer saccharinum	12	3	B	TO SAVE
1694	CHERRY-BLACK	Prunus serotina	13	4	C	TO REMOVE
1695	CHERRY-BLACK	Prunus serotina	9	4	C	TO REMOVE
1696	BOXELDER	Acer negundo	19	4	C	TO SAVE
1697	MAPLE SILVER	Acer saccharinum	17	2	B	TO SAVE
1698	MAPLE SILVER	Acer saccharinum	29	3	B	TO REMOVE
1699	CHERRY-BLACK	Prunus serotina	16	4	C	TO SAVE
1700	CHERRY-BLACK	Prunus serotina	9	3	C	TO REMOVE
1701	CHERRY-BLACK	Prunus serotina	19	4	C	TO SAVE
1702	MAPLE SILVER	Acer saccharinum	15	2	B	TO SAVE
1703	MAPLE SILVER	Acer saccharinum	16	3	B	TO SAVE
1704	BOXELDER	Acer negundo	12	4	C	TO REMOVE
1705	BOXELDER	Acer negundo	12	3	C	TO SAVE
1706	MAPLE SILVER	Acer saccharinum	24	3	B	TO SAVE
1707	MAPLE SILVER	Acer saccharinum	22	3	B	TO SAVE
1708	MAPLE SILVER	Acer saccharinum	23	3	B	TO SAVE
1709	MAPLE SILVER	Acer saccharinum	10	3	B	TO SAVE
1710	COTTONWOOD	Populus deltoides	16	3	C	TO REMOVE
1711	COTTONWOOD	Populus deltoides	16	3	C	TO REMOVE
1712	MAPLE SILVER	Acer saccharinum	23	3	B	TO SAVE
1713	COTTONWOOD	Populus deltoides	20	3	C	TO REMOVE
1714	MAPLE SILVER	Acer saccharinum	21	3	B	TO SAVE
1715	COTTONWOOD	Populus deltoides	35	3	C	TO REMOVE
1716	MAPLE SILVER	Acer saccharinum	12	3	B	TO SAVE
1717	MAPLE SILVER	Acer saccharinum	17	3	B	TO SAVE
1718	MAPLE SILVER	Acer saccharinum	12	3	B	TO SAVE
1719	WALNUT-BLACK	Juglans nigra	14	2	B	TO SAVE
1720	CHERRY-BLACK	Prunus serotina	18	4	C	TO REMOVE
1721	MAPLE SILVER	Acer saccharinum	15	4	B	TO REMOVE
1722	BOXELDER	Acer negundo	9	3	C	TO REMOVE
1723	CHERRY-BLACK	Prunus serotina	14	5	C	TO REMOVE
1724	MAPLE SILVER	Acer saccharinum	10	3	B	TO REMOVE
1725	CHERRY-BLACK	Prunus serotina	12	4	C	TO REMOVE
1726	MAPLE SILVER	Acer saccharinum	16	3	B	TO SAVE
1727	MAPLE SILVER	Acer saccharinum	36	3	B	TO REMOVE
1728	MAPLE SILVER	Acer saccharinum	11	3	B	TO SAVE
1729	MAPLE SILVER	Acer saccharinum	9	4	B	TO SAVE
1730	MAPLE SILVER	Acer saccharinum	32	3	B	TO REMOVE
1731	MAPLE SILVER	Acer saccharinum	21	2	B	TO SAVE
1732	MAPLE SILVER	Acer saccharinum	22	3	B	TO REMOVE
1733	BOXELDER	Acer negundo	9	4	C	TO REMOVE
1734	WALNUT-BLACK	Juglans nigra	17	3	B	TO SAVE
1735	ELM-AMERICAN	Ulmus americana	13	4	C	TO SAVE

TREE_NUM	SPECIES	LATIN	DBH	CONDITION	LIBERTYVILLE TREE-GROUP	STATUS
1736	MAPLE SILVER	Acer saccharinum	23	2	B	TO REMOVE
1737	BOXELDER	Acer negundo	16	4	C	TO SAVE
1738	ELM-AMERICAN	Ulmus americana	9	3	C	TO SAVE
1739	CHERRY-BLACK	Prunus serotina	13	4	C	TO REMOVE
1740	WALNUT-BLACK	Juglans nigra	13	4	B	TO SAVE
1741	CHERRY-BLACK	Prunus serotina	9	4	C	TO REMOVE
1742	BOXELDER	Acer negundo	8	4	C	TO SAVE
1743	MAPLE SILVER	Acer saccharinum	30	3	B	TO SAVE
1744	MAPLE SILVER	Acer saccharinum	24	2	B	TO SAVE
1745	MAPLE SILVER	Acer saccharinum	32	4	B	TO SAVE
1746	MAPLE SILVER	Acer saccharinum	18	3	B	TO SAVE
1747	MAPLE SILVER	Acer saccharinum	10	3	B	TO SAVE
1748	MAPLE SILVER	Acer saccharinum	11	3	B	TO SAVE
1749	MAPLE SILVER	Acer saccharinum	22	3	B	TO SAVE
1750	MAPLE SILVER	Acer saccharinum	24	3	B	TO SAVE
1751	MAPLE SILVER	Acer saccharinum	17	3	B	TO SAVE
1752	MAPLE SILVER	Acer saccharinum	23	3	B	TO SAVE
1753	MAPLE SILVER	Acer saccharinum	18	3	B	TO SAVE
1754	MAPLE SILVER	Acer saccharinum	17	3	B	TO SAVE
1755	MAPLE SILVER	Acer saccharinum	8	4	B	TO SAVE
1756	MAPLE SILVER	Acer saccharinum	35	3	B	TO SAVE
1757	BOXELDER	Acer negundo	12	4	C	TO SAVE
1758	ELM-AMERICAN	Ulmus americana	19	3	C	TO SAVE
1759	BOXELDER	Acer negundo	8	3	C	TO SAVE
1760	BOXELDER	Acer negundo	10	3	C	TO SAVE
1761	ELM-AMERICAN	Ulmus americana	27	2	C	TO SAVE
1762	BOXELDER	Acer negundo	13	3	C	TO SAVE
1763	BOXELDER	Acer negundo	8	3	C	TO SAVE
1764	POPLAR-WHITE	Populus alba	10	3	D	TO SAVE
1765	BOXELDER	Acer negundo	13	4	C	TO SAVE
1766	BOXELDER	Acer negundo	14	4	C	TO SAVE
1767	BOXELDER	Acer negundo	12	3	C	TO SAVE
1768	BOXELDER	Acer negundo	7	3	C	TO SAVE
1769	COTTONWOOD	Populus deltoides	27	2	C	TO REMOVE
1770	BOXELDER	Acer negundo	6	4	C	TO REMOVE
1771	BOXELDER	Acer negundo	31	4	C	TO REMOVE
1772	COTTONWOOD	Populus deltoides	10	3	C	TO REMOVE
1773	MAPLE SILVER	Acer saccharinum	12	4	B	TO REMOVE
1774	COTTONWOOD	Populus deltoides	19	2	C	TO SAVE
1775	MAPLE SILVER	Acer saccharinum	12	4	B	TO REMOVE
1776	BOXELDER	Acer negundo	6	4	C	TO SAVE
1777	BOXELDER	Acer negundo	6	4	C	TO SAVE
1778	MAPLE SILVER	Acer saccharinum	36	3	B	TO REMOVE
1779	BOXELDER	Acer negundo	12	3	C	TO SAVE
1780	BOXELDER	Acer negundo	28	4	C	TO REMOVE
1781	BOXELDER	Acer negundo	9	3	C	TO SAVE
1782	BOXELDER	Acer negundo	7	4	C	TO REMOVE
1783	MAPLE SILVER	Acer saccharinum	11	3	B	TO REMOVE
1784	BOXELDER	Acer negundo	7	3	C	TO SAVE
1785	MAPLE SILVER	Acer saccharinum	47	2	B	TO REMOVE
1786	BOXELDER	Acer negundo	9	3	C	TO SAVE
1787	BOXELDER	Acer negundo	17	3	C	TO SAVE
1788	CHERRY-BLACK	Prunus serotina	9	4	C	TO REMOVE
1789	CHERRY-BLACK	Prunus serotina	9	4	C	TO REMOVE
1790	BOXELDER	Acer negundo	12	3	C	TO SAVE
1791	CHERRY-BLACK	Prunus serotina	10	4	C	TO REMOVE
1792	BOXELDER	Acer negundo	6	3	C	TO SAVE
1793	CHERRY-BLACK	Prunus serotina	10	4	C	TO REMOVE
1794	MAPLE CRAB-SPP	Malus spp	10	4	A	TO REMOVE
1795	ELM-AMERICAN	Ulmus americana	10	3	C	TO SAVE
1796	BOXELDER	Acer negundo	8	3	C	TO SAVE
1797	MAPLE SILVER	Acer saccharinum	28	2	B	TO REMOVE
1798	BOXELDER	Acer negundo	11	3	C	TO SAVE
1799	MAPLE SILVER	Acer saccharinum	11	3	B	TO REMOVE
1800	MAPLE SILVER	Acer saccharinum	16	3	B	TO REMOVE
1801	BOXELDER	Acer negundo	9	3	C	TO SAVE
1802	MAPLE SILVER	Acer saccharinum	13	3	B	TO REMOVE
1803	MULBERRY-SPP	Morus spp	17	3	D	TO REMOVE
1804	MAPLE SILVER	Acer saccharinum	20	3	B	TO REMOVE
1805	COTTONWOOD	Populus deltoides	17	2	C	TO SAVE
1806	MAPLE SILVER	Acer saccharinum	26	3	B	TO REMOVE
1807	BOXELDER	Acer negundo	14	3	C	TO SAVE
1808	CHERRY-BLACK	Prunus serotina	13	4	C	TO REMOVE
1809	CHERRY-BLACK	Prunus serotina	11	4	C	TO REMOVE
1810	MULBERRY-SPP	Morus spp	12	4	D	TO REMOVE
1811	MULBERRY-SPP	Morus spp	8	3	D	TO SAVE
1812	MAPLE SILVER	Acer saccharinum	25	3	B	TO REMOVE
1813	BOXELDER	Acer negundo	6	3	C	TO SAVE
1814	MULBERRY-SPP	Morus spp	12	3	D	TO SAVE
1815	MAPLE SILVER	Acer saccharinum	24	3	B	TO REMOVE
1816	BOXELDER	Acer negundo	11	3	C	TO SAVE
1817	BOXELDER	Acer negundo	15	3	C	TO SAVE
1818	BOXELDER	Acer negundo	9	3	C	TO SAVE
1819	BOXELDER	Acer negundo	14	3	C	TO SAVE
1820	CHERRY-BLACK	Prunus serotina	10	4	C	TO REMOVE
1821	CHERRY-BLACK	Prunus serotina	11	4	C	TO REMOVE
1822	MAPLE SILVER	Acer saccharinum	9	3	B	TO SAVE
1823	BOXELDER	Acer negundo	9	4	C	TO REMOVE
1824	MULBERRY-SPP	Morus spp	11	3	D	TO SAVE
1825	BOXELDER	Acer negundo	13	3	C	TO SAVE
1826	CHERRY-BLACK	Prunus serotina	21	4	C	TO REMOVE
1827	BOXELDER	Acer negundo	17	3	C	TO SAVE
1828	CHERRY-BLACK	Prunus serotina	20	4	C	TO REMOVE
1829	MAPLE SILVER	Acer saccharinum	17	3	B	TO REMOVE
1						

Drawing name: K:\GIS\DEV\168247001_Libertyville_IL2_Design\CA2\PlantSheet\L1.0_LANDSCAPE_PLAN.dwg L1.0 LANDSCAPE PLAN May 16, 2025 1:32pm by: Daniel Crowe
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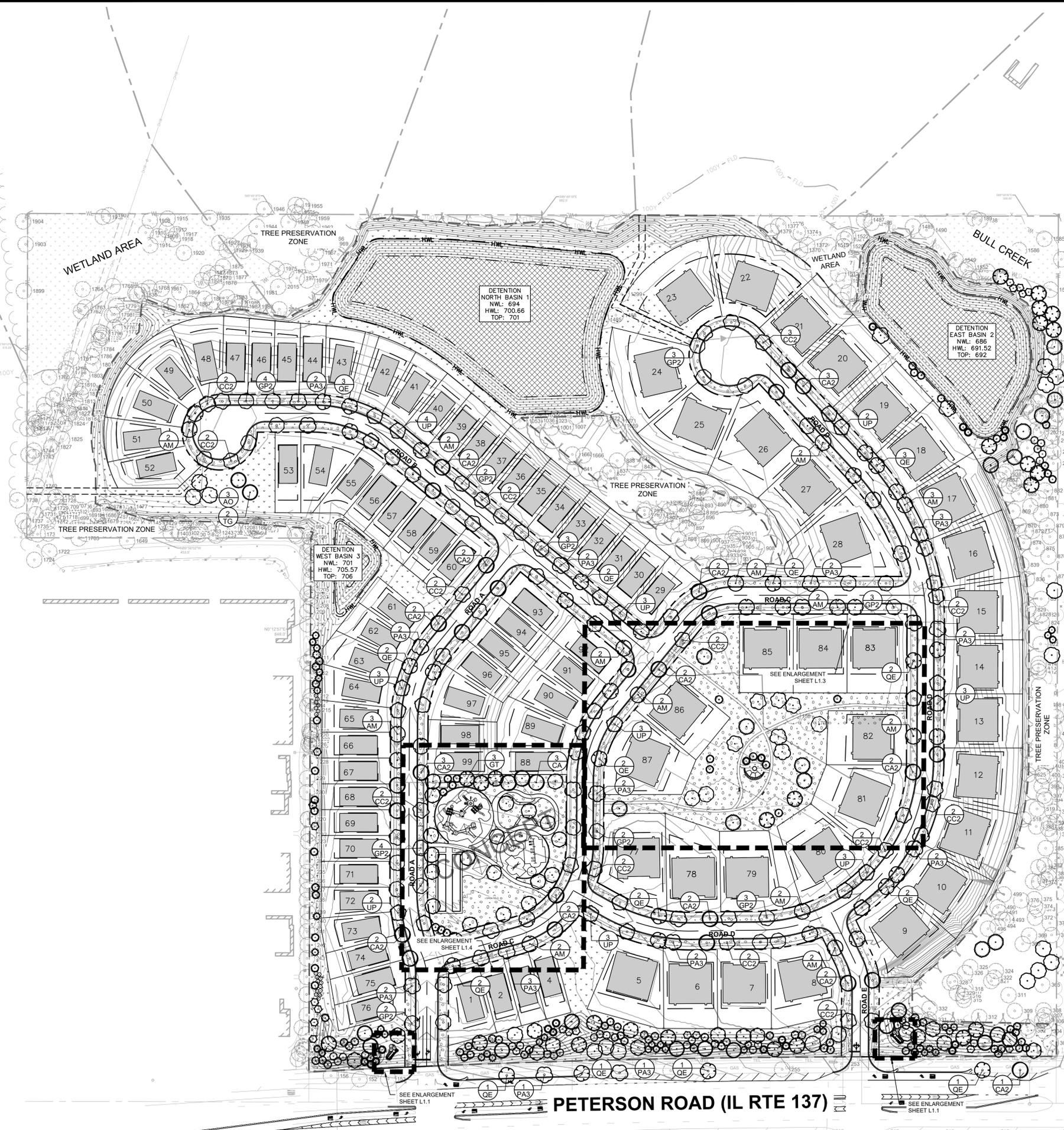
PULTE HOME COMPANY, LLC

LANDSCAPE PLAN

GREENWAY CHASE
 610 PETERSON ROAD
 LIBERTYVILLE, IL 60048

ORIGINAL ISSUE:
 01/03/2025
 KHA PROJECT NO.
 168247001

SHEET NUMBER

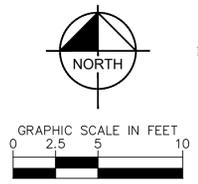
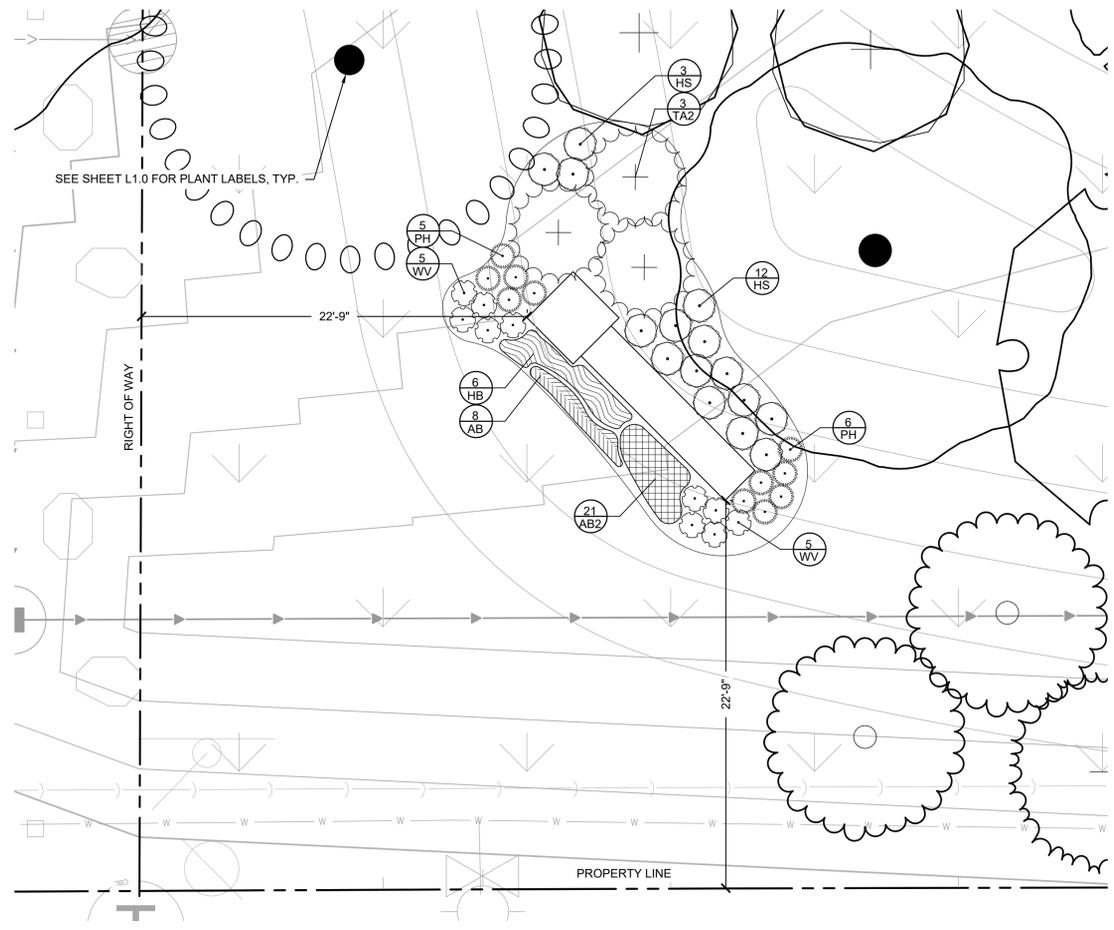
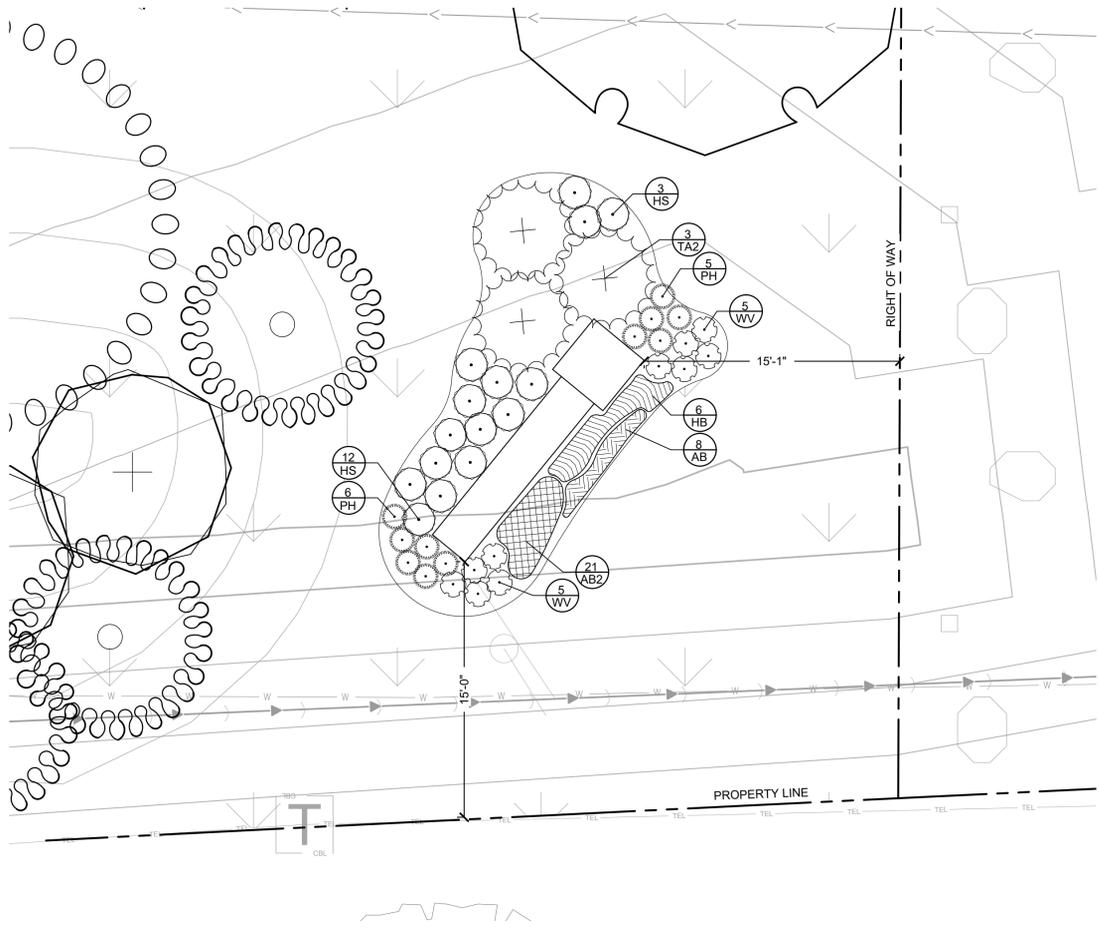


SYMBOL	BOTANICAL / COMMON NAME
	CRUSHED STONE
	PIZZO DRY BOTTOM DETENTION SEED MIX
	PIZZO LOW PROFILE PRAIRIE SEED MIX
	PIZZO WET-MESIC PRAIRIE SEED MIX
	TURF SEED

Libertyville, IL - LANDSCAPE CODE		
ORDINANCE	REQUIRED	PROVIDED
ARTICLE 6: SEC 22-113 LANDSCAPING IN SUBDIVISIONS TREES SHALL BE PLANTED WITH AT LEAST ONE (1) TREE PLANTED PER LOT OR PARCEL AND AT LEAST ONE (1) TREE EVERY SIXTY (60) FEET OR FRACTION THEREOF. TREES SHALL BE CENTERED IN THE PARKWAY, MIDWAY BETWEEN CURB AND SIDEWALK.	ONE (1) TREE PER LOT AND MAXIMUM ONE (1) TREE EVERY SIXTY (60) FEET REQUIRED.	ONE (1) TREE PER LOT AND MAXIMUM ONE (1) TREE EVERY SIXTY (60) FEET PROVIDED
ARTICLE 11: SEC 3-11 SIGNS FOR EVERY ONE (1) SQUARE FOOT OF GROSS SIGN SURFACE AREA, THERE SHALL BE PROVIDED ONE (1) SQUARE FOOT OF LANDSCAPE AREA IMMEDIATELY ADJACENT TO THE SIGN BASE	APPROXIMATELY 100 SQUARE FEET OF GROSS SIGN SURFACE AREA PER SIGN = 100 SQUARE FEET OF LANDSCAPE AREA IMMEDIATELY ADJACENT TO THE SIGN BASE REQUIRED FOR EACH SIGN.	288 SQUARE FEET OF LANDSCAPE AREA IMMEDIATELY ADJACENT TO THE SIGN BASE PROVIDED FOR EACH SIGN.

L1.0

Drawing name: K:\GIS_DEVELOPMENT\16827001_Pulte_Libertyville_IL2_Design\CD\PlantSchedule\1.0_LANDSCAPE_PLAN.dwg L1.1 SIGNAGE LANDSCAPE May 16, 2025 1:33pm by: Daniel Drow
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PULTE HOME COMPANY, LLC

SIGNAGE LANDSCAPE

GREENWAY CHASE
 610 PETERSON ROAD
 LIBERTYVILLE, IL 60048

ORIGINAL ISSUE:
 01/03/2025
 KHA PROJECT NO.
 168247001
 SHEET NUMBER

L1.1

PLANT SCHEDULE WEST SIGN

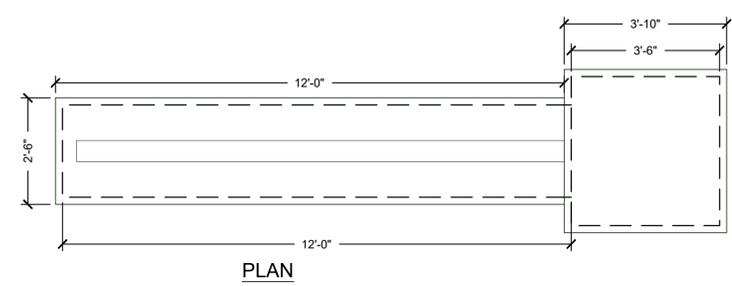
SYMBOL	CODE	QTY	BOTANICAL / COMMON NAME	CONT	CAL	SIZE
ORNAMENTAL TREES						
	CE	1	CERCIS CANADENSIS / EASTERN REDBUD	B & B	-	6" HT. MIN.
SHRUBS						
	HS	15	DIERVILLA X SPLENDENS 'EL MADRIGAL' / FIREFLY NIGHTGLOW BUSH HONEYSUCKLE	-	SEE PLAN	10" HT. MIN.
	WV	10	WEIGELA FLORIDA 'VERWEIG' / MY MONET® WEIGELA	-	SEE PLAN	6" HT. MIN.
EVERGREEN SHRUBS						
	TA2	3	THUJA OCCIDENTALIS 'HOLMSTRUP' / HOLMSTRUP EASTERN ARBORVITAE	-	SEE PLAN	24" HT. MIN.
ORNAMENTAL GRASSES						
	PH	11	PANICUM VIRGATUM 'HEAVY METAL' / HEAVY METAL SWITCH GRASS	-	SEE PLAN	24" HT. MIN.
SHRUB AREAS						
	AB2	21	ACHILLEA MILLEFOLIUM 'BALVINOLET' / NEW VINTAGE™ VIOLET COMMON YARROW	1 GAL.	SEE PLAN	6" HT. MIN.
	AB	8	AMSONIA X 'BLUE ICE' / BLUE ICE BLUESTAR	1 GAL.	SEE PLAN	6" HT. MIN.
	HB	6	HEUCHERA X 'BLACK BEAUTY' / BLACK BEAUTY CORAL BELLS	1 GAL.	SEE PLAN	6" HT. MIN.

PLANT SCHEDULE EAST SIGN

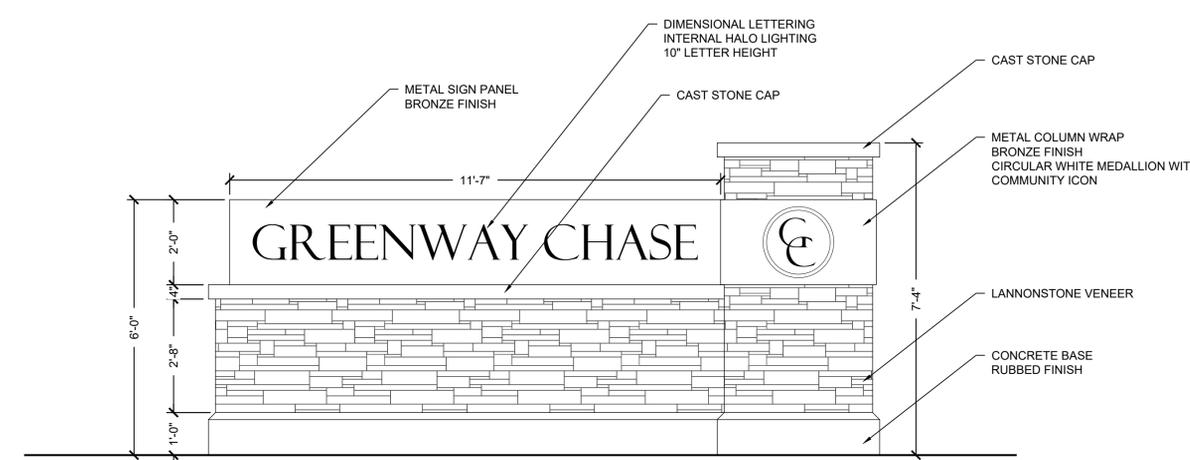
SYMBOL	CODE	QTY	BOTANICAL / COMMON NAME	CONT	CAL	SIZE
CANOPY TREES						
	AO	1	AESCULUS GLABRA / OHIO BUCKEYE	B & B	-	2.5" CAL. MIN.
SHRUBS						
	HS	15	DIERVILLA X SPLENDENS 'EL MADRIGAL' / FIREFLY NIGHTGLOW BUSH HONEYSUCKLE	-	SEE PLAN	10" HT. MIN.
	WV	10	WEIGELA FLORIDA 'VERWEIG' / MY MONET® WEIGELA	-	SEE PLAN	6" HT. MIN.
EVERGREEN SHRUBS						
	TA2	3	THUJA OCCIDENTALIS 'HOLMSTRUP' / HOLMSTRUP EASTERN ARBORVITAE	-	SEE PLAN	24" HT. MIN.
ORNAMENTAL GRASSES						
	PH	11	PANICUM VIRGATUM 'HEAVY METAL' / HEAVY METAL SWITCH GRASS	-	SEE PLAN	24" HT. MIN.
SHRUB AREAS						
	AB2	21	ACHILLEA MILLEFOLIUM 'BALVINOLET' / NEW VINTAGE™ VIOLET COMMON YARROW	1 GAL.	SEE PLAN	6" HT. MIN.
	AB	8	AMSONIA X 'BLUE ICE' / BLUE ICE BLUESTAR	1 GAL.	SEE PLAN	6" HT. MIN.
	HB	6	HEUCHERA X 'BLACK BEAUTY' / BLACK BEAUTY CORAL BELLS	1 GAL.	SEE PLAN	6" HT. MIN.

Drawing name: K:\GIS_DEVELOPMENT\16827001_Pulte_Libertyville_IL2_Design\DWG\16827001_L1.2_SIGNAGE_PLAN.dwg L1.2 SIGNAGE PLAN AND DETAILS May 16, 2025 1:33pm by: Daniel.Grove
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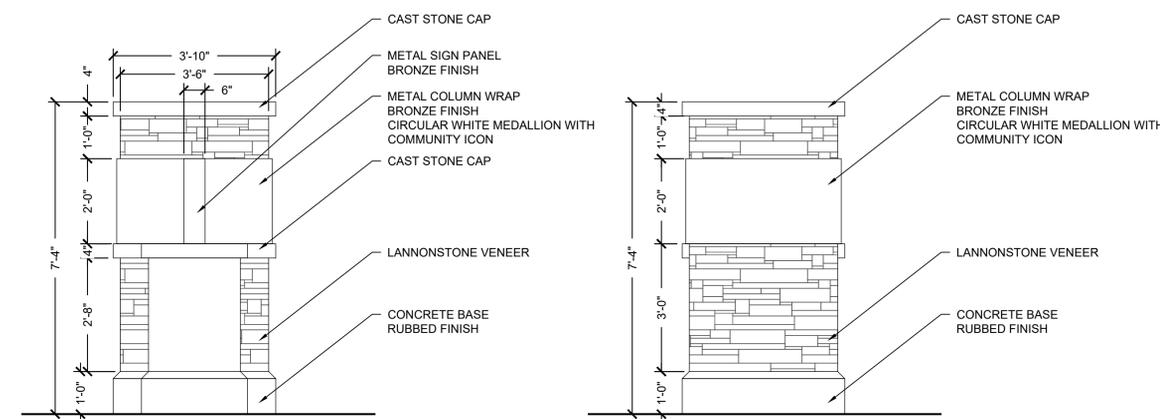


PLAN

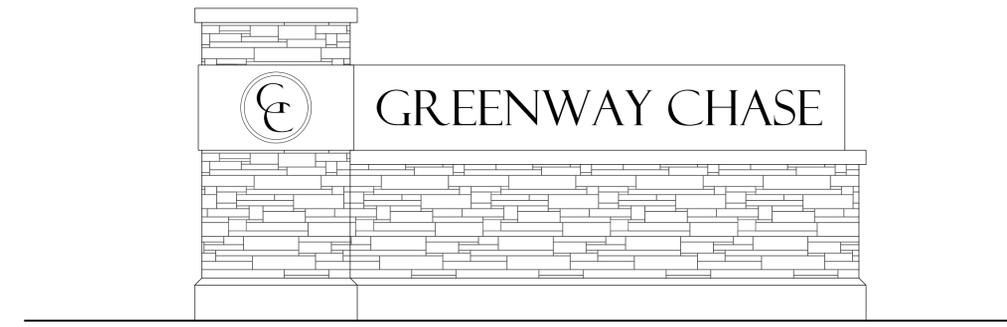


ELEVATION

01 WEST MONUMENT SIGN - FRONT ELEVATION AND PLAN 1/2" = 1'-0"



02 WEST MONUMENT SIGN - SIDE ELEVATIONS 1/2" = 1'-0"



03 EAST MONUMENT SIGN - FRONT ELEVATION 1/2" = 1'-0"

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ORIGINAL ISSUE: 01/03/2025				
KHA PROJECT NO. 168247001				
SHEET NUMBER				
L1.2				

Drawing name: K:\GIS_DEVELOPMENT\168247001_Pulte_Libertyville_IL2_Design\PlantSheet\1.0 LANDSCAPE PLAN.dwg L1.3 PARK ENLARGEMENT May 16, 2025 3:34pm by: DanielDrow
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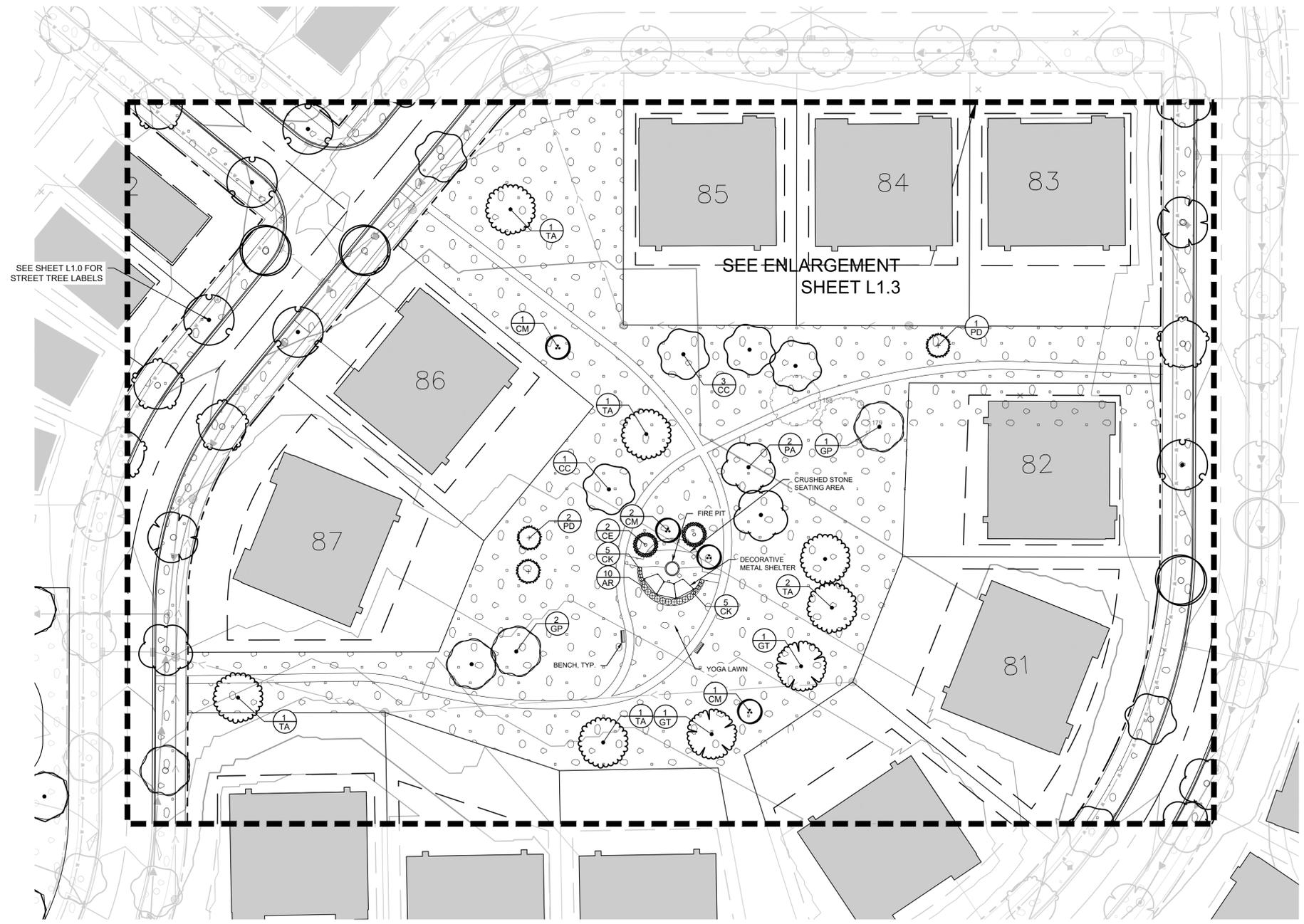

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3	VILLAGE RESUBMITTAL	05/19/25	INS



SYMBOL	BOTANICAL / COMMON NAME
	CRUSHED STONE
	PIZZO DRY BOTTOM DETENTION SEED MIX
	PIZZO LOW PROFILE PRAIRIE SEED MIX
	PIZZO WET-MESIC PRAIRIE SEED MIX

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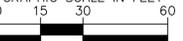
**PULTE HOME
 COMPANY, LLC**

**DUPLEX
 AMENITY
 ENLARGEMENT**

**GREENWAY
 CHASE**
610 PETERSON ROAD
 LIBERTYVILLE, IL 60048

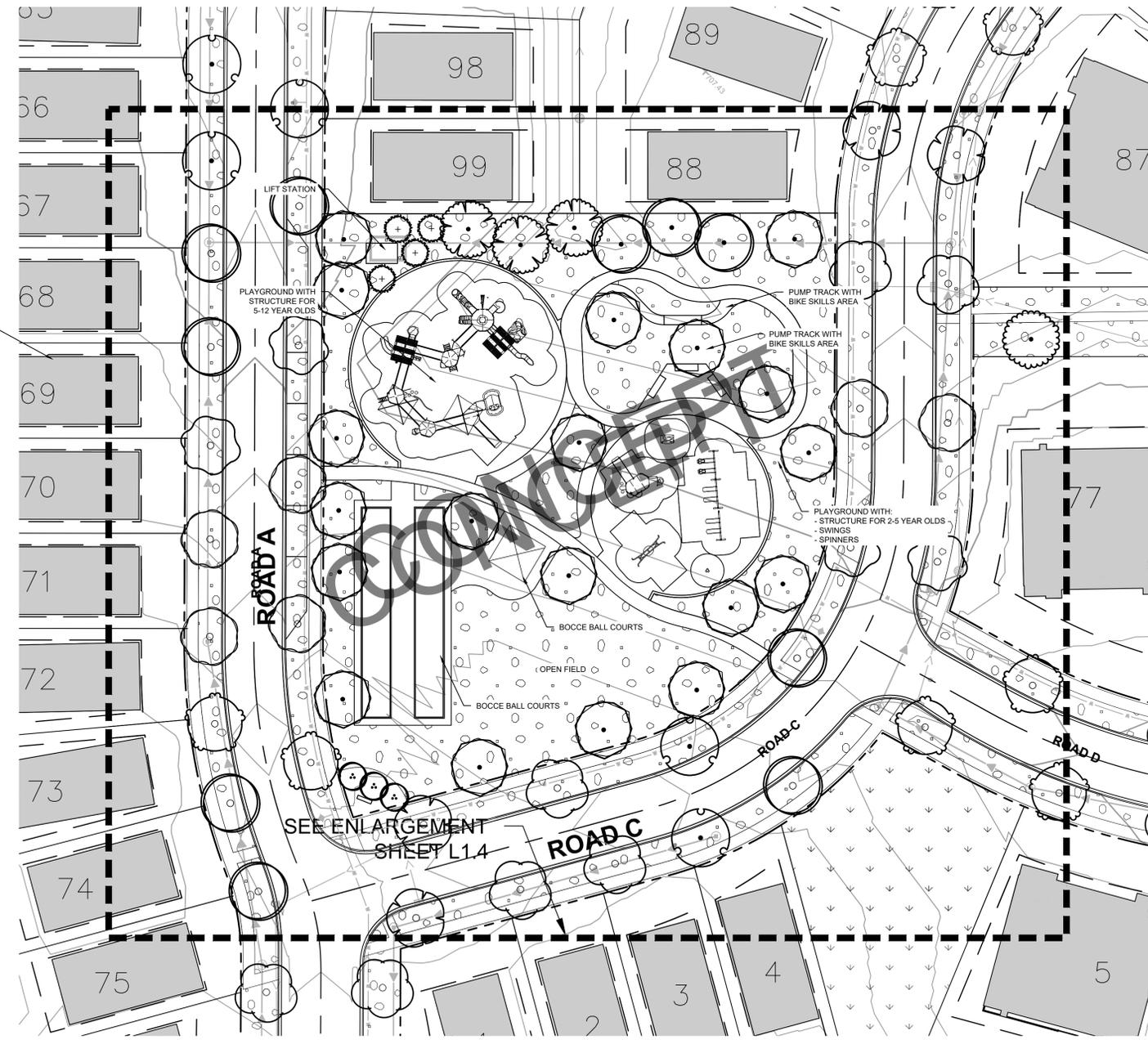
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KHA PROJECT NO. 168247001
SHEET NUMBER L1.3

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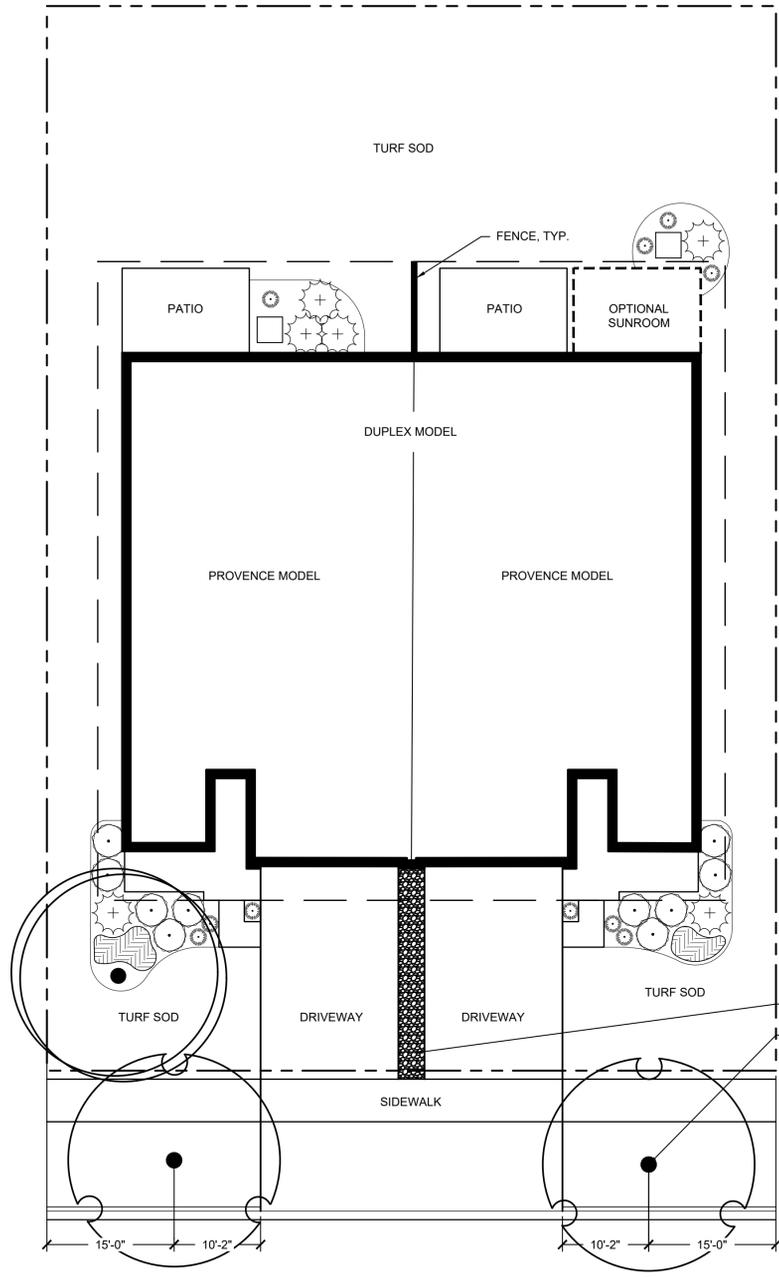


SYMBOL	BOTANICAL / COMMON NAME
	CRUSHED STONE
	PIZZO DRY BOTTOM DETENTION SEED MIX
	PIZZO LOW PROFILE PRAIRIE SEED MIX
	PIZZO WET-MESIC PRAIRIE SEED MIX

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	RNM	03/28/25	VILLAGE RESUBMITTAL	DG
	DG	03/07/25	ARC RE-SUBMITTAL	DATE
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KHA PROJECT NO. 168247001				
SHEET NUMBER				
L1.4				

Drawing name: K:\GIS_DEVELOPMENT\2025\16827001_Pulte_Libertyville_IL2_Design\Concept\PlantSchedule\1.0_LANDSCAPE_PLAN.dwg L1.5 DUPLEX FOUNDATION PROTOTYPES May 16, 2025 1:35pm by: Daniel Gowen
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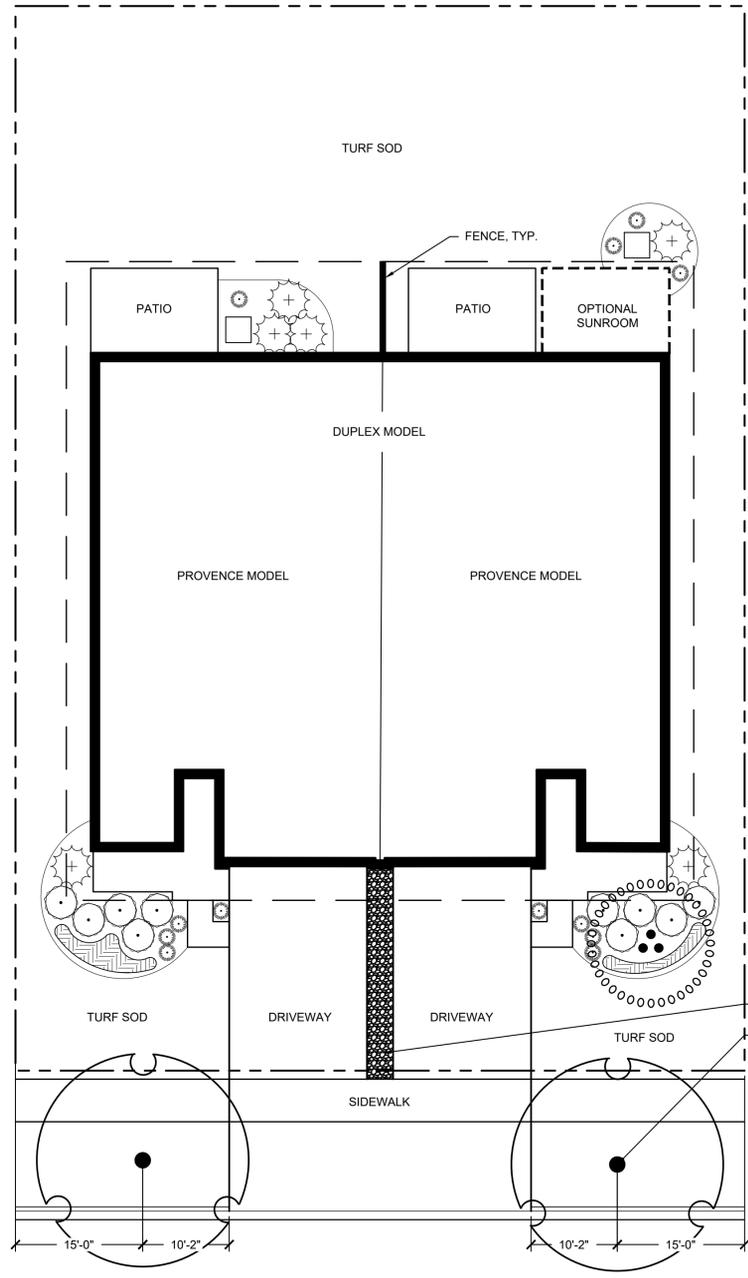
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DUPLEX FAMILY HOME FOUNDATION PLANTING PROTOTYPE - EVEN

CONCEPT PLANT SCHEDULE DUPLEX - EVEN

	CANOPY TREE	1
	MEDIUM SHRUB	10
	EVERGREEN SHRUB	6
	ORNAMENTAL GRASS	10
	PERENNIALS	44 SF



DUPLEX FAMILY HOME FOUNDATION PLANTING PROTOTYPE - ODD

CONCEPT PLANT SCHEDULE DUPLEX - ODD

	ORNAMENTAL TREE	1
	MEDIUM SHRUB	10
	EVERGREEN SHRUB	6
	ORNAMENTAL GRASS	12
	PERENNIALS	47 SF

INS	05/19/25	DATE
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DG	03/07/25	DATE
BY		
REVISIONS		

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PULTE HOME COMPANY, LLC

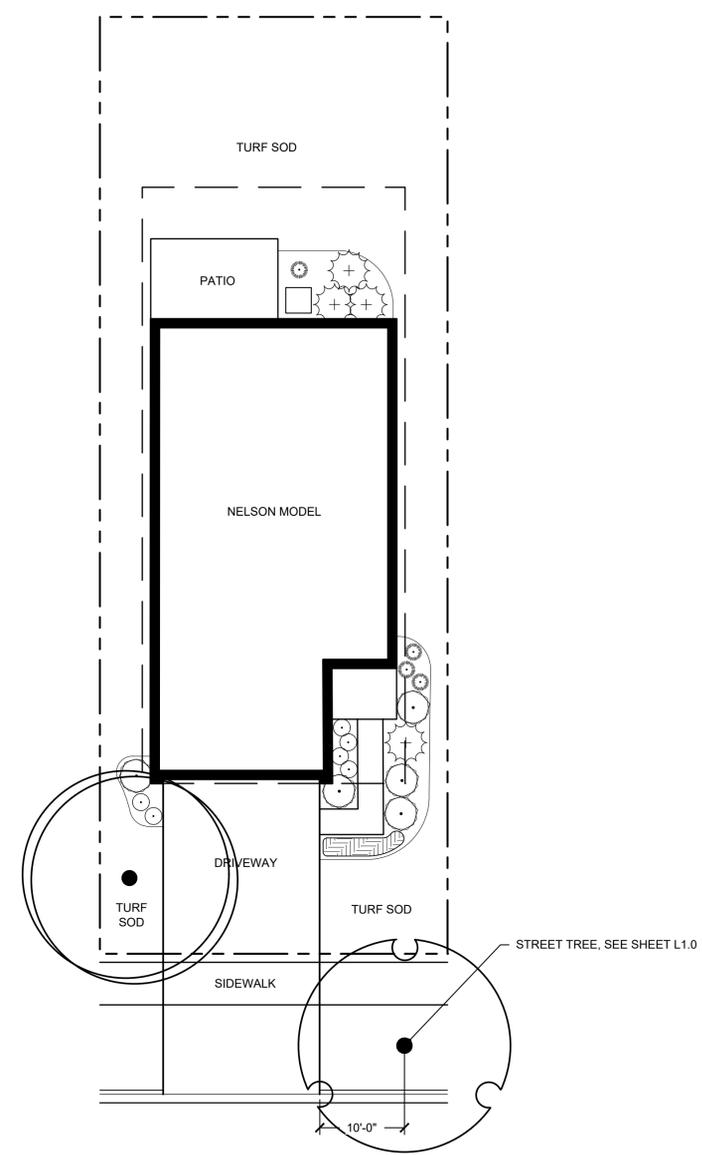
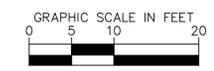
DUPLEX FOUNDATION PROTOTYPES

GREENWAY CHASE
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LIBERTYVILLE, IL 60048

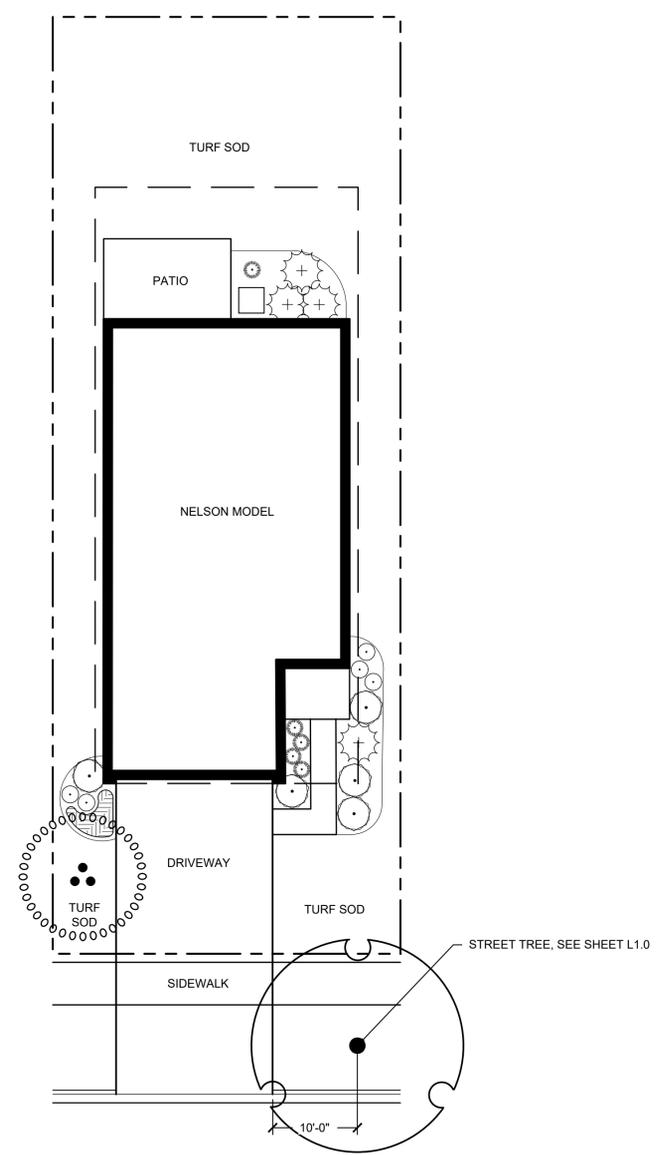
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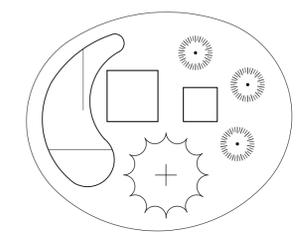
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SINGLE FAMILY HOME FOUNDATION PLANTING PROTOTYPE - EVEN



SINGLE FAMILY HOME FOUNDATION PLANTING PROTOTYPE - ODD



PEDESTAL SCREENING FOR FRONT YARD LOCATION - TYPICAL

CONCEPT PLANT SCHEDULE PEDESTAL SCREENING

	EVERGREEN SHRUB	1
	ORNAMENTAL GRASS	3
	PERENNIALS	24 SF

CONCEPT PLANT SCHEDULE SINGLE FAMILY EVEN

	CANOPY TREE	1
	MEDIUM SHRUB	5
	EVERGREEN SHRUB	4
	ORNAMENTAL GRASS	4
	SMALL SHRUB	6
	PERENNIALS	19 SF

CONCEPT PLANT SCHEDULE SINGLE FAMILY ODD

	ORNAMENTAL TREE	1
	MEDIUM SHRUB	5
	EVERGREEN SHRUB	4
	ORNAMENTAL GRASS	5
	SMALL SHRUB	5
	PERENNIALS	17 SF

INS	05/19/25	DATE
RNM	03/28/25	DATE
DG	03/07/25	DATE
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Kimley»Horn
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SCALE: AS NOTED
DESIGNED BY: INS
DRAWN BY: KTRM
CHECKED BY: RNM

**PULTE HOME
 COMPANY, LLC**

**SINGLE FAMILY
 FOUNDATION
 PROTOTYPES**

**GREENWAY
 CHASE**
 610 PETERSON ROAD
 LIBERTYVILLE, IL 60048

ORIGINAL ISSUE:
 01/03/2025
 KHA PROJECT NO.
 168247001

SHEET NUMBER
L1.6

Drawing name: K:\GIS\DEV\16827001_Pulte_Libertyville_IL2_Design\CAD\PlantSchedule_L2.0_LANDSCAPE_PLAN.dwg L2.0 LANDSCAPE SCHEDULE May 16, 2025 1:35pm by: DanielDrow
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PLANT SCHEDULE

SYMBOL	CODE	QTY	BOTANICAL / COMMON NAME	CONT	CAL	SIZE
CANOPY TREES						
	AM	28	ACER MIYABEI 'MORTON' / STATE STREET™ MIYABE MAPLE	B & B	2.5" CAL. MIN.	
	AO	16	AESCULUS GLABRA / OHIO BUCKEYE	B & B	2.5" CAL. MIN.	
	CA	3	CARPINUS CAROLINIANA / AMERICAN HORNBEAM	B & B	2.5" CAL. MIN.	
	CC	4	CELTIS OCCIDENTALIS 'CHICAGOLAND' / CHICAGOLAND HACKBERRY	B & B	2.5" CAL. MIN.	
	GP	3	GINKGO BILOBA 'PRINCETON SENTRY' / PRINCETON SENTRY MAIDENHAIR TREE	B & B	2.5" CAL. MIN.	
	GT	24	GLEDTISIA TRIACANTHOS F. INERMIS / THORNLESS HONEY LOCUST	B & B	2.5" CAL. MIN.	
	GD	14	GYMNOCLADUS DIOICUS 'ESPRESSO' / KENTUCKY COFFEETREE	B & B	2.5" CAL. MIN.	
	PA	7	PLATANUS X ACERIFOLIA / LONDON PLANE TREE	B & B	2.5" CAL. MIN.	
	TA	14	TILIA AMERICANA / AMERICAN LINDEN	B & B	2.5" CAL. MIN.	
	TG	14	TILIA CORDATA 'GREENSPIRE' / GREENSPIRE LITTLELEAF LINDEN	B & B	2.5" CAL. MIN.	
EVERGREEN TREES						
	JE	27	JUNIPERUS VIRGINIANA / EASTERN REDCEDAR	B & B	-	6' HT. MIN.
	PA2	19	PICEA ABIES / NORWAY SPRUCE	B & B	-	6' HT. MIN.
	PD	28	PICEA GLAUCA 'DENSATA' / BLACK HILLS WHITE SPRUCE	B & B	-	6' HT. MIN.
	PF	21	PINUS FLEXILIS 'VANDERWOLF'S PYRAMID' / VANDERWOLF'S PYRAMID LIMBER PINE	B & B	-	6' HT. MIN.
	TT	16	THUJA OCCIDENTALIS 'TECHNY' / TECHNY ARBORVITAE	B & B	-	6' HT. MIN.
	TI	20	THUJA X 'GREEN GIANT' / GREEN GIANT ARBORVITAE	B & B	-	6' HT. MIN.
ORNAMENTAL TREES						
	CE	19	CERCIS CANADENSIS / EASTERN REDBUD	B & B	-	6' HT. MIN.
	CM	17	CORNUS MAS / CORNELIAN CHERRY MULTI-STEM	B & B	-	6' HT. MIN.
	MP	17	MALUS X 'PRAIRIFIRE' / PRAIRIFIRE CRABAPPLE	B & B	-	6' HT. MIN.
STREET TREES						
	CA2	27	CARPINUS CAROLINIANA / AMERICAN HORNBEAM	B & B	2.5" CAL. MIN.	
	CC2	27	CELTIS OCCIDENTALIS 'CHICAGOLAND' / CHICAGOLAND HACKBERRY	B & B	2.5" CAL. MIN.	
	GP2	26	GINKGO BILOBA 'PRINCETON SENTRY' / PRINCETON SENTRY MAIDENHAIR TREE	B & B	2.5" CAL. MIN.	
	PA3	26	PLATANUS X ACERIFOLIA / LONDON PLANE TREE	B & B	2.5" CAL. MIN.	
	QE	26	QUERCUS X 'CRIMSCHMIDT' / CRIMSON SPIRE™ OAK	B & B	2.5" CAL. MIN.	
	UP	26	ULMUS AMERICANA 'PRINCETON' / PRINCETON AMERICAN ELM	B & B	2.5" CAL. MIN.	

SYMBOL	CODE	QTY	BOTANICAL / COMMON NAME	CONT	SPACING	SIZE
SHRUBS						
	HS	30	DIERVILLA X SPLENDENS 'EL MADRIGAL' / FIREFLY NIGHTGLOW BUSH HONEYSUCKLE	-	SEE PLAN	10" HT. MIN.
	VA	19	VIBURNUM DENTATUM 'CHRISTOM' / BLUE MUFFIN® ARROWWOOD VIBURNUM	-	SEE PLAN	18" HT. MIN.
	WV	20	WEIGELA FLORIDA 'VERWEIG' / MY MONET® WEIGELA	-	SEE PLAN	6" HT. MIN.
EVERGREEN SHRUBS						
	TA2	6	THUJA OCCIDENTALIS 'HOLMSTRUP' / HOLMSTRUP EASTERN ARBORVITAE	-	SEE PLAN	24" HT. MIN.
ORNAMENTAL GRASSES						
	AR	10	ANDROPOGON GERARDII 'RED OCTOBER' / RED OCTOBER BIG BLUESTEM	-	SEE PLAN	12" HT MIN
	CK	10	CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER' / KARL FOERSTER FEATHER REED GRASS	-	SEE PLAN	12" HT MIN
	PH	22	PANICUM VIRGATUM 'HEAVY METAL' / HEAVY METAL SWITCH GRASS	-	SEE PLAN	24" HT. MIN.
SHRUB AREAS						
	AB2	42	ACHILLEA MILLEFOLIUM 'BALVINOLET' / NEW VINTAGE™ VIOLET COMMON YARROW	1 GAL.	SEE PLAN	6" HT. MIN.
	AB	16	AMSONIA X 'BLUE ICE' / BLUE ICE BLUESTAR	1 GAL.	SEE PLAN	6" HT. MIN.
	HB	12	HEUCHERA X 'BLACK BEAUTY' / BLACK BEAUTY CORAL BELLS	1 GAL.	SEE PLAN	6" HT. MIN.
GROUND COVERS						
			CRUSHED STONE			
			PIZZO DRY BOTTOM DETENTION SEED MIX			
			PIZZO LOW PROFILE PRAIRIE SEED MIX			
			PIZZO WET-MESIC PRAIRIE SEED MIX			
			TURF SEED			

SYMBOL	BOTANICAL / COMMON NAME
	CRUSHED STONE
	PIZZO DRY BOTTOM DETENTION SEED MIX
	PIZZO LOW PROFILE PRAIRIE SEED MIX
	PIZZO WET-MESIC PRAIRIE SEED MIX
	TURF SEED



INS	05/19/25
RNM	03/28/25
DG	03/07/25
DATE	

VILLAGE RESUBMITTAL	
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ARC RE-SUBMITTAL	
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LANDSCAPE SCHEDULE

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Low Profile Prairie Seed Mix (Dry-Mesic Soils)

MIX STATISTICS table with columns for Average Mix Height (ft), Median Mix Height (ft), Species Heights (# of Occurrences in Mix), Number of Native Species in Mix, Native FQ, Native Mean C Value, National Wetland Category, Lbs/Acre of Native Seed, Seeds per Square Foot, and Percent of Mix (by Seed Count) Requiring Stratification.

Grasses, Sedges, & Rushes table with columns for CODE, SCIENTIFIC NAME, COMMON NAME, C-VALUE, W-VALUE, WETNESS, HEIGHT, BLOOM COLOR, BLOOM TIME, SEEDS/OZ, OZ/ACRE, LBS/ACRE, % OF MIX, GERMINATION, and TOP SOIL.

Wildflowers table with columns for CODE, SCIENTIFIC NAME, COMMON NAME, C-VALUE, W-VALUE, WETNESS, HEIGHT, BLOOM COLOR, BLOOM TIME, SEEDS/OZ, OZ/ACRE, LBS/ACRE, % OF MIX, GERMINATION, and TOP SOIL.

Wet-Mesic Prairie Seed Mix (Wet-Mesic Soils)

MIX STATISTICS table for Wet-Mesic Prairie Seed Mix, including Average Mix Height (ft), Median Mix Height (ft), Species Heights (# of Occurrences in Mix), Number of Native Species in Mix, Native FQ, Native Mean C Value, National Wetland Category, Lbs/Acre of Native Seed, Seeds per Square Foot, and Percent of Mix (by Seed Count) Requiring Stratification.

Grasses, Sedges, & Rushes table for Wet-Mesic Prairie Seed Mix, including columns for CODE, SCIENTIFIC NAME, COMMON NAME, C-VALUE, W-VALUE, WETNESS, HEIGHT, BLOOM COLOR, BLOOM TIME, SEEDS/OZ, OZ/ACRE, LBS/ACRE, % OF MIX, GERMINATION, and TOP SOIL.

Wildflowers table for Wet-Mesic Prairie Seed Mix, including columns for CODE, SCIENTIFIC NAME, COMMON NAME, C-VALUE, W-VALUE, WETNESS, HEIGHT, BLOOM COLOR, BLOOM TIME, SEEDS/OZ, OZ/ACRE, LBS/ACRE, % OF MIX, GERMINATION, and TOP SOIL.



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Dry-Bottom Detention Basin Seed Mix (Mesic-Wet Soils at the Bottom of Basins or Swales)

MIX STATISTICS table for Dry-Bottom Detention Basin Seed Mix, including Average Mix Height (ft), Median Mix Height (ft), Species Heights (# of Occurrences in Mix), Number of Native Species in Mix, Native FQ, Native Mean C Value, National Wetland Category, Lbs/Acre of Native Seed, Seeds per Square Foot, and Percent of Mix (by Seed Count) Requiring Stratification.

Grasses, Sedges, & Rushes table for Dry-Bottom Detention Basin Seed Mix, including columns for CODE, SCIENTIFIC NAME, COMMON NAME, C-VALUE, W-VALUE, WETNESS, HEIGHT, BLOOM COLOR, BLOOM TIME, SEEDS/OZ, OZ/ACRE, LBS/ACRE, % OF MIX, GERMINATION, and TOP SOIL.

Wildflowers table for Dry-Bottom Detention Basin Seed Mix, including columns for CODE, SCIENTIFIC NAME, COMMON NAME, C-VALUE, W-VALUE, WETNESS, HEIGHT, BLOOM COLOR, BLOOM TIME, SEEDS/OZ, OZ/ACRE, LBS/ACRE, % OF MIX, GERMINATION, and TOP SOIL.

Vertical sidebar containing project information: VILLAGE RESUBMITTAL (05/19/25), VILLAGE RESUBMITTAL (03/28/25), ARC RE-SUBMITTAL (03/07/25), DATE, REVISIONS, SCALE: AS NOTED, DESIGNED BY: INS, DRAWN BY: KITM, CHECKED BY: RNM, and logo for Kimley-Horn & Associates, Inc.

Vertical sidebar containing company information: PULTE HOME COMPANY, LLC, LANDSCAPE SEED MIXES, GREENWAY CHASE, 610 PETERSON ROAD, LIBERTYVILLE, IL 60048, ORIGINAL ISSUE: 01/03/2025, KHA PROJECT NO. 168247001, SHEET NUMBER L2.1

Vertical text on the left margin: Drawing name: \\GIS\DEV\BIB2025\000_Pulte_Libertyville_IL2_Design\CD\PlanSheets\L2.1 LANDSCAPE PLAN.dwg, L2.1 LANDSCAPE SEED MIXES, May 16, 2025, 1:36pm, by: Connie Brown, This document is the property of Kimley-Horn and Associates, Inc. and is intended for the specific project and site for which it was prepared. Reuse of any portion of this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

Drawing name: K:\GIS\DEV\16827001_Pulte_Libertyville_IL2 Landscape Plan.dwg L2.2 LANDSCAPE NOTES AND DETAILS May 16, 2025 1:30pm by: DanielaGrove
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PULTE HOME COMPANY, LLC

LANDSCAPE NOTES AND DETAILS

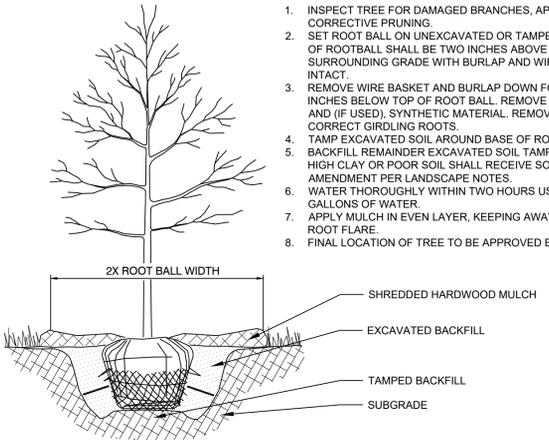
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ORIGINAL ISSUE:	01/03/2025
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L2.2

NOTES:

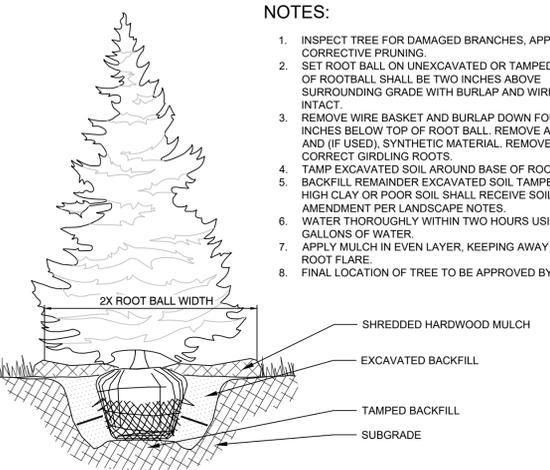
1. INSPECT TREE FOR DAMAGED BRANCHES, APPLY CORRECTIVE PRUNING.
2. SET ROOT BALL ON UNEXCAVATED OR TAMPED SOIL. TOP OF ROOTBALL SHALL BE TWO INCHES ABOVE SURROUNDING GRADE WITH BURLAP AND WIRE BASKET INTACT.
3. REMOVE WIRE BASKET AND BURLAP DOWN FOUR TO SIX INCHES BELOW TOP OF ROOT BALL. REMOVE ALL TWINE AND (IF USED), SYNTHETIC MATERIAL. REMOVE OR CORRECT GIRDLING ROOTS.
4. TAMP EXCAVATED SOIL AROUND BASE OF ROOTBALL. BACKFILL REMAINDER EXCAVATED SOIL TAMPED LIGHTLY. HIGH CLAY OR POOR SOIL SHALL RECEIVE SOIL AMENDMENT PER LANDSCAPE NOTES.
5. WATER THOROUGHLY WITHIN TWO HOURS USING 10 TO 15 GALLONS OF WATER.
7. APPLY MULCH IN EVEN LAYER, KEEPING AWAY FROM ROOT FLARE.
8. FINAL LOCATION OF TREE TO BE APPROVED BY OWNER.



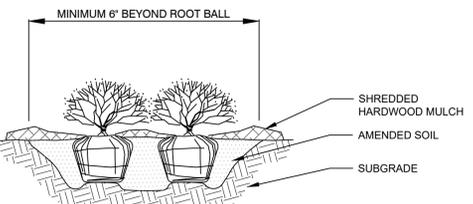
1 TREE PLANTING NTS

NOTES:

1. INSPECT TREE FOR DAMAGED BRANCHES, APPLY CORRECTIVE PRUNING.
2. SET ROOT BALL ON UNEXCAVATED OR TAMPED SOIL. TOP OF ROOTBALL SHALL BE TWO INCHES ABOVE SURROUNDING GRADE WITH BURLAP AND WIRE BASKET INTACT.
3. REMOVE WIRE BASKET AND BURLAP DOWN FOUR TO SIX INCHES BELOW TOP OF ROOT BALL. REMOVE ALL TWINE AND (IF USED), SYNTHETIC MATERIAL. REMOVE OR CORRECT GIRDLING ROOTS.
4. TAMP EXCAVATED SOIL AROUND BASE OF ROOTBALL. BACKFILL REMAINDER EXCAVATED SOIL TAMPED LIGHTLY. HIGH CLAY OR POOR SOIL SHALL RECEIVE SOIL AMENDMENT PER LANDSCAPE NOTES.
5. WATER THOROUGHLY WITHIN TWO HOURS USING 10 TO 15 GALLONS OF WATER.
7. APPLY MULCH IN EVEN LAYER, KEEPING AWAY FROM ROOT FLARE.
8. FINAL LOCATION OF TREE TO BE APPROVED BY OWNER.



2 EVERGREEN TREE PLANTING NTS



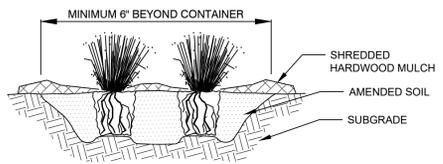
NOTES:

1. APPLY CORRECTIVE PRUNING.
2. SET ROOT BALL OR CONTAINER ON UNEXCAVATED OR TAMPED SOIL. TOP OF ROOTBALL (CONTAINER) SHALL BE ONE INCH ABOVE SURROUNDING GRADE. FOR LARGER SHRUBS WITHIN PLANTING BED DIG A DEEPER PIT ONLY FOR THOSE SHRUBS.
3. REMOVE BURLAP FROM TOP HALF THE LENGTH OF ROOTBALL. TWINE AND (IF USED) SYNTHETIC MATERIAL SHALL BE REMOVED FROM PLANTING BED. FOR CONTAINER GROWN SHRUBS, REMOVE CONTAINER AND LOOSEN ROOTS PRIOR TO INSTALLATION.
4. REMOVE OR CORRECT GIRDLING ROOTS.
5. PLUMB AND BACKFILL WITH AMENDED SOIL PER LANDSCAPE NOTES. WATER THOROUGHLY WITHIN TWO HOURS.
6. APPLY MULCH IN EVEN LAYER, KEEPING AWAY FROM ROOT FLARE. MULCH LIMITS FOR SHRUBS EXTEND TO ALL LIMITS OF PLANTING BED. SEE PLANS FOR BED LAYOUTS.

3 SHRUB PLANTING NTS

NOTES:

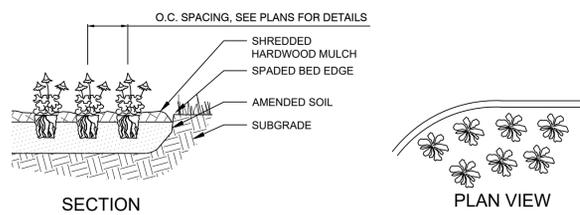
1. EXCAVATE PLANTING BED.
2. BED HEIGHT IS TO BE 2" ABOVE FINISH GRADE AND WELL DRAINED.
3. REMOVE CONTAINER. SCORE SOIL MASS TO REDIRECT AND PREVENT CIRCLING ROOTS. CORRECT GIRDLING ROOTS.
2. PLANT MATERIAL SHALL BE LAID OUT BY FOLLOWING THE BED EDGE, WORKING TOWARDS THE CENTER OF THE BED USING TRIANGULAR (STAGGERED) SPACING AS PLAUSIBLE.
3. PLUMB AND BACKFILL WITH PLANTING MIX AS SPECIFIED IN LANDSCAPE NOTES.
4. APPLY MULCH IN EVEN LAYER, KEEPING AWAY FROM ROOT FLARE. MULCH LIMITS FOR PERENNIALS/GROUNDCOVER EXTEND TO ALL LIMITS OF PLANTING BED. SEE PLANS FOR BED LAYOUTS.
5. SPACING TO BE AS SPECIFIED IN THE PLANT LIST. PERENNIALS SHALL BE PLACED WITH THEIR CENTER 24" FROM EDGE OF BED.



NOTES:

1. APPLY CORRECTIVE PRUNING.
2. SET ROOT BALL OR CONTAINER ON UNEXCAVATED OR TAMPED SOIL. TOP OF CONTAINER SHALL BE ONE INCH ABOVE SURROUNDING GRADE.
3. SYNTHETIC MATERIAL SHALL BE REMOVED FROM PLANTING BED. FOR CONTAINER GROWN GRASSES, REMOVE CONTAINER AND LOOSEN ROOTS PRIOR TO INSTALLATION.
4. REMOVE OR CORRECT GIRDLING ROOTS.
5. PLUMB AND BACKFILL WITH AMENDED SOIL PER LANDSCAPE NOTES. WATER THOROUGHLY WITHIN TWO HOURS.
6. APPLY MULCH IN EVEN LAYER, KEEPING AWAY FROM ROOT FLARE. MULCH LIMITS FOR GRASS EXTEND TO ALL LIMITS OF PLANTING BED. SEE PLANS FOR BED LAYOUTS.

4 ORNAMENTAL GRASS PLANTING NTS



5 PERENNIAL PLANTING NTS

LANDSCAPE NOTES

1. THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING MATERIALS AND PLANTS SHOWN ON THE LANDSCAPE PLAN. THE CONTRACTOR IS RESPONSIBLE FOR THE COST TO REPAIR UTILITIES, ADJACENT LANDSCAPE, PUBLIC AND PRIVATE PROPERTY THAT IS DAMAGED BY THE CONTRACTOR OR THEIR SUBCONTRACTOR'S OPERATIONS DURING INSTALLATION OR DURING THE SPECIFIED MAINTENANCE PERIOD. CALL FOR UTILITY LOCATIONS PRIOR TO ANY EXCAVATION.
2. THE CONTRACTOR SHALL REPORT ANY DISCREPANCY IN PLAN VS. FIELD CONDITIONS IMMEDIATELY TO THE LANDSCAPE ARCHITECT, PRIOR TO CONTINUING WITH THAT PORTION OF WORK.
3. NO PLANTING WILL BE INSTALLED UNTIL ALL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPAIR OF ANY OF THEIR TRENCHES OR EXCAVATIONS THAT SETTLE.
5. ALL PLANTS TO BE SPECIMEN GRADE, WELL BRANCHED, HEALTHY, FULL, PRE-INOCULATED AND FERTILIZED. PLANTS SHALL BE FREE FROM DISEASE, PESTS, WOUNDS, AND SCARS. PLANTS SHALL BE FREE FROM NOTICEABLE GAPS, HOLES, OR DEFORMITIES. PLANTS SHALL BE FREE FROM BROKEN OR DEAD BRANCHES. TRUNKS WILL BE WRAPPED IF NECESSARY TO PREVENT SUN SCALD AND INSECT DAMAGE. THE LANDSCAPE CONTRACTOR SHALL REMOVE THE WRAP AT THE PROPER TIME AS PART OF THIS CONTRACT.
6. THE OWNER'S REPRESENTATIVE MAY REJECT ANY PLANT MATERIALS THAT ARE DISEASED, DEFORMED, OR OTHERWISE NOT EXHIBITING SUPERIOR QUALITY.
7. ALL NURSERY STOCK SHALL BE GUARANTEED, BY THE CONTRACTOR, FOR ONE YEAR FROM DATE OF FINAL INSPECTION. THE GUARANTEE BEGINS ON THE DATE OF THE LANDSCAPE ARCHITECT'S OR OWNERS WRITTEN ACCEPTANCE OF THE INITIAL PLANTING. REPLACEMENT PLANT MATERIAL SHALL HAVE A ONE YEAR GUARANTEE COMMENCING UPON PLANTING.
8. PLANTS TO MEET AMERICAN STANDARD FOR NURSERY STOCK (ANSI Z60.1-2014 OR MOST CURRENT VERSION) REQUIREMENTS FOR SIZE AND TYPE SPECIFIED.
9. PRUNE PLANTS AS NECESSARY- PER STANDARD NURSERY PRACTICE AND TO CORRECT POOR BRANCHING OF EXISTING AND PROPOSED TREES.
10. TOPSOIL SHALL BE PROVIDED AND GRADED BY THE GENERAL CONTRACTOR UP TO 6 INCHES BELOW FINISHED GRADE IN TURF AREAS AND 18 INCHES IN PLANTING AREAS.
11. PLANTING AREA TOPSOIL SHALL BE AMENDED WITH 25% SPHAGNUM PEATMOSS, 5% HUMUS AND 70% PULVERIZED SOIL FOR ALL NON TURF SEED MIX AREAS, SHRUB, ORNAMENTAL GRASS, PERENNIAL AND ANNUAL BEDS.
12. SEED/SOD LIMIT LINES ARE APPROXIMATE. CONTRACTOR SHALL SEED/SOD ALL AREAS WHICH ARE DISTURBED BY GRADING WITH THE SPECIFIED SEED/SOD MIXES.
13. EDGING TO BE A SPADED EDGE UNLESS INDICATED OTHERWISE ON THE PLANS. SPADED EDGE TO PROVIDE V-SHAPED DEPTH AND WIDTH TO CREATE SEPARATION BETWEEN MULCH AND GRASS. A SPADED BED EDGE SHALL SEPARATE MULCH BEDS FROM TURF OR SEEDED AREAS. A SPADED EDGE IS NOT REQUIRED ALONG CURBED EDGES.
14. CONTRACTOR SHALL INSTALL SHREDDED HARDWOOD MULCH AT A 3" DEPTH TO ALL TREES, SHRUB, PERENNIAL, AND GROUNDCOVER AREAS. TREES PLACED IN AREA COVERED BY TURF SHALL RECEIVE A 4 FT WIDE MAXIMUM TREE RING WITH 3" DEPTH SHREDDED HARDWOOD MULCH.
15. INSTALLATION OF TREES WITHIN PARKWAYS SHALL BE COORDINATED IN THE FIELD WITH LOCATIONS OF UNDERGROUND UTILITIES. TREES SHALL NOT BE LOCATED CLOSER THAN 5' FROM UNDERGROUND UTILITY LINES AND NO CLOSER THAN 10' FROM UTILITY STRUCTURES.
16. DO NOT DISTURB THE EXISTING PAVING, LIGHTING, OR LANDSCAPING THAT EXISTS ADJACENT TO THE SITE UNLESS OTHERWISE NOTED ON PLAN.
17. ALL DISTURBED AREAS TO BE SODDED OR SEEDED, UNLESS OTHERWISE NOTED. SOD/SEED SHALL BE LOCAL HARDY TURF GRASS MIX UNLESS, OTHERWISE NOTED.
18. PLANT QUANTITIES SHOWN ARE FOR THE CONVENIENCE OF THE OWNER AND JURISDICTIONAL REVIEW AGENCIES. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL PLANT QUANTITIES AS DRAWN.

